



## DEVELOPMENT APPLICATION

**PDPLANPMTD-2024/041321**

**PROPOSAL:** Educational & Occasional Care (Childcare Centre)

**LOCATION:** 21 Commerce Drive, Howrah

**RELEVANT PLANNING SCHEME:** Tasmanian Planning Scheme - Clarence

**ADVERTISING EXPIRY DATE:** 14 March 2024

The relevant plans and documents can be inspected at the Council offices, 38 Bligh Street, Rosny Park, during normal office hours until 14 March 2024. In addition to legislative requirements, plans and documents can also be viewed at [www.ccc.tas.gov.au](http://www.ccc.tas.gov.au) during these times.

Any person may make representations about the application to the Chief Executive Officer, by writing to PO Box 96, Rosny Park, 7018 or by electronic mail to [clarence@ccc.tas.gov.au](mailto:clarence@ccc.tas.gov.au). Representations must be received by Council on or before 14 March 2024.

To enable Council to contact you if necessary, would you please also include a day time contact number in any correspondence you may forward.

Any personal information submitted is covered by Council's privacy policy, available at [www.ccc.tas.gov.au](http://www.ccc.tas.gov.au) or at the Council offices.

# Clarence City Council



## APPLICATION FOR DEVELOPMENT / USE OR SUBDIVISION

The personal information on this form is required by Council for the development of land under the Land Use Planning and Approvals Act 1993. We will only use your personal information for this and other related purposes. If this information is not provided, we may not be able to deal with this matter. You may access and/or amend your personal information at any time. How we use this information is explained in our **Privacy Policy**, which is available at [www.ccc.tas.gov.au](http://www.ccc.tas.gov.au) or at Council offices.

Proposal:

Childcare centre (education and occasional care)

Location:

Address 21 Commerce Drive

Suburb/Town Howrah

Postcode 7018

Current  
Owners/s:

Applicant:

**Personal Information Removed**

Tax Invoice for  
application fees to  
be in the name of:  
(if different from  
applicant)

Estimated cost of development

\$3,160,000

Is the property on the Tasmanian Heritage Register?

Yes

☐

No

☒

(if yes, we recommend you discuss your proposal with Heritage Tasmania prior to lodgement as exemptions may apply which may save you time on your proposal)



If you had pre-application discussions with a Council Officer, please give their name

Memory Hatendi and Amanda Beyer

Current Use of Site:

Vacant

Does the proposal involve land administered or owned by the Crown or Council?

Yes

☐

No

☒

**Declaration:**

- *I have read the Certificate of Title and Schedule of Easements for the land and am satisfied that this application is not prevented by any restrictions, easements or covenants.*
- *I authorise the provision of a copy of any documents relating to this application to any person for the purposes of assessment or public consultation. I agree to arrange for the permission of the copyright owner of any part of this application to be obtained. I have arranged permission for Council's representatives to enter the land to assess this application*
- *I declare that, in accordance with Section 52 of the Land Use Planning and Approvals Act 1993, that I have notified the owner of the intention to make this application. Where the subject property is owned or controlled by Council or the Crown, their signed consent is attached. Where the application is submitted under Section 43A, the owner's consent is attached.*
- *I declare that the information in this declaration is true and correct.*

**Acknowledgement:**

- *I acknowledge that the documentation submitted in support of my application will become a public record held by Council and may be reproduced by Council in both electronic and hard copy format in order to facilitate the assessment process; for display purposes during public consultation; and to fulfil its statutory obligations. I further acknowledge that following determination of my application, Council will store documentation relating to my application in electronic format only.*

Applicant's  
Signature:

Signature Monica Lamer Date 22/12/2023

**PLEASE REFER TO THE DEVELOPMENT/USE AND SUBDIVISION CHECKLIST  
ON THE FOLLOWING PAGES TO DETERMINE WHAT DOCUMENTATION MUST  
BE SUBMITTED WITH YOUR APPLICATION.**

### Documentation required:

#### 1. **MANDATORY DOCUMENTATION**

*This information is required for the application to be valid. An application lodged without these items is unable to proceed.*

- ✓ Details of the location of the proposed use or development.
- ✓ A copy of the current Certificate of Title, Sealed Plan, Plan or Diagram and Schedule of Easements and other restrictions for each parcel of land on which the use or development is proposed.
- ✓ Full description of the proposed use or development.
- ✓ Description of the proposed operation.  
*May include where appropriate: staff/student/customer numbers; operating hours; truck movements; and loading/unloading requirements; waste generation and disposal; equipment used; pollution, including noise, fumes, smoke or vibration and mitigation/management measures.*
- ✓ Declaration the owner has been notified if the applicant is not the owner.
- ☐ Crown or Council consent (if publically-owned land).
- ✓ Any reports, plans or other information required by the relevant zone or code.
- ☐ Fees prescribed by the Council.

*Application fees (please phone 03 6217 9550 to determine what fees apply). An invoice will be emailed upon lodgement.*

#### 2. **ADDITIONAL DOCUMENTATION**

*In addition to the mandatory information required above, Council may, to enable it to consider an application, request further information it considers necessary to ensure that the proposed use or development will comply with any relevant standards and purpose statements in the zone, codes or specific area plan, applicable to the use or development.*

#### ✓ **Site analysis plan and site plan**, including where relevant:

- Existing and proposed use(s) on site.
- Boundaries and dimensions of the site.
- Topography, including contours showing AHD levels and major site features.
- Natural drainage lines, watercourses and wetlands on or adjacent to the site.
- Soil type.
- Vegetation types and distribution, and trees and vegetation to be removed.
- Location and capacity of any existing services or easements on/to the site.
- Existing pedestrian and vehicle access to the site.
- Location of existing and proposed buildings on the site.
- Location of existing adjoining properties, adjacent buildings and their uses.
- Any natural hazards that may affect use or development on the site.
- Proposed roads, driveways, car parking areas and footpaths within the site.
- Any proposed open space, communal space, or facilities on the site.
- Main utility service connection points and easements.
- Proposed subdivision lot boundaries.

# Clarence City Council

## DEVELOPMENT/USE OR SUBDIVISION CHECKLIST



- ✓ Where it is proposed to erect buildings, **detailed plans** with dimensions at a scale of 1:100 or 1:200 showing:
  - *Internal layout of each building on the site.*
  - *Private open space for each dwelling.*
  - *External storage spaces.*
  - *Car parking space location and layout.*
  - *Major elevations of every building to be erected.*
  - *Shadow diagrams of the proposed buildings and adjacent structures demonstrating the extent of shading of adjacent private open spaces and external windows of buildings on adjacent sites.*
  - *Relationship of the elevations to natural ground level, showing any proposed cut or fill.*
  - *Materials and colours to be used on rooves and external walls.*
- ✓ Where it is proposed to erect buildings, a plan of the proposed **landscaping** showing:
  - *Planting concepts.*
  - *Paving materials and drainage treatments and lighting for vehicle areas and footpaths.*
  - *Plantings proposed for screening from adjacent sites or public places.*
- ✓ Any additional reports, plans or other information required by the relevant zone or code.

*This list is not comprehensive for all possible situations. If you require further information about what may be required as part of your application documentation, please contact Council's Planning Officers on (03) 6217 9550 who will be pleased to assist.*

## SEARCH OF TORRENS TITLE

VOLUME 181634	FOLIO 705
EDITION 2	DATE OF ISSUE 15-Oct-2021

SEARCH DATE : 13-Dec-2023

SEARCH TIME : 02.51 PM

DESCRIPTION OF LAND

City of CLARENCE

Lot 705 on Sealed Plan 181634

Derivation : Part of 390 Acres Gtd. to William Stanley

Sharland, William Tarleton and William Lovett

Prior CT 167648/802

SCHEDULE 1

D156967    TRANSFER to HIGHCLERE HOLDINGS PTY LTD    Registered  
02-Mar-2015 at noon

SCHEDULE 2

D17327 & D27051    Land is limited in depth to 15 metres,  
excludes minerals and is subject to reservations  
relating to drains sewers and waterways in favour of  
the Crown

SP181634 EASEMENTS in Schedule of Easements

SP181634 COVENANTS in Schedule of Easements

SP181634 FENCING PROVISION in Schedule of Easements

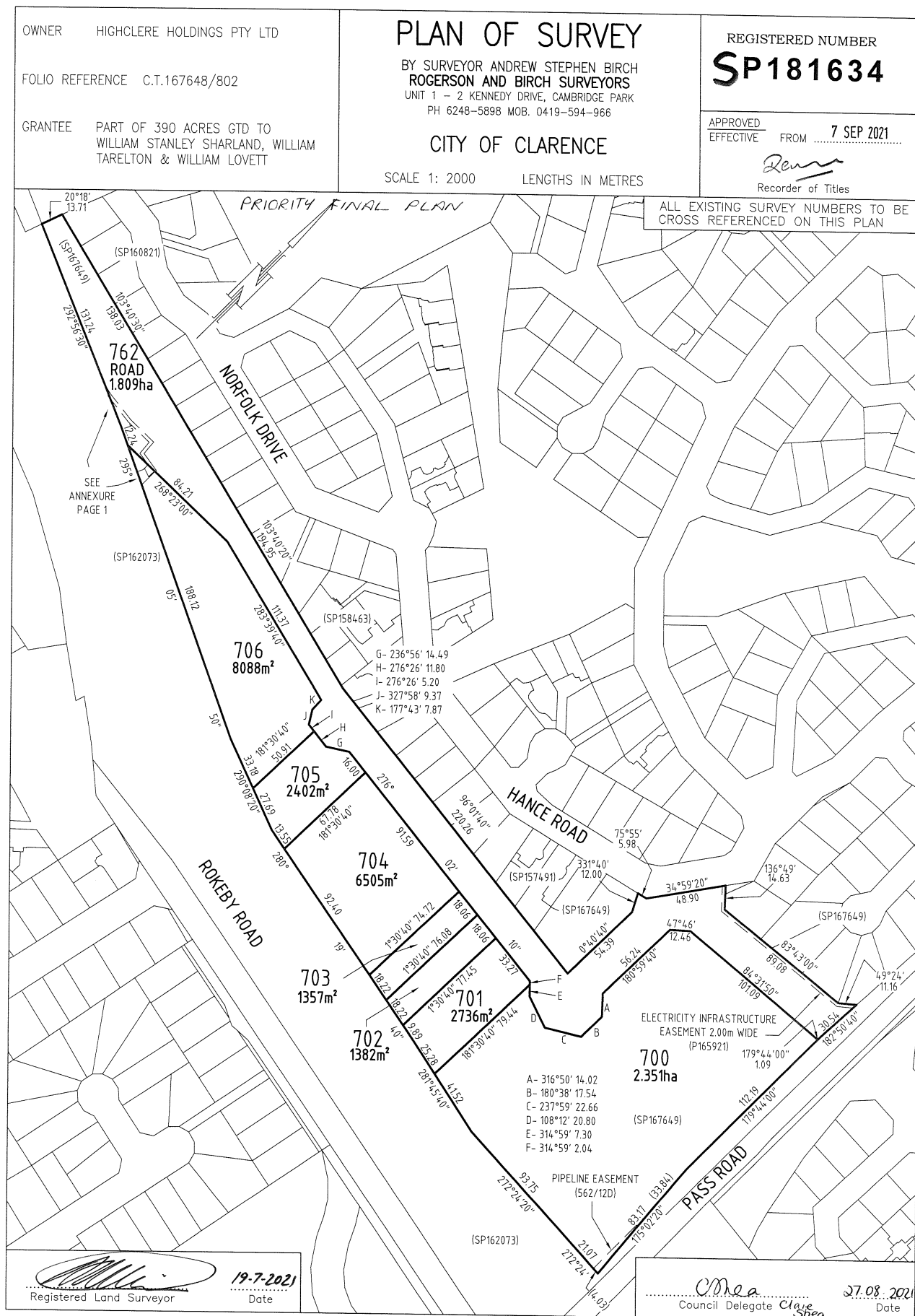
SP167488 &amp; SP167648 FENCING PROVISION in Schedule of Easements


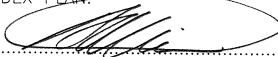
D17327 &amp; D27051    FENCING PROVISION in Transfer

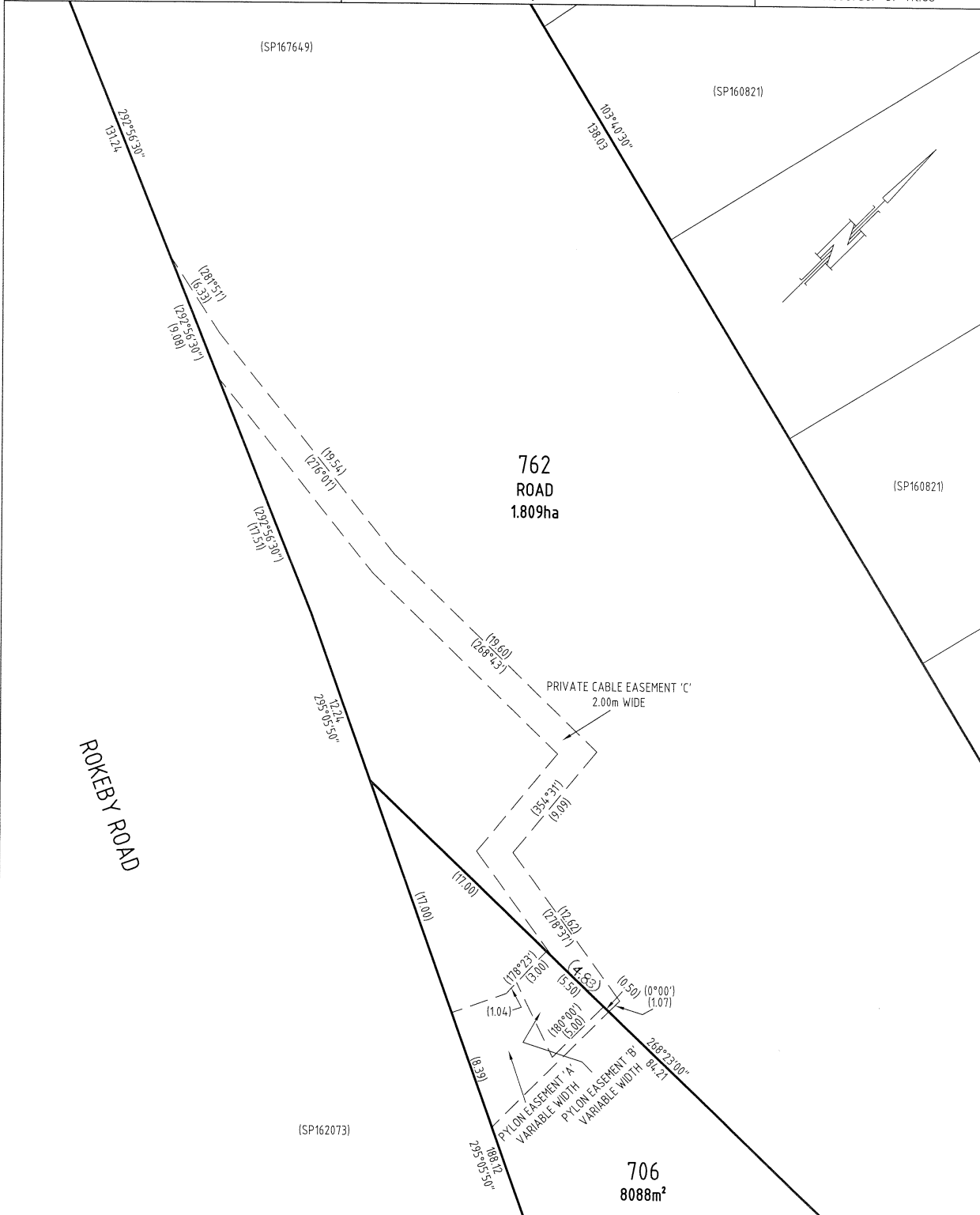
B330343    PROCLAMATION under Section 9A and 52A of the Roads  
and Jetties Act 1935

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



<h1>PLAN OF SURVEY</h1> <h2>ANNEXURE SHEET</h2> <p>SHEET 1 OF 1 SHEETS</p>	OWNER: HIGHCLERE HOLDINGS PTY LTD FOLIO REFERENCE: C.T.167648/802 SCALE 1:250                      LENGTH IN METRES	Registered Number <h1>SP181634</h1>
	SIGNED FOR IDENTIFICATION PURPOSES <div> <div>             Council Delegate         </div> <div>           27.08.2021            Date         </div> </div>	THIS ANNEXURE SHEET FORMS PART OF THE ATTACHED INDEX PLAN. <div> <div>             Registered Land Surveyor         </div> <div>           19-7-2021            Date         </div> </div>



The diagram is a survey plan showing two main parcels: 762 ROAD (1.809ha) and 706 (8088m²). The plan includes several easements: a 'PRIVATE CABLE EASEMENT C' (2.00m wide), 'PYLON EASEMENT A' (variable width), and 'PYLON EASEMENT B' (variable width 84.21m). The plan also shows 'ROKEBY ROAD' and various survey points with bearings and distances. A north arrow is present in the upper right quadrant.

<b>SCHEDULE OF EASEMENTS</b> <b>NOTE:</b> THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	Registered Number <b>SP 181634</b>
--	---------------------------------------

PAGE 1 OF 8 PAGES

EASEMENTS AND PROFITS

Each lot on the plan is together with:

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

**EASEMENTS**

Lot 762 on the plan is subject to an Electricity Infrastructure Easement (created by and more particularly defined in D90506) in gross in favour of Aurora Energy Pty Ltd over the land shown on the plan as **ELECTRICITY INFRASTRUCTURE EASEMENT 2.00m WIDE (P165921)**.

Lot 700 on the plan is subject to a Pipeline Easement (created by and more particularly defined in SP163394) in gross in favour of the Tasmanian Water and Sewerage Corporation (Southern Region) Pty Limited over the land marked PIPELINE EASEMENT on SP163394 and shown on the plan as **PIPELINE EASEMENT (562/12D)**.

Lot 700 on the plan is together with a Private Signage Easement A (as defined) over that part of Lot 706 marked **PYLON EASEMENT 'A' VARIABLE WIDTH** on the plan.

Lot 706 on the plan is subject to a Private Signage Easement A (as defined) over the land marked **PYLON EASEMENT 'A' VARIABLE WIDTH** on the plan (appurtenant to Lot 700 on the plan).

Lot 700 on the plan is together with a Private Electricity Infrastructure Easement A (as defined) over that part of Lot 762 marked **PRIVATE CABLE EASEMENT 'C' 2.00m WIDE** on the plan.

Director

Director

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: Highclere Holdings Pty Ltd FOLIO REF: 167648/802 SOLICITOR & REFERENCE: Page Seager (DAS 060985)	PLAN SEALED BY: DATE: 27 August 2021 SD-2016/12 ..... O'Meara REF NO. CLARE SHEA Council Delegate
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**NOTE:** The Council Delegate must sign the Certificate for the purposes of identification.



<b>ANNEXURE TO SCHEDULE OF EASEMENTS</b> PAGE 2 OF 8 PAGES	Registered Number <b>SP 181634</b>
SUBDIVIDER: Highclere Holdings Pty Ltd FOLIO REFERENCE: 167648/802	

Lot 762 on the plan is subject to a Private Electricity Infrastructure Easement A (as defined) over the land marked **PRIVATE CABLE EASEMENT 'C' 2.00mWIDE** on the plan (appurtenant to Lot 700 on the plan).

Lots 701, 702, 703, 704, and 705 on the plan are each together with a Private Signage Easement B (as defined) over that part of Lot 706 marked **PYLON EASEMENT 'B' VARIABLE WIDTH** on the plan.

Lot 706 on the plan is subject to a Private Signage Easement B (as defined) over the land marked **PYLON EASEMENT 'B' VARIABLE WIDTH** on the plan (appurtenant to Lots 701, 702, 703, 704, and 705 on the plan).

Lots 701, 702, 703, 704, 705 and 706 on the plan are each together with the Private Electricity Infrastructure Easement B (as defined) over that part of Lot 762 marked **PRIVATE CABLE EASEMENT 'C' 2.00m WIDE** on the plan.

Lot 762 on the plan is subject to the Private Electricity Infrastructure Easement B (as defined) over the land marked **PRIVATE CABLE EASEMENT 'C' 2.00mWIDE** on the plan (appurtenant to Lots 701, 702, 703, 704, 705 and 706 on the plan).

### COVENANTS

The owners of lots 701 to 706 (inclusive) on the plan covenant with the Vendor (Highclere Holdings Pty Ltd) and the owners for the time being of every other lot shown on the Plan to the intent that the burden of these covenants may run with and bind the covenantors' lot and every part thereof and that the benefit thereof may be annexed to and devolve with each and every part of every other lot shown on the Plan to observe the following stipulations:-

Director

Director

**NOTE:** Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.



<b>ANNEXURE TO SCHEDULE OF EASEMENTS</b> PAGE 3 OF 8 PAGES	Registered Number <b>SP 181634</b>
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1. Not to construct, or allow to be constructed, on such lot any improvements above a height as may be approved for one storey buildings under the Clarence Interim Planning Scheme 2015 or such planning scheme as replaces it; except:
  - (i) in respect of lot 701 on the plan, this covenant does not restrict a service station canopy being constructed above that height limit as is customary for service stations; and
  - (ii) in respect of lot 706 on the plan, this covenant does not restrict an indoor children's slide and covering structure being constructed above that height limit.
2. The owners of all lots on the plan (excepting lot 762) covenant with the Clarence City Council to the intent that the burden of these covenants may run with and bind the covenantors' lot and every part thereof and that the benefit thereof may be annexed to and devolve with each and every part of every other lot shown on the Plan to observe the following stipulations:-
  - (i) not to discharge any stormwater from such lot other than in a manner consistent with the State Stormwater Strategy 2010 (or its successor).

**FENCING PROVISION**

In respect of the lots shown on the Plan, the Vendor (Highclere Holdings Pty Ltd) shall not be required to fence.

**DEFINITIONS**

"Private Signage Easement A" means the full and free right of every person (**Dominant Owner**) who is entitled to an estate or interest in possession indicated as the dominant tenement, or any part of it, with which that right will be capable of enjoyment in common with the owner of the servient tenement to erect, construct, place, inspect, alter, add, repair, renew, maintain and use in, upon, over and along and remove from the servient land a single pylon sign together with such poles, wires, cables, apparatus, appliances,

Director

Director

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SUBDIVIDER: Highclere Holdings Pty Ltd FOLIO REFERENCE: 167648/802	

signage (including illuminated signage) and other ancillary work for the benefit of such single pylon sign (all of which are hereinafter collectively referred to as “the infrastructure”) and for the transmission and distribution of electrical energy to such signage in the land shown on the plan and marked **PYLON EASEMENT ‘A’ VARIABLE WIDTH (Servient Land)** and for the Dominant Owner and their employees, agents and contractors from time to time and at all times to enter into and upon the Servient Land or any part of it bringing upon the Servient Land such material, machinery and other things as it considers fit and proper to inspect the condition of the infrastructure and to repair, alter and amend HOWEVER that any damage occasioned in doing so will be made good AND together to the benefit of a restriction on the owner of the servient tenement from constructing, or allowing the construction of, any structure, landscaping or obstruction on the Servient Land which would at any time restrict or hinder the visibility of the pylon signage installed by the Dominant Owner.

“**Private Signage Easement B**” means the full and free right of every person (**Dominant Owner**) who is entitled to an estate or interest in possession indicated as the dominant tenement, or any part of it, with which that right will be capable of enjoyment in common with the owner of the servient tenement to erect, construct, place, inspect, alter, add, repair, renew, maintain and use in, upon, over and along and remove from the servient land a single pylon sign together with such poles, wires, cables, apparatus, appliances, signage (including illuminated signage) and other ancillary work for the benefit of such single pylon sign (all of which are hereinafter collectively referred to as “the infrastructure”) and for the transmission and distribution of electrical energy to such signage in the land shown on the plan and marked **PYLON EASEMENT ‘B’ VARIABLE WIDTH (Servient Land)** and for the Dominant Owner and their employees, agents and contractors from time to time and at all times to enter into and upon the Servient Land or any part of it bringing upon the Servient Land such material, machinery and other things as it considers fit and proper to inspect the condition of the infrastructure and to repair, alter and amend HOWEVER that any damage occasioned in doing so will be made good AND such right being:

1. together to the benefit of a restriction on the owner of the servient tenement from constructing, or allowing the construction of, any structure, landscaping or obstruction on the Servient Land which

  
\_\_\_\_\_  
Director  
\_\_\_\_\_  
Director

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C

<b>ANNEXURE TO SCHEDULE OF EASEMENTS</b> PAGE 5 OF 8 PAGES	Registered Number <b>SP 181634</b>
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would at any time restrict or hinder the visibility of the pylon signage installed by the Dominant Owner;

2. subject to an obligation to properly maintain, repair and replace such signage and infrastructure;
3. subject to the further proportionate rights, restrictions and contributions (including the reservation to the owner of the servient tenement of use of pylon signage in the Servient Land) as follows:

Lot No.	% of total signage area on pylon sign available for use	Cost of installation of own signage (%)	Cost of maintenance, repair and replacement of the infrastructure (%)
Lot 701	5	100	5
Lot 702	5	100	5
Lot 703	5	100	5
Lot 704	50	100	50
Lot 705	10	100	10
Lot 706	25	100	25

“Private Electricity Infrastructure Easement A” means the full and free right of every person (Dominant Owner) who is entitled to an estate or interest in possession indicated as the dominant tenement, or any part of it, with which that right will be capable of enjoyment in common with the owner of the servient tenement to erect, construct, place, inspect, alter, add, repair, renew, maintain and use in, upon,

Director

Director

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<b>ANNEXURE TO SCHEDULE OF EASEMENTS</b> PAGE 6 OF 8 PAGES	Registered Number <b>SP 181634</b>
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over and along and remove from the servient land towers, poles, wires, cables, apparatus, appliances and other ancillary work (all of which are hereinafter collectively referred to as “the infrastructure”) for the transmission and distribution of electrical energy and for purposes incidental thereto in the strip of land shown on the plan and marked **PRIVATE CABLE EASEMENT 'C' VARIABLE WIDTH (Servient Land)** and for the Dominant Owner and their employees, agents and contractors from time to time and at all times to enter into and upon the Servient Land or any part of it bringing upon the Servient Land such material, machinery and other things as it considers fit and proper to inspect the condition of the infrastructure and to repair, alter and amend HOWEVER that any damage occasioned in doing so will be made good, and

“**Private Electricity Infrastructure Easement B**” means the full and free right of every person (**Dominant Owner**) who is entitled to an estate or interest in possession indicated as the dominant tenement, or any part of it, with which that right will be capable of enjoyment in common with the owner of the servient tenement to erect, construct, place, inspect, alter, add, repair, renew, maintain and use in, upon, over and along and remove from the servient land towers, poles, wires, cables, apparatus, appliances, and other ancillary work (all of which are hereinafter collectively referred to as “the infrastructure”) for the transmission and distribution of electrical energy and for purposes incidental thereto in the strip of land shown on the plan and marked **PRIVATE CABLE EASEMENT 'C' VARIABLE WIDTH (Servient Land)** and for the Dominant Owner and their employees, agents and contractors from time to time and at all times to enter into and upon the Servient Land or any part of it bringing upon the Servient Land such material, machinery and other things as it considers fit and proper to inspect the condition of the infrastructure and to repair, alter and amend HOWEVER that any damage occasioned in doing so will be made good AND such right being:

1. subject to an obligation to properly maintain, repair and replace such infrastructure;
2. subject to the further obligation to pay the proportionate contributions to properly maintain, repair and replace such infrastructure as follows:

  
Director  
Director

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<b>ANNEXURE TO SCHEDULE OF EASEMENTS</b> PAGE 7 OF 8 PAGES	Registered Number <b>SP 181634</b>
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Lot No.	Cost of maintenance, repair and replacement of the infrastructure (%)
Lot 701	5
Lot 702	5
Lot 703	5
Lot 704	50
Lot 705	10
Lot 706	25

  
Director  
Director

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<b>ANNEXURE TO SCHEDULE OF EASEMENTS</b> PAGE 8 OF 8 PAGES	Registered Number <b>SP 181634</b>
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**EXECUTED by HIGHCLERE HOLDINGS PTY LTD )**  
**(ACN 603 116 650) as registered proprietor of the land )**  
 comprised in Folio of the Register Volume 167648 )  
 Folio 802 pursuant to section 127 of the *Corporations Act* )  
 2001 (Cth) by: )



.....  
 Director Signature

.....  
 Director Full Name (print)

.....  
 \*Director/\*Secretary Signature

.....  
 \*Director/\*Secretary Full Name (print)

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21 Commerce Drive,  
Howrah  
**Tipalea Partners**

Supporting planning report | 22 December 2023

ERA Planning and Environment acknowledge *palawa* as the Traditional Owners of *lutruwita* (Tasmania).

They are the original custodians of our land, sky and waters. We respect their unique ability to care for country and deep spiritual connection to it.

We honour and pay our respect to Elders past and present, whose knowledge and wisdom has and will ensure the continuation of culture and traditional practices.

We acknowledge that their sovereignty has never been ceded.

Always was, always will be.

## ERA Planning Pty Ltd trading as ERA Planning and Environment

ABN 67 141 991 004

This document may only be used for the purposes for which it was commissioned and in accordance with the Terms of Engagement for the commission. Unauthorised use of this document in any form whatsoever is prohibited.

**Job Number:** 2324-030

### Document Status

Document Version	Date	Author	Reviewer
Draft	22 December 2023	Dana Elphinstone Monica Cameron	Clare Hester
Final	22 December 2023	Dana Elphinstone Monica Cameron	For submission



# Permit overview

## Permit application details

<b>Applicant</b>	Tipalea Private No. 28
<b>Owner</b>	Highclere Holdings Pty Ltd
<b>Address</b>	21 Commerce Drive, Howrah
<b>Lot description</b>	Folio of the Register 181634, Lot 705
<b>Description of proposal</b>	Development and use of childcare centre.

## Relevant Planning Provisions

<b>Applicable planning scheme</b>	Tasmanian Planning Scheme - Clarence
<b>Zone(s)</b>	General Business
<b>Codes</b>	Parking and Sustainable Transport Code Road and Railway Assets Code Safeguarding of Airports Code Glebe Hill Neighbourhood Centre Specific Area Plan
<b>Discretions</b>	Clause 15.3.1 All uses (P1) Clause 15.4.2 Setbacks (P1) Clause 15.4.4 Fencing (P1) Clause C2.6.5 Pedestrian access (P1) Clause C2.6.8 Siting of parking and turning areas (P1) Clause C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction (P1) Clause C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area (P1) Clause CLA-S13.6.1 Precinct objectives (P1) Clause CLA-S13.7.1 Materials and design (P1 and P2) Clause CLA-S13.7.2 Siting and scale (P1) Clause CLA-S13.7.3 Staging (P1) Clause CLA-S13.7.4 Public art (P1)

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# 1 Introduction

## 1.1 Purpose of the report

ERA Planning and Environment (ERA) has been engaged by Tipalea Private No. 28 to seek planning approval for the use and development of a childcare centre located at 21 Commerce Drive, Howrah. This report provides the relevant background material, proposal details, and an appraisal of the development against the relevant planning provisions.

## 1.2 Name of planning authority

The relevant planning authority is the Clarence City Council.

## 1.3 Statutory controls

This planning permit application is to be assessed in accordance with the *Land Use Planning and Approvals Act 1993* (LUPAA) and is subject to the provisions of the *Tasmanian Planning Scheme - Clarence* (the planning scheme).

Specifically, the proposal requires assessment against the applicable zone purpose, use standards, development standards, and code requirements of the planning scheme.

## 1.4 Title documentation

This planning permit application relates to land at 21 Commerce Drive, Howrah (title reference CT 181634/705), under the ownership of Highclere Holdings Pty Ltd.

The landowner has been notified of the intention to lodge this planning permit application pursuant to clause 52 of LUPAA.

Title documents are available at Appendix A.

## 1.5 Enquiries

Enquiries relating to this planning report should be directed to:

Monica Cameron  
Senior Planner  
ERA Planning and Environment  
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Phone: 03 6165 0443

## 2 Proposal

The proposal seeks approval for the use and development of a childcare centre at 21 Commerce Drive in Howrah. The subject site is currently vacant, so no demolition works are proposed.

The proposed development includes:

- The development of a 5.3 m high building with a floor area of 766 m<sup>2</sup> providing six activity rooms, a kitchen, laundry, reception area, four bathrooms, three prep spaces, and associated office and training spaces.
- Two outdoor play areas of 645 m<sup>2</sup> and 171 m<sup>2</sup>.
- Parking for 24 vehicles and circulation areas including:
  - 12 x 90 degree parking spaces (2.6 m wide by 4.8 m long)
  - 11 x 90 degree parking spaces (2.6 m wide by 5.4 m long)
  - One accessible space (2.4 m wide by 5.4 m long) with 2.6 m wide by 5.4 m long adjoining space
- Bicycle parking spaces for four bicycles
- Outdoor bin storage surrounded by 1.8 m high colorbond fencing
- Trees along southern fence line and low shrubs around the parking area.
- Fencing:
  - 1.8 m high white picket fence along frontage
  - 2.4 m colorbond fence along side and rear boundaries
- Public art work is proposed for the site to be designed in consultation with Council

Architectural plans prepared by Brown Falconer are provided in Appendix C.

Concept servicing plans have been prepared by Gandy and Roberts for the development and confirm that it can be adequately serviced by TasWater infrastructure (refer to Appendix D for Concept Services Report).

The centre will provide 19 staff to care for up to 114 children across six activity spaces including:

- 24 children aged from 0 to 24 months, with six staff;
- 40 children aged 24 to 36 months with eight staff; and
- 50 children aged over 36 months with five staff.

The centre is proposed to operate 6.30 am to 6.30 pm Monday to Saturday.

A Traffic Impact Assessment prepared by Midson Traffic is provided in Appendix E.

An Acoustic Impact Assessment prepared by Noise Vibration Consulting is provided in Appendix F.

No signage is proposed as part this development. Any future signage will seek planning approval as required.

## 3 Site description

### 3.1 Site and surrounds

The subject site is land located at 21 Commerce Drive, Howrah and contains one title, CT 181634/705. The site is generally flat with one existing vehicle crossing to Commerce Drive. The site has been cleared of vegetation and has no existing buildings. The site is 2,405 m<sup>2</sup> in area and has 42.29 m frontage to Commerce Drive on the northern boundary.

The site is located in the General Business zone under the Tasmanian Planning Scheme – Clarence. The site adjoins Rokeby Road to the south and undeveloped General Business land to the east and west. There is an existing residential area to the north of Commerce Drive, and to the south of Rokeby Road.

An aerial image of the subject site and surrounding context is shown in Figure 1.



Figure 1 Aerial image of the site shown in blue outline (Source: <https://www.thelist.tas.gov.au/>)

## 4 Zoning assessment

### 4.1 Zoning

The site is zoned General Business in the planning scheme. The proposal requires assessment against the applicable zone purpose, use standards, and development standards of the General Business zone.

### 4.2 Use status

The proposed use is defined as educational and occasional care under the planning scheme. Educational and occasional care is a permitted use in the General Business zone.

### 4.3 Zone purpose

The General Business zone purpose in clause 15.1 is:

*15.1.1 To provide for business, retail, administrative, professional, community, and entertainment functions within Tasmania's main suburban and rural centres.*

*15.1.2 To ensure that the type and scale of use and development does not compromise or distort the activity centre hierarchy.*

*15.1.3 To encourage activity at pedestrian levels with active frontages and shop windows offering interest and engagement to shoppers.*

*15.1.4 To encourage Residential and Visitor Accommodation use if it supports the viability of the activity centre and an active street frontage is maintained.*

The proposed use and development is consistent with the zone purpose statements. The proposed development will provide a business function that serves the local community.

However, as the application is for a permitted use an assessment against the zone purpose is not required.

### 4.4 Use and development standards

Table 1 provides a summary of the applicable use and development standards for the proposal. An assessment against the applicable standards is provided in the sections following.

Table 1: Applicable standards in the General Residential Zone

Clause	Applicability
<b>Use standards</b>	
Clause 15.3.1 All uses	Applicable
Clause 15.3.2 Discretionary uses	Not applicable
Clause 15.3.3 Retail impact	Not applicable
<b>Development standards</b>	
Clause 15.4.1 Building height	This clause is replaced by clause CLA S13.7.2 under the Glebe Hill Neighbourhood Centre Specific Area Plan. Clause CLA S13.7.2 is addressed in Section 5.5.3 of this report.
Clause 15.4.2 Setbacks	Applicable
Clause 15.4.3 Design	This clause is replaced by clause CLA S13.7.1 under the Glebe Hill Neighbourhood Centre Specific Area Plan. Clause CLA S13.7.1 is addressed in Section 5.5.2 of this report.



Clause	Applicability
Clause 15.4.4 Fencing	Applicable
Clause 15.4.5 Outdoor storage areas	Applicable
Clause 15.4.6 Dwellings	Not applicable
<b>Subdivision standards</b>	
Subdivision clauses	Not applicable. No subdivision is proposed.

#### 4.4.1 All uses

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause 15.3.1 All uses</b>	
<b>A1</b> Hours of operation of a use, excluding Emergency Services, Natural and Cultural Values Management, Passive Recreation, Residential, Utilities or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must be within the hours of: (a) 7.00am to 9.00pm Monday to Saturday; and (b) 8.00am to 9.00pm Sunday and public holidays.	<b>P1</b> Hours of operation of a use, excluding Emergency Services, Natural and Cultural Values Management, Passive Recreation, Residential, Utilities or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must not cause an unreasonable loss of amenity to the residential zones having regard to: (a) the timing, duration or extent of vehicle movements; and (b) noise, lighting or other emissions.

#### Planner Response

The site is located approximately 20 m from the General Residential zone. The proposed operating hours for the development are 6.30 am to 6.30 pm. This exceeds the operating hours allowed under the acceptable solution. Therefore, the performance criteria have been addressed.

It is expected that the majority of children will be dropped off between the hours of 7 am to 8 am. This is generally followed by a period of settling in with children normally inside the centre. The number of children in the outdoor areas is expected to be minimal in the early hours of the morning and in the evening, particularly during the colder months.

A Traffic Impact Assessment has been prepared to determine the impact of the timing, duration, and extent of vehicle movements. This finds the opening hours of the childcare centre to be suitable for the location. Refer to Appendix E for further details.

External lighting will be in operation between 6.30 am to 6.30 pm to ensure people can safely use the site. External lighting will be designed in accordance with the Australian Standard to ensure appropriate baffling, angling and strength so there is no light spillage on adjoining sites. The street lighting in Commerce Drive is closer to the General Residential zone than the subject site and consequently will have more of an impact on residential amenity given the proximity.

An acoustic fence is proposed around the outdoor play areas. This will reduce any noise impacts from children playing, to adjoining residential properties. Refer to the Noise Impact Assessment in Appendix E for details regarding the fence and acoustic recommendations for the site.

Based on the above, the proposed use is not considered to have an unreasonable impact of the amenity of adjacent sensitive uses.

**The performance criterion (P1) is met.**

<b>A2</b> External lighting for a use, excluding Natural and Cultural Values Management, Passive Recreation, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must: (a) not operate within the hours of 11.00pm to 6.00am, excluding any security lighting; and	<b>P2</b> External lighting for a use, excluding Natural and Cultural Values Management, Passive Recreation, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must not cause an unreasonable loss of amenity to the residential zones, having regard to: (a) the level of illumination and duration of lighting; and
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(b) if for security lighting, must be baffled so that direct light does not extend into the adjoining property in those zones.	(b) the distance to habitable rooms of an adjacent dwelling.
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### Planner Response

External lighting will be in operation between 6.30 am to 6.30 pm. The site does not adjoin any zone referenced in the standard.

**The acceptable solution (A2) is met.**

<b>A3</b> Commercial vehicle movements and the unloading and loading of commercial vehicles for a use, excluding Emergency Services, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must be within the hours of: (a) 7.00am to 9.00pm Monday to Saturday; and (b) 8.00am to 9.00pm Sunday and public holidays.	<b>P3</b> Commercial vehicle movements and the unloading and loading of commercial vehicles for a use, excluding Emergency Services, Residential or Visitor Accommodation, on a site within 50m of a General Residential Zone or Inner Residential Zone, must not cause an unreasonable loss of amenity to the residential zones, having regard to: (a) the time and duration of commercial vehicle movements; (b) the number and frequency of commercial vehicle movements; (c) the size of commercial vehicles involved; (d) manoeuvring required by the commercial vehicles, including the amount of reversing and associated warning noise; (e) any noise mitigation measures between the vehicle movement areas and the residential zone; and (f) potential conflicts with other traffic.
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### Planner Response

Commercial vehicle movements will be limited to the hours detailed in the acceptable solution.

**The acceptable solution (A3) is met.**

## 4.4.2 Setbacks

Clause 15.4.2 Setbacks	
<b>A1</b> Buildings must be: (a) built to the frontage at ground level; or (b) have a setback of not more or less than the maximum and minimum setbacks of the buildings on adjoining properties.	<b>P1</b> Buildings must have a setback from a frontage that is compatible with the streetscape and minimizes opportunities for crime and anti-social behaviour, having regard to: (a) providing small variations in building alignment to break up long façades; (b) providing variations in building alignment appropriate to provide a forecourt or space for public use, such as outdoor dining or landscaping; (c) the avoidance of concealment spaces; (d) the ability to achieve passive surveillance; and (e) the availability of lighting.

### Planner Response

The proposed building is setback 9 m from the frontage. The adjoining properties are currently undeveloped. The proposal does not meet the acceptable solution; therefore, the performance criteria have been addressed.

The 9m setback is a requirement of the Glebe Hill Neighbourhood Centre Specific Area Plan, therefore the setback is considered compatible with the desired streetscape for the area. Refer to Section 5.5.3 of this report.

The design of the frontage façade includes architectural detail to minimise expanses of blank wall including variation in building alignment and glazing.

There is ample separation between the main building and the outdoor storage area, with open space surrounding both. This will limit the potential for concealment.

The building design includes multiple glazed features in the frontage façade to allow for passive surveillance. Lighting is proposed to ensure safety within the parking lot area and will be provided in accordance with Australian Standards.

**The performance criterion (P1) is met.**

<p><b>A2</b></p> <p>Building must have a setback from an adjoining property within a General Residential Zone or Inner Residential Zone of not less than:</p> <p>(a) 5m; or</p> <p>(b) half the wall height of the building, whichever is the greater.</p>	<p><b>P2</b></p> <p>Buildings must be sited to not cause an unreasonable loss of residential amenity to adjoining properties within a General Residential Zone or Inner Residential Zone, having regard to:</p> <p>(a) overshadowing and reduction in sunlight to habitable rooms and private open space of dwellings;</p> <p>(b) overlooking and reduction of privacy to the adjoining property; or</p> <p>(c) visual impacts caused by the apparent scale, bulk or proportions of the building when viewed from the adjoining property.</p>
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#### Planner Response

The site does not adjoin the General Residential or Inner Residential zone.

**The acceptable solution (A2) is met.**

<p><b>A3</b></p> <p>Air extraction, pumping, refrigeration systems or compressors must be separated a distance of not less than 10m from a General Residential Zone or Inner Residential Zone.<sup>1</sup></p> <p><sup>1</sup>An exemption applies for air conditioners and heat pumps in this zone – see Table 4.6</p>	<p><b>P3</b></p> <p>Air conditioning, air extraction, pumping, heating or refrigeration systems or compressors within 10m of a General Residential Zone or Inner Residential Zone, must be designed, located, baffled or insulated to not cause an unreasonable loss of amenity to the adjoining residential zones, having regard to:</p> <p>(a) the characteristics and frequency of emissions generated;</p> <p>(b) the nature of the proposed use;</p> <p>(c) the topography of the site and location of the sensitive use; and</p> <p>(d) any proposed mitigation measures.</p>
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#### Planner Response

The site is located at least 20 m from the General Residential zone, and there is no Inner Residential zoned land in the area.

**The acceptable solution (A3) is met.**

### 4.4.3 Fencing

Clause 15.4.4 Fencing	
<p><b>A1</b></p> <p>No Acceptable Solution.<sup>2</sup></p> <p><sup>2</sup> An exemption applies for fences in this zone – see Table 4.6.</p>	<p><b>P1</b></p> <p>A fence (including a free-standing wall) within 4.5m of a frontage must contribute positively to the streetscape, having regard to:</p> <p>(a) its height, design, location and extent;</p> <p>(b) its degree of transparency; and</p> <p>(c) the proposed materials and construction.</p>

#### Planner Response

The fence along the front title boundary, being 1.8m picket fence within 4.5 m of the boundary, meets the exemption under Clause 4.6.3 of the planning scheme.

The side fences where they are more than 4.5 m from the front boundary, and internal fences, meet the exemption under Clause 4.6.3 of the planning scheme.

The only fencing that requires assessment includes:

- The side boundary fences that are within 4.5 m of the front boundary. These are 1.8 m high colorbond fences.
- the part of the fencing that surrounds the bin enclosure that is within 4.5 m of the front boundary. This fencing type is 1.8m high colorbond fencing.
- The part of the acoustic fence separating the outdoor play area from the car park that is within 4.5 m of the front boundary. This fencing is 1.8 m-2 m high 12 mm fixed cement sheet.

These fences require assessment against the performance criteria.

The side boundary fence is 1.8 m high to minimise viewlines from the neighbouring properties to protect the safety of the children using the site, minimise noise emissions, and to minimise the car park noise and light emissions. The fence has limited transparency which is considered necessary for these reasons.

The bin enclosure is surrounded by a 1.8 m high fence to minimise viewlines into the bin enclosure from pedestrians walking past. At this height, most adults would not be able to see over the fence and into the bin area.

The acoustic fence separating the outdoor play area from the car park has been recommended by the Acoustic Engineers on the project, as discussed in the Noise Impact Assessment. This is required to be solid cement sheeting to meet acoustic requirements.

**The performance criterion (P1) is met.**

<p><b>A2</b></p> <p>Common boundary fences with a property in a General Residential Zone or Inner Residential Zone, if not within 4.5m of a frontage, must:</p> <p>(a) have a height above existing ground level of not more than 2.1m; and</p> <p>(b) not contain barbed wire.<sup>2</sup></p> <p><sup>2</sup> An exemption applies for fences in this zone – see Table 4.6.</p>	<p><b>P2</b></p> <p>Common boundary fences with a property in a General Residential Zone or Inner Residential Zone, if not within 4.5m of a frontage, must not cause an unreasonable loss of residential amenity, having regard to:</p> <p>(a) their height, design, location and extent; and</p> <p>(b) the proposed materials and construction.</p>
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#### Planner Response

The site has no boundaries that are shared with the General Residential or Inner Residential zone. The boundary fences not within 4.5 m of the frontage also meet the exemption under Clause 4.6.3 of the planning scheme.

**The acceptable solution (A2) is met.**

### 4.4.4 Outdoor storage areas

#### Clause 15.4.5 Outdoor storage areas

<p><b>A1</b></p> <p>Outdoor storage areas, excluding for the display of goods for sale, must not be visible from any road or public open space adjoining the site.</p>	<p><b>P1</b></p> <p>Outdoor storage areas, excluding for the display of goods for sale, must be located, treated or screened to not cause an unreasonable loss of visual amenity.</p>
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#### Planner Response

Outdoor bin storage is proposed on the northern side of the site. This area will be shielded from view by a 1.8 m high colorbond fence and therefore will not be visible from the road adjoining the site.

**The acceptable solution (A1) is met.**

## 5 Code assessment

The relevant planning scheme codes and specific area plans against which the proposal requires consideration are:

- Parking and Sustainable Transport Code
- Road and Railway Asset Code
- Flood-Prone Areas Code
- Safeguarding of Airports Code
- Glebe Hill Neighbourhood Centre Specific Area Plan

### 5.1 Parking and sustainable transport code

The Parking and Sustainable Transport Code applies to all proposed use and development, with limited exception. Table 2 provides a summary of the applicable use and development standards for the proposal. An assessment against the applicable standards is provided in the sections following Table 2.

Table 2: Applicable standards in the Parking and Sustainable Transport Code

Clause	Applicability
<b>Use standards</b>	
Clause C2.5.1 Car parking numbers	Applicable.
Clause C2.5.2 Bicycle parking numbers	Applicable.
Clause C2.5.3 Motorcycle parking numbers	Applicable.
Clause C2.5.4 Loading bays	Not applicable. Proposed use is Educational and Occasional Care.
Clause C2.5.5 Number of car parking spaces within the General Residential Zone and Inner Residential Zone	Not applicable. Site is in the General Business Zone
<b>Development standards</b>	
Clause C2.6.1 Construction of parking areas	Applicable.
Clauses C2.6.2 Design and layout of parking areas	Applicable.
Clauses C2.6.3 Number of accesses for vehicles	Applicable.
Clause C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone	Applicable.
Clause C2.6.5 Pedestrian access	Applicable.
Clause C2.6.6 Loading bays	Not applicable. No loading bays are required or proposed.
Clause C2.6.7 Bicycle parking and storage facilities within the General Business Zone and Central Business Zone	Applicable.
Clause C2.6.8 Siting of parking and turning areas	Applicable.
<b>Parking precinct plan standards</b>	
Parking precinct plan clauses	Not applicable. No parking precinct plans apply to the site.

## 5.1.1 Car parking numbers

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C2.5.1 Car parking numbers</b>	
<p><b>A1</b></p> <p>The number of on-site car parking spaces must be no less than the number specified in Table 2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if:</p> <ul style="list-style-type: none"> <li>(a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;</li> <li>(b) the site is contained within a parking precinct plan and subject to Clause C2.7;</li> <li>(c) the site is subject to Clause C2.5.5; or</li> <li>(d) it relates to an intensification of an existing use or development or a change of use where: <ul style="list-style-type: none"> <li>(i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or</li> <li>(ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows: <math display="block">N = A + (C - B)</math> <p>N = Number of on-site car parking spaces required</p> <p>A = Number of existing on site car parking spaces</p> <p>B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1</p> <p>C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.</p> </li> </ul> </li> </ul>	<p><b>P1.1</b></p> <p>The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> <li>(a) the availability of off-street public car parking spaces within reasonable walking distance of the site;</li> <li>(b) the ability of multiple users to share spaces because of: <ul style="list-style-type: none"> <li>(i) variations in car parking demand over time; or</li> <li>(ii) efficiencies gained by consolidation of car parking spaces;</li> </ul> </li> <li>(c) the availability and frequency of public transport within reasonable walking distance of the site;</li> <li>(d) the availability and frequency of other transport alternatives;</li> <li>(e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping;</li> <li>(f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity;</li> <li>(g) the effect on streetscape; and</li> <li>(h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development.</li> </ul> <p><b>P1.2</b></p> <p>The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> <li>(a) the nature and intensity of the use and car parking required;</li> <li>(b) the size of the dwelling and the number of bedrooms; and</li> <li>(c) the pattern of parking in the surrounding area.</li> <li>(d)</li> </ul>

#### Planner Response

The proposal includes 24 parking spaces. This exceeds the minimum requirements for the use. Further detail is provided in the Traffic Impact Assessment provided in Appendix E.

**The acceptable solution (A1) is met.**

## 5.1.2 Bicycle parking numbers

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C2.5.2 Bicycle parking numbers</b>	
<p><b>A1</b></p> <p>Bicycle parking spaces must:</p> <ul style="list-style-type: none"> <li>(a) be provided on the site or within 50m of the site; and</li> </ul>	<p><b>P1</b></p> <p>Bicycle parking spaces must be provided to meet the reasonable needs of the use, having regard to:</p>

(b) be no less than the number specified in Table C2.1.	(a) the likely number of users of the site and their opportunities and likely need to travel by bicycle; and (b) the availability and accessibility of existing and any planned parking facilities for bicycles in the surrounding area.
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#### Planner Response

Educational and occasional care for a childcare centre requires 1 bicycle space per five employees. Based on a total of 19 staff members, 4 bicycle parking spaces are required. The proposal includes 4 bicycle parking spaces.

**The acceptable solution (A1) is met.**

### 5.1.3 Motorcycle parking numbers

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C2.5.3 Number of motorcycle spaces</b>	
<b>A1</b> The number of on-site motorcycle parking spaces for all uses must: <ul style="list-style-type: none"> <li>(a) be no less than the number specified in Table C2.4; and</li> <li>(b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle parking spaces is maintained.</li> </ul>	<b>P1</b> Motorcycle parking spaces for all uses must be provided to meet the reasonable needs of the use, having regard to: <ul style="list-style-type: none"> <li>(a) the nature of the proposed use and development;</li> <li>(b) the topography of the site;</li> <li>(c) the location of existing buildings on the site;</li> <li>(d) any constraints imposed by existing development; and</li> <li>(e) the availability and accessibility of motorcycle parking spaces on the street or in the surrounding area.</li> </ul>

#### Planner Response

There is no requirement for motorcycle parking spaces where fewer than 20 car parking spaces are required. The proposed use requires 19 car parking spaces; therefore, no motorcycle parking spaces are required.

**The acceptable solution (A1) is met.**

### 5.1.4 Construction of parking areas

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C2.6.1 Construction of parking area</b>	
<b>A1</b> All parking, access ways, manoeuvring and circulation spaces must: <ul style="list-style-type: none"> <li>(a) be constructed with a durable all weather pavement;</li> <li>(b) be drained to the public stormwater system, or contain stormwater on the site; and</li> <li>(c) excluding all uses in the Rural Zone, Agriculture Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.</li> </ul>	<b>P1</b> All parking, access ways, manoeuvring and circulation spaces must be readily identifiable and constructed so that they are useable in all weather conditions, having regard to: <ul style="list-style-type: none"> <li>(a) the nature of the use;</li> <li>(b) the topography of the land;</li> <li>(c) the drainage system available;</li> <li>(d) the likelihood of transporting sediment or debris from the site onto a road or public place;</li> <li>(e) the likelihood of generating dust; and</li> <li>(f) the nature of the proposed surfacing.</li> </ul>

#### Planner Response

The proposed parking and access way will be constructed with a durable all weather pavement and be drained to the public stormwater system.

**The acceptable solution (A1) is met.**

## 5.1.5 Design and layout of parking areas

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clauses C2.6.2 Design and layout of parking areas</b>	
<b>A1.1</b> Parking, access ways, manoeuvring and circulation spaces must either: (a) comply with the following: (i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6; (ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces; (iii) have an access width not less than the requirements in Table C2.2; (iv) have car parking space dimensions which satisfy the requirements in Table C2.3; (v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces; (vi) have a vertical clearance of not less than 2.1m above the parking surface level; and (vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or (b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6.	<b>P1</b> All parking, access ways, manoeuvring and circulation spaces must be designed and readily identifiable to provide convenient, safe and efficient parking, having regard to: (a) the characteristics of the site; (b) the proposed slope, dimensions and layout; (c) useability in all weather conditions; (d) vehicle and pedestrian traffic safety; (e) the nature and use of the development; (f) the expected number and type of vehicles; (g) the likely use of the parking areas by persons with a disability; (h) the nature of traffic in the surrounding area; (i) the proposed means of parking delineation; and (j) the provisions of Australian Standard AS 2890.1:2004 - Parking facilities, Part 1: Off-street car parking and AS 2890.2 -2002 Parking facilities, Part 2: Off-street commercial vehicle facilities.
<b>A1.2</b> Parking spaces provided for use by persons with a disability must satisfy the following: (a) be located as close as practicable to the main entry point to the building; (b) be incorporated into the overall car park design; and (c) be designed and constructed in accordance with Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities.	

#### Planner Response

The proposed parking and access has been assessed in the Traffic Impact Assessment in Appendix E. It concludes that the car parking design complies with the acceptable solution.

**The acceptable solution (A1.1) is met.**

There is a requirement to provide one accessible parking space associated with the proposed car park given the number of parking spaces proposed. This space is provided on site near the main entrance to the childcare centre. The accessible space is to be designed and constructed in accordance with *Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities*.

**The acceptable solution (A1.2) is met.**

## 5.1.6 Number of accesses for vehicles

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clauses C2.6.3 Number of accesses for vehicles</b>	
<b>A1</b> The number of accesses provided for each frontage must: <ul style="list-style-type: none"> <li>(a) be no more than 1; or</li> <li>(b) no more than the existing number of accesses, whichever is the greater.</li> </ul>	<b>P1</b> The number of accesses for each frontage must be minimised, having regard to: <ul style="list-style-type: none"> <li>(a) any loss of on-street parking; and</li> <li>(b) pedestrian safety and amenity;</li> <li>(c) traffic safety;</li> <li>(d) residential amenity on adjoining land; and</li> <li>(e) the impact on the streetscape.</li> </ul>

#### Planner Response

The site has one existing access on Commerce Drive that will be used.

**The acceptable solution (A1) is met.**

<b>A2</b> Within the Central Business Zone or in a pedestrian priority street no new access is provided unless an existing access is removed.	<b>P2</b> Within the Central Business Zone or in a pedestrian priority street, any new accesses must: <ul style="list-style-type: none"> <li>(a) not have an adverse impact on:               <ul style="list-style-type: none"> <li>(i) pedestrian safety and amenity; or</li> <li>(ii) traffic safety; and</li> </ul> </li> <li>(b) be compatible with the streetscape.</li> </ul>
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#### Planner Response

The site is not in the Central Business zone or a pedestrian priority street.

**The acceptable solution (A2) is met.**

## 5.1.7 Lighting of parking areas

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>C2.6.4 Lighting of parking areas within the General Business Zone and Central Business Zone</b>	
<b>A1</b> In car parks within the General Business Zone and Central Business Zone, parking and vehicle circulation roads and pedestrian paths serving 5 or more car parking spaces, which are used outside daylight hours, must be provided with lighting in accordance with Clause 3.1 "Basis of Design" and Clause 3.6 "Car Parks" in Australian Standard/New Zealand Standard AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting – Performance and design requirements.	<b>P1</b> In car parks within the General Business Zone and Central Business Zone, parking and vehicle circulation roadways and pedestrian paths, which are used outside daylight hours must be provided with lighting, having regard to: <ul style="list-style-type: none"> <li>(a) enabling easy and efficient use of the area;</li> <li>(b) minimising potential for conflicts involving pedestrians, cyclists and vehicles;</li> <li>(c) minimising opportunities for crime or anti-social behaviour through the creation of concealment spaces;</li> <li>(d) any unreasonable impact on the amenity of adjoining properties through light overspill; and</li> <li>(e) the hours of operation of the use.</li> </ul>



### Planner Response

Lighting in the car parks will be provided in accordance with the applicable Australian Standard.

**The acceptable solution (A1) is met.**

## 5.1.8 Pedestrian access

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C2.6.5 Pedestrian access</b>	
<b>A1.1</b> Uses that require 10 or more car parking spaces must: (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by: (i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or (ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and (b) be signed and line marked at points where pedestrians cross access ways or parking aisles.	<b>P1</b> Safe and convenient pedestrian access must be provided within parking areas, having regard to: (c) the characteristics of the site; (d) the nature of the use; (e) the number of parking spaces; (f) the frequency of vehicle movements; (g) the needs of persons with a disability; (h) the location and number of footpath crossings; (i) vehicle and pedestrian traffic safety; (j) the location of any access ways or parking aisles; and (k) any protective devices proposed for pedestrian safety.
<b>A1.2</b> In parking areas containing accessible car parking spaces for use by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.	

### Planner Response

The proposed use requires 19 car parking spaces. The footpath surrounding the car park is not 1 m in width in all places, therefore the proposal must be assessed against the performance criteria.

An assessment against the performance criteria is provided in the Traffic Impact Assessment in Appendix E. It determines that the development meets the requirements of P1.

**The performance criteria (P1) are satisfied.**

## 5.1.9 Bicycle parking and storage facilities

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C2.6.7 Bicycle parking and storage facilities within the General Business Zone and Central Business Zone</b>	
<b>A1</b> Bicycle parking for uses that require 5 or more bicycle spaces in Table C2.1 must: (a) be accessible from a road, cycle path, bicycle lane, shared path or access way; (b) be located within 50m from an entrance; (c) be visible from the main entrance or otherwise signed; and (d) be available and adequately lit during the times they will be used, in accordance with Table 2.3 of Australian/New Zealand Standard AS/NZS 1158.3.1:	<b>P1</b> Bicycle parking must be provided in a safe, secure and convenient location, having regard to: (a) the accessibility to the site; (b) the characteristics of the site; (c) the nature of the proposed use; (d) the number of employees; (e) the users of the site and the likelihood of travel by bicycle;

2005 Lighting for roads and public spaces - Pedestrian area (Category P) lighting - Performance and design requirements.	<p>(f) the location and visibility of proposed parking for bicycles;</p> <p>(g) whether there are other parking areas on the site; and</p> <p>(h) the opportunity for sharing bicycle parking on nearby sites.</p>
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#### Planner Response

Bicycle parking is required for four bicycles. Bicycle parking is conveniently provided at the front of the site, as shown on the architectural plans.

**The acceptable solution (A1) is met.**

<p><b>A2</b></p> <p>Bicycle parking spaces must:</p> <p>(a) have dimensions not less than:</p> <ul style="list-style-type: none"> <li>(i) 1.7m in length;</li> <li>(ii) 1.2m in height; and</li> <li>(iii) 0.7m in width at the handlebars;</li> </ul> <p>(b) have unobstructed access with a width of not less than 2m and a gradient not steeper than 5% from a road, cycle path, bicycle lane, shared path or access way; and</p> <p>(c) include a rail or hoop to lock a bicycle that satisfies Australian Standard AS 2890.3-2015 Parking facilities - Part 3: Bicycle parking.</p>	<p><b>P2</b></p> <p>Bicycle parking spaces and access must be convenient, safe, secure and efficient to use, having regard to:</p> <ul style="list-style-type: none"> <li>(a) the characteristics of the site;</li> <li>(b) the space available;</li> <li>(c) the safety of cyclists; and</li> <li>(d) the provisions of Australian Standard AS 2890.3-2015 Parking facilities - Part 3: Bicycle parking</li> </ul>
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#### Planner Response

The bicycle parking spaces will be designed to meet the requirements in A2.

**The acceptable solution (A2) is met.**

### 5.1.10 Siting of parking and turning areas

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C2.6.8 Siting of parking and turning areas</b>	
<p><b>A1</b></p> <p>Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas must be located behind the building line of buildings, excluding if a parking area is already provided in front of the building line.</p>	<p><b>P1</b></p> <p>Within an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone or General Business Zone, parking spaces and vehicle turning areas, including garages or covered parking areas, may be located in front of the building line where this is the only practical solution and does not cause an unreasonable loss of amenity to adjoining properties, having regard to:</p> <ul style="list-style-type: none"> <li>(a) topographical or other site constraints;</li> <li>(b) availability of space behind the building line;</li> <li>(c) availability of space for vehicle access to the side or rear of the property;</li> <li>(d) the gradient between the front and the rear of existing or proposed buildings;</li> <li>(e) the length of access or shared access required to service the car parking;</li> <li>(f) the location of the access driveway at least 2.5m from a window of a habitable room of a dwelling;</li> <li>(g) the visual impact of the vehicle parking and access on the site;</li> <li>(h) the streetscape character and amenity;</li> <li>(i) the nature of the zone in which the site is located and its preferred uses; and</li> </ul>

(j) opportunities for passive surveillance of the road.

#### Planner Response

The proposed car park is only partially located behind the building line and cannot meet the acceptable solution. This is due to the front setback requirements under the relevant specific area plan.

The subject site is not adjacent to residential properties, is located in a planned, commercial area; the noise and light emissions from the car park will be minimised by the use of fencing on site. The carpark is setback a minimum of 5.66 m from the title boundary, therefore impacts on public spaces are likely to be minimal. Refer to the Traffic Impact Assessment in Appendix E for further details.

**The performance criteria (P1) are satisfied.**

<b>A2</b> Within the Central Business Zone, on-site parking at ground level adjacent to a frontage must: (a) have no new vehicle accesses, unless an existing access is removed; (b) retain an active street frontage; and (c) not result in parked cars being visible from public places in the adjacent roads	<b>P2</b> Within the Central Business Zone, on-site parking at ground level adjacent to a frontage must be designed to screen the views of cars from public places in the adjacent roads, without blank walls facing onto a road, having regard to: (a) the streetscape; (b) any unreasonable loss of amenity of the occupants of adjoining properties; and (c) maintaining opportunities for active uses on a street frontage in a pedestrian priority street.
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#### Planner Response

The subject site is not located in the Central Business zone.

**The acceptable solution (A2) is met.**

## 5.2 Road and railway assets code

The Road and Railway Assets Code applies to the proposal because the proposal will increase the amount of vehicular traffic using an existing vehicle crossing. Table 3 provides a summary of the applicable use and development standards for the proposal. An assessment against the applicable standards is provided in the sections following Table 3.

Table 3: Applicable standards in the Road and Railway Assets Code

Clause	Applicability
<b>Use standards</b>	
Clause C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction	Applicable.
<b>Development standards for buildings or works</b>	
Clause C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area	Applicable.
<b>Development standards for subdivision</b>	
Subdivision clauses	Not applicable. No subdivision is proposed.

### 5.2.1 Traffic generation

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction</b>	

<p><b>A1.1</b></p> <p>For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:</p> <ul style="list-style-type: none"> <li>(a) a new junction;</li> <li>(b) a new vehicle crossing; or</li> <li>(c) a new level crossing.</li> </ul> <p><b>A1.2</b></p> <p>For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.</p> <p><b>A1.3</b></p> <p>For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.</p> <p><b>A1.4</b></p> <p>Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than:</p> <ul style="list-style-type: none"> <li>(a) the amounts in Table C3.1; or</li> <li>(b) allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road.</li> </ul> <p><b>A1.5</b></p> <p>Vehicular traffic must be able to enter and leave a major road in a forward direction.</p>	<p><b>P1</b></p> <p>Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:</p> <ul style="list-style-type: none"> <li>(a) any increase in traffic caused by the use;</li> <li>(b) the nature of the traffic generated by the use;</li> <li>(c) the nature of the road;</li> <li>(d) the speed limit and traffic flow of the road;</li> <li>(e) any alternative access to a road;</li> <li>(f) the need for the use;</li> <li>(g) any traffic impact assessment; and</li> <li>(h) any advice received from the rail or road authority.</li> </ul>
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#### Planner Response

Commerce Drive is not a category 1 road, therefore A1.1 is not applicable.

No new junction, vehicle crossing, or level crossing is proposed, therefore A1.2 is not applicable.

No new private rail crossing/s are proposed, therefore A1.3 is not applicable.

The acceptable increase in vehicles movements per day for an existing vehicle crossing on a non-major road is 20% or 40 vehicle movements per day for vehicles up to 5.5 m long. It is anticipated that the majority of vehicles access the site will be under 5.5 m long. The increase in vehicle movements per day resulting from the proposal will exceed the acceptable solution based on the number of children (114). Therefore, the performance criteria have to be addressed and a Traffic Impact Assessment has been prepared to demonstrate how the proposal satisfies the requirements (refer to Appendix D).

Vehicular traffic will be able to enter and exit the site in a forward direction, compliant with A1.5.

**The performance criteria (P1) are satisfied.**

## 5.2.2 Habitable buildings for sensitive use

### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area</b>	
<p><b>A1</b></p> <p>Unless within a building area on a sealed plan approved under this planning scheme, habitable buildings for a sensitive use within a road or railway attenuation area, must be:</p> <ul style="list-style-type: none"> <li>(a) within a row of existing habitable buildings for sensitive uses and no closer to the existing or future major road or rail network than the adjoining habitable building;</li> <li>(b) an extension which extends no closer to the existing or future major road or rail network than:</li> </ul>	<p><b>P1</b></p> <p>Habitable buildings for sensitive uses within a road or railway attenuation area, must be sited, designed or screened to minimise adverse effects of noise, vibration, light and air emissions from the existing or future major road or rail network, having regard to:</p> <ul style="list-style-type: none"> <li>(a) the topography of the site;</li> <li>(b) the proposed setback;</li> <li>(c) any buffers created by natural or other features;</li> </ul>

<ul style="list-style-type: none"> <li>(i) the existing habitable building; or</li> <li>(ii) an adjoining habitable building for a sensitive use; or</li> <li>(c) located or designed so that external noise levels are not more than the level in Table C3.2 measured in accordance with Part D of the Noise Measurement Procedures Manual, 2nd edition, July 2008.</li> </ul>	<ul style="list-style-type: none"> <li>(d) the location of existing or proposed buildings on the site;</li> <li>(e) the frequency of use of the rail network;</li> <li>(f) the speed limit and traffic volume of the road;</li> <li>(g) any noise, vibration, light and air emissions from the rail network or road;</li> <li>(h) the nature of the road;</li> <li>(i) the nature of the development;</li> <li>(j) the need for the development;</li> <li>(k) any traffic impact assessment;</li> <li>(l) any mitigating measures proposed;</li> <li>(m) any recommendations from a suitably qualified person for mitigation of noise; and</li> <li>(n) any advice received from the rail or road authority.</li> </ul>
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#### Planner Response

The proposed building is not within a building area on a sealed plan, is not an extension, and is not in an existing row of buildings. The proposed use is a sensitive use. The proposal cannot meet the acceptable solution and must be assessed against the performance criteria.

A traffic impact assessment and noise impact assessment have been prepared to ensure the proposed childcare centre is suitably located on the subject site. Both assessments have provided recommendations to ensure that the building is designed and screened to minimise adverse effects of emissions from Rokeby Road, and these have been incorporated into the design. Refer to Appendix E and Appendix F.

**The performance criteria (P1) are satisfied.**

## 5.3 Flood-prone hazard areas code

The site includes flood-prone hazard areas; however, no development is proposed within the flood-prone hazard area. Therefore, the Flood-Prone Hazard Areas Code is not applicable.

## 5.4 Safeguarding of airports code

The Safeguarding of Airports Code applies to the proposal because the site is located within an airport obstacle limitation area. The proposed development is exempt from the requirements of the code as the maximum height of the development is far below the specified 147 m AHD height for the obstacle limitation area.

## 5.5 Glebe Hill neighbourhood centre specific area plan

The Glebe Hill Neighbourhood Centre Specific Area Plan (the specific area plan) applies to the site as it is located within the overlay for the specific area plan. The provisions of the specific area plan apply in addition to the provisions of the General Business zone.

Educational and Occasional Care is a discretionary use in the specific area plan.

Table 3 provides a summary of the applicable use and development standards for the proposal. An assessment against the applicable standards is provided in the sections following Table 3.

Table 4: Applicable standards in the Glebe Hill Neighbourhood Centre Specific Area Plan

Clause	Applicability
<b>Use standards</b>	
Clause CLA-S13.6.1 Precinct objectives	Applicable.
<b>Development standards for buildings or works</b>	
Clause CLA-S13.7.1 Materials and design	Applicable.

Clause	Applicability
Clause CLA-S13.7.2 Siting and scale	Applicable.
Clause CLA-S13.7.3 Staging	Applicable.
Clause CLA-S13.7.4 Public art	Applicable.
<b>Development standards for subdivision</b>	
Subdivision clauses	Not applicable. No subdivision is proposed.

### 5.5.1 Precinct objectives

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause CLA-S13.6.1 Precinct objectives</b>	
<b>A1</b> Use must be a Permitted use.	<b>P1</b> Use must demonstrate it is consistent with the specific area plan Purpose statements.

#### Planner Response

Educational and Occasional Care is a discretionary use in the specific area plan. Therefore, the proposal cannot meet the acceptable solution and the performance criteria have been addressed.

*CLA-S13.1.1 To provide for the staged development of a Neighbourhood Centre at Glebe Hill that will serve the residential development in the Glebe Hill Estate and the adjoining Rokeby/Droughty Point growth corridor.*

The proposed development will allow for staged development of the neighbourhood centre and serves the residential community.

*CLA-S13.1.2 To ensure that the Neighbourhood Centre comprises a supermarket and supporting retail and food services, and complementary commercial and community development.*

The proposed development provides complementary community development by providing childcare services. The development will not interfere with the supermarket and is considered a supporting use.

*CLA-S13.1.3 To provide for Neighbourhood Centre that is of a scale consistent with surrounding residential development that does not adversely impact the visual and general amenity.*

Given the scale of lot sizes in the General Business zone, and the scale of lot sizes in the surrounding General Residential zone, it is difficult to compare based on scale. Additionally, as one of the first developments within the General Business zone, the perceived impact on visual and general amenity may be greater than if there was existing commercial and retail development. The proposed development has therefore been considered based on what a fully developed neighbourhood centre may look like.

The proposed development will have a footprint of around 60% based on the area of the building (32%) and the parking areas (around 26%). The height of the development is single storey, and no greater than 5.3m, being of a height consistent with dwelling scale that is found in the surrounding area. The childcare centre is set back from the road to minimise noise emissions, and landscaping is proposed surrounding the site to soften built form. It is considered that the architecturally design building will fit appropriately into the area, and not adversely impact on visual and general amenity.

*CLA-S13.1.4 To ensure that function of the Glebe Hill Neighbourhood Activity Centre serves local food retailing needs supported by additional uses providing community or public benefit to the residents in the Glebe Hill Estate and the adjoining Rokeby/Droughty Point growth corridor.*

The proposed use is considered to support the activity centre and provide a service that is of community and public benefit.

The proposed use is considered to be consistent with the specific area plan Purpose statements.

**The performance criteria (P1) are satisfied.**

### 5.5.2 Materials and design

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause CLA-S13.7.1 Materials and design</b>	
<b>A1</b> Development does not involve: (a) the construction of a new building; or (b) external alterations to an existing building.	<b>P1</b> (a) external finishes of buildings (walls, roofing and windows) are to be compatible with existing residential development within the vicinity; (b) walls constructed of face brick, rendered masonry or similar, should borrow texture and colours from existing residential development in the vicinity; (c) tilt-up concrete slabs and similar large scale wall construction must include sufficient detail and relief to enable a scale of structure that is compatible with residential development in the vicinity; (d) to provide for domestic scale elements within its residential setting, roof form must: (i) be low pitched gable, hipped, skillion or a combination of such forms; and (ii) large expanses of planar roof forms in view from adjacent residential areas must be mitigated through suitable architectural design and building elements, building orientation, or landscaping; and (e) roof top infrastructure is to be suitably screened, details of which are to be included on the relevant elevations.

### Planner Response

The development involves the construction of a new building and therefore cannot meet the acceptable solution.

The external finishes of the building are proposed to be a combination of brick, white render, pale grey weatherboard and pale wood wall treatments, with colorbond corrugated roofing, as seen in surrounding residential development (refer to photos in Appendix C).

The elevations of the building include varied finishes, textures, and colours, breaking up the extent of the facades and providing sufficient detail and relief (refer to elevation plans in Appendix C). The design is considered compatible with the residential design in the vicinity.

The proposed roof is a low pitched gabled roof incorporating varied pitch and eaves.

No rooftop infrastructure is proposed.

**The performance criteria (P1) are satisfied.**

<b>A2</b> A landscaping treatment must be provided and incorporate the following features as applicable to development in Precinct 1, 2 and 3: (a) an evergreen tree screen between the rear of property boundaries in Hance Road and Norfolk Drive, and the proposed new slip lane off Rokeby Road/South Arm Highway; (b) an avenue of evergreen street trees within the road reserve on the southern side of the proposed new slip lane; (c) an avenue of deciduous street trees and lower non-deciduous understorey; (d) plantings within the 15m setback from boundaries to Rokeby Road/South Arm Highway; (e) street trees are to be protected with metal guards until maturity; and (f) on-site parking for individual developments must include shade trees which are to be protected by metal guards. Such details are to be provided as part of a landscape plan for individual developments.	<b>P2</b> For new development a landscaping treatment must be provided that: (a) enhances the appearance of development and provide for a high quality streetscape and visual amenity within the Glebe Hill Neighbourhood Centre; (b) enhances amenity and interest having regard to species, diversity, a range of plant height, forms, density and maintenance requirements; (c) seeks to establish avenue type plantings adjacent to the road boundaries; and (d) does not create concealed entrapment spaces.
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### Planner Response

The site is located in Precinct 3. Given a 15m setback from Rokeby Road is not provided, the proposal is assessed against the performance criteria.

A row of lemon lime willow myrtle trees is proposed along the boundary adjoining the Rokeby Road road reserve. This species was chosen on advice from Clarence City Council. Ornamental pear trees are provided in the car parking area. Details are provided on the landscaping plans in Appendix C.

The proposed trees within the site will enhance the appearance of the development and contribute to a high quality streetscape and high visual amenity. Plantings visible from the streetscape include ornamental pear trees and low shrubs providing a diversity of species, and plant height, form, and density.

The willow myrtle trees provide an avenue type planting. Council has advised that they will grow to a mature height of 5m and width of 4m. As demonstrated on the architectural plans, the proposed minimum setback of 5m allows for the trees to fully mature and create an avenue. In places, the setback is greater than 5m, as the building form has been staggered to create visual interest for passers by and create a more exciting outdoor play area for the children attending the childcare centre.

The layout of planting in the frontage reduces the potential for concealment.

**The performance criteria (P2) are satisfied.**

### 5.5.3 Siting and scale

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause CLA-S13.7.2 Siting and scale</b>	
<b>A1</b> Development must have a setback from boundaries to Rokeby Road/South Arm Highway of not less than 15m.	<b>P1</b> Development is set back sufficiently to provide and maintain a continuous avenue of trees and screen plantings between Rokeby Road/South Arm Highway and development.

### Planner Response

The proposed development has a minimum setback of 5 m from the boundary to Rokeby Road and therefore cannot comply with the acceptable solution.

The proposal includes a setback that is sufficient in width (5m) to allow for a continuous avenue of lemon lime willow myrtle trees along the south boundary adjoining the road reserve for Rokeby Road as shown in Appendix C. The trees grow to a mature width of 4 m; therefore the proposed setback sufficiently allows for the trees to grow to full maturity.

**The performance criteria (P1) are satisfied.**

<b>A2</b> (a) Development must have a setback from side boundaries of not less than 4m. (b) Development must have a setback to the slip lane, or future slip lane, of not less than 9m.	<b>P2</b> No Performance Criterion.
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### Planner Response

The CLA-S13.7.2 Siting and scale clause of the Local Provisions Schedule is in addition to the Clause 15.4.2 Setbacks of the General Business zone of the State Planning Provisions. The setbacks clause in the General Business zone relates to the setback of buildings. Therefore, it is purported that the application of clause CLA-S13.7.2 relates to the siting and scale of buildings as opposed to the siting and scale of all development. Were clause CLA-S13.7.2 to apply to all development, then the landscaping requirements under the acceptable solution of Clause CLA-S13.7.1 Materials and design would not be possible. Based on the assumption that development relates only to buildings for the application of this clause, the proposed building is setback a minimum of 4.68 m from a side boundary and therefore complies with the acceptable solution. Similarly, a minimum front setback of 9 m is provided to the slip lane, meeting A2.

**The acceptable solution (A2) is met.**

<b>A3</b> Building height and signage must be not more than 8.5m.	<b>P3</b> Building height must: (a) be consistent with the purpose and objectives of this specific area plan;
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	<ul style="list-style-type: none"> <li>(b) is compatible with the scale of adjoining residential development within proximity to the site;</li> <li>(c) not unreasonably overshadow public spaces; and</li> <li>(d) provide a transition in height between adjoining buildings, if appropriate.</li> </ul>
--	--

#### Planner Response

The proposed building height is 5.3 m. There is no signage proposed as part of this application.

**The acceptable solution (A3) is met.**

### 5.5.4 Staging

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause CLA-S13.7.3 Staging</b>	
<b>A1</b> (a) Development is associated with Precinct 1; or (b) development in Precinct 2 provided that construction works associated with the development of the supermarket in Precinct 1 has commenced; or (c) development in Precinct 3 provided that construction works associated with the development of Precinct 2 has commenced.	<b>P1</b> Precincts 2 and 3 may be developed in the reverse order where it can be demonstrated that the proposal is consistent with the Purpose of this specific area plan.

#### Planner Response

The proposed development is located in Precinct 3. Development in Precinct 1 has commenced. Development in Precinct 2 has not commenced. Therefore, the proposal cannot meet the acceptable solution as it will occur in reverse order.

The proposal is considered to meet the purpose of the specific area plan as detailed in section 5.5.1 above. Given the proposal provides an important community facility for the local area it is considered appropriate that precincts 2 and 3 be developed in reverse order.

**The performance criteria (P1) are satisfied.**

### 5.5.5 Public art

#### PLANNING SCHEME REQUIREMENT

Acceptable Solutions	Performance Criteria
<b>Clause CLA-S13.7.4 Public art</b>	
<b>A1</b> Development is not for a major development.	<b>P1</b> That a major development contributes to the amenity of the site and nearby public places by the installation of public art works having regard to: <ul style="list-style-type: none"> <li>(a) the proximity of the proposed public art works to the major development and a public place;</li> <li>(b) the size, scale and design of the proposed major development;</li> <li>(c) the design of the proposed public art works, including their intended enhancement of the streetscape, or the cultural, environmental or built values of the location;</li> <li>(d) the visibility and accessibility of the proposed public art works to the public; and</li> <li>(e) the need for public art and any existing public art on or near the site.</li> </ul>

---

### **Planner Response**

The proposed development is considered a 'major development' as defined under the specific area plan. The proponent is committed to providing public artwork as part of the development. The design of the artwork is to be provided to Council's satisfaction, pending consultation with Council regarding what might be suitable for this site. It is proposed that this item be conditioned as part of the planning permit to facilitate the consultation process with Council.

**The performance criteria (P1) are satisfied.**

---

## 6 Conclusion

The proposal seeks planning approval for the use and development of a childcare centre at 21 Commerce Drive, Howrah. This report identifies that the proposal is subject to the provisions of the *Tasmanian Planning Scheme - Clarence*. In particular, the zone purpose, use, and development standards in the General Business zone. The proposal also requires assessment against the relevant Codes and the Glebe Hill Neighbourhood Centre Specific Area Plan.

An assessment against all relevant standards has been outlined in this report, including its appendices, and is summarised in Table 5 below. The assessment has demonstrated that even where the acceptable solution is not met, the performance criterion is achieved; accordingly, the proposal should be approved.

Table 5: Summary of relevant standards and whether the proposal meets the acceptable solution or performance criteria

Clause	Standard	Acceptable solution or performance criteria
<b>General Business Zone</b>		
15.3.1	All uses	Relies on P1
		Complies with A2
		Complies with A3
15.4.2	Setbacks	Relies on P1
		Complies with A2
		Complies with A3
15.4.4	Fencing	Relies on P1
		Complies with A2
15.4.5	Outdoor storage areas	Complies with A1
<b>Parking and Access Code</b>		
C2.5.1	Car parking numbers	Complies with A1
C2.5.2	Bicycle parking numbers	Complies with A1
C2.5.3	Number of motorcycle spaces	Complies with A1
C2.6.1	Construction of parking area	Complies with A1
C2.6.2	Design and layout of parking areas	Complies with A1.1 and A1.2
C2.6.3	Number of accesses for vehicles	Complies with A1
		Complies with A2
C2.6.4	Lighting of parking areas within the General Business Zone and Central Business Zone	Complies with A1
C2.6.5	Pedestrian access	Relies on P1
C2.6.7	Bicycle parking and storage facilities within the General Business Zone and Central Business Zone	Complies with A1
		Complies with A2
C2.6.8	Siting of parking and turning areas	Relies on P1
		Complies with A2

Clause	Standard	Acceptable solution or performance criteria
<b>Road and Railway Assets Code</b>		
C3.5.1	Traffic generation at a vehicle crossing, level crossing or new junction	Relies on P1
C3.6.1	Habitable buildings for sensitive uses within a road or railway attenuation area	Relies on P1
<b>Glebe Hill Neighbourhood Centre Specific Area Plan</b>		
CLA-S13.6.1	Precinct objectives	Relies on P1
CLA-S13.7.1	Material and design	Relies on P1
		Relies on P2
CLA-S13.7.2	Siting and scale	Relies on P1
		Complies with A2
		Complies with A3
CLA-S13.7.3	Staging	Relies on P1
CLA-S13.7.4	Public art	Relies on P1

# Appendix A   Application form

# Appendix B    Certificate of Title

## Appendix C Architectural plans

## **Appendix D   Concept services report and Civil Engineering drawings**



## Appendix E Traffic impact assessment

## **Appendix F    Acoustic impact assessment**





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# HOWRAH CHILD CARE CENTRE

21 COMMERCE DRIVE, HOWRAH, TAS 7018

## PLANNING ISSUE

16 FEBRUARY 2023

### ARCHITECTURAL DRAWING SCHEDULE

DA SHEET LIST				
Sheet Number	Sheet Name	Revision	Revision Date	
DA01	COVER SHEET	3	16/02/24	
DA02	EXISTING CONDITIONS	2	19/12/23	
DA03	CONTEXT & SITE ANALYSIS	2	19/12/23	
DA04	SITE PLAN	4	16/02/24	
DA05	FLOOR PLAN	4	16/02/24	
DA06	ROOF PLAN	3	05/02/24	
DA07	ELEVATIONS	4	16/02/24	
DA07A	FENCE ELEVATIONS	2	16/02/24	
DA08	SECTIONS	4	16/02/24	
DA09	3D IMAGES	4	16/02/24	
DA10	LANDSCAPE PLAN	4	16/02/24	

## DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL

Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23
3	DA SET UPDATES	16/02/24

**PRINTING NOTE**  
THIS DRAWING IS TO BE PRINTED IN COLOUR.  
THIS NOTE IS RED.

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COVER SHEET

Scale  
Date 12/12/23  
BF Job/Site No. 2023095  
Dwg No. **DA01** Rev: **3** A1 SHEET



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EXISTING CONDITIONS

Scale 1 : 150  
Date 12/12/23  
BF Job/Site No. 2023095

Dwg No. DA02 Rev: 2 A1 SHEET

COMMERCE

DRIVE

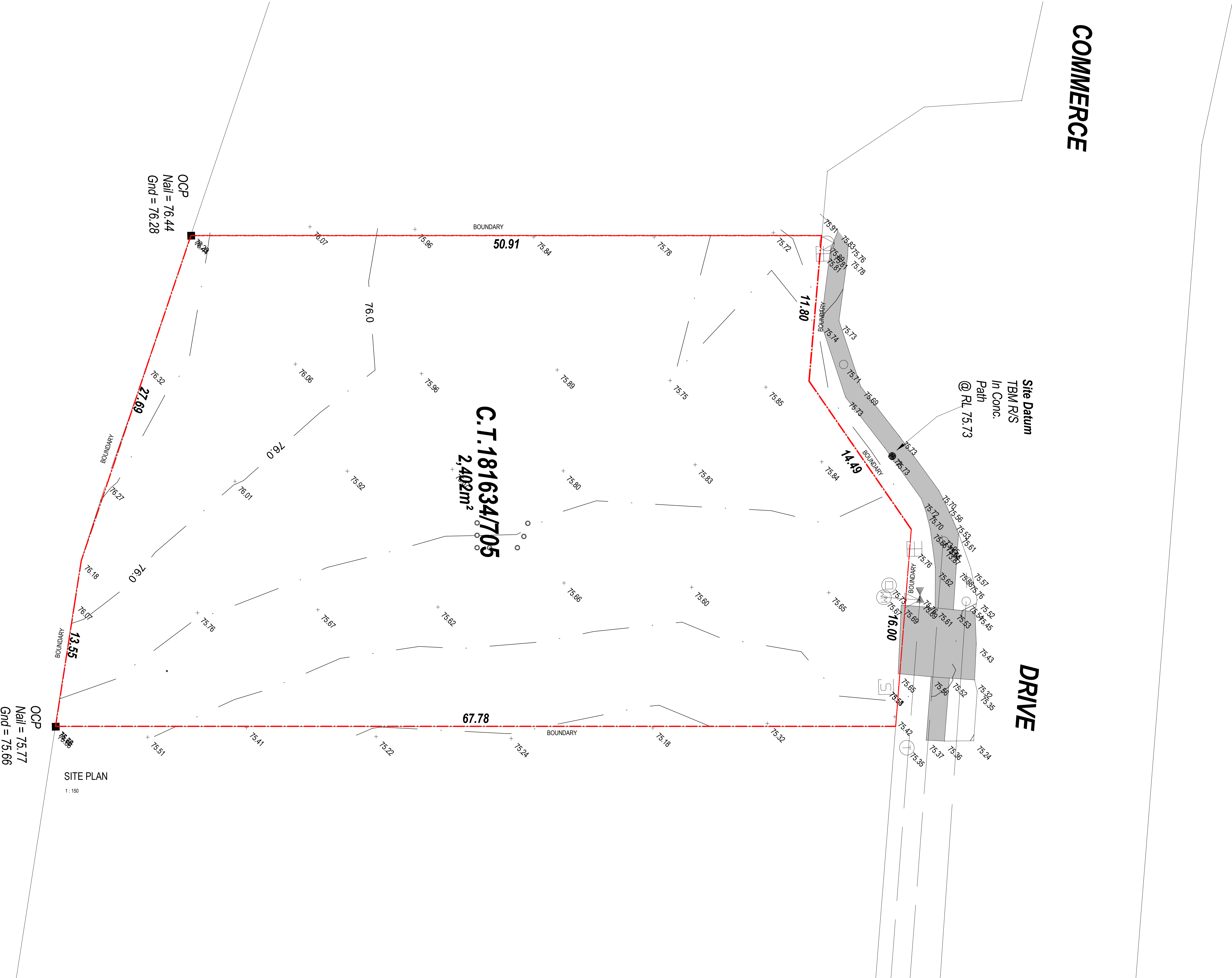
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TBM R/S  
In Conc.  
Path  
@ RL 75.73

C.T.181634/705  
2,402m<sup>2</sup>

SITE PLAN  
1 : 150

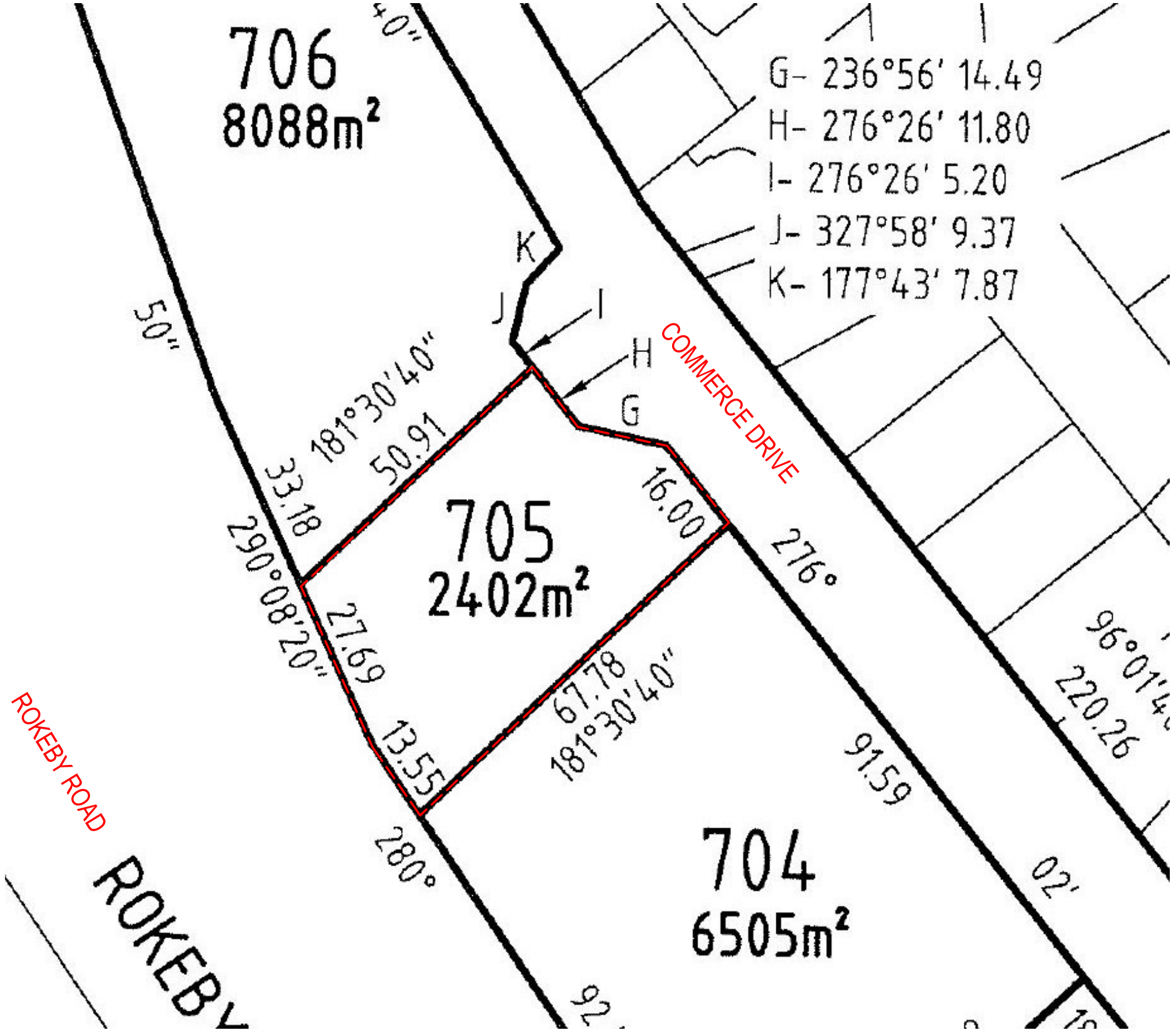
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Gnd = 76.28

OCP  
Nail = 75.77  
Gnd = 75.66





Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23



GLEBE HILL VILLAGE



GLEBE HILL VILLAGE



GLEBE HILL FAMILY PRACTICE

SURROUNDING RESIDENTIAL DEVELOPMENT



26 & 28 HANCE RD



34, 36 & 38 HANCE RD



42 & 44 HANCE RD



49 & 51 HANCE RD



68 GLEBE HILL RD



104 GOODWINS RD

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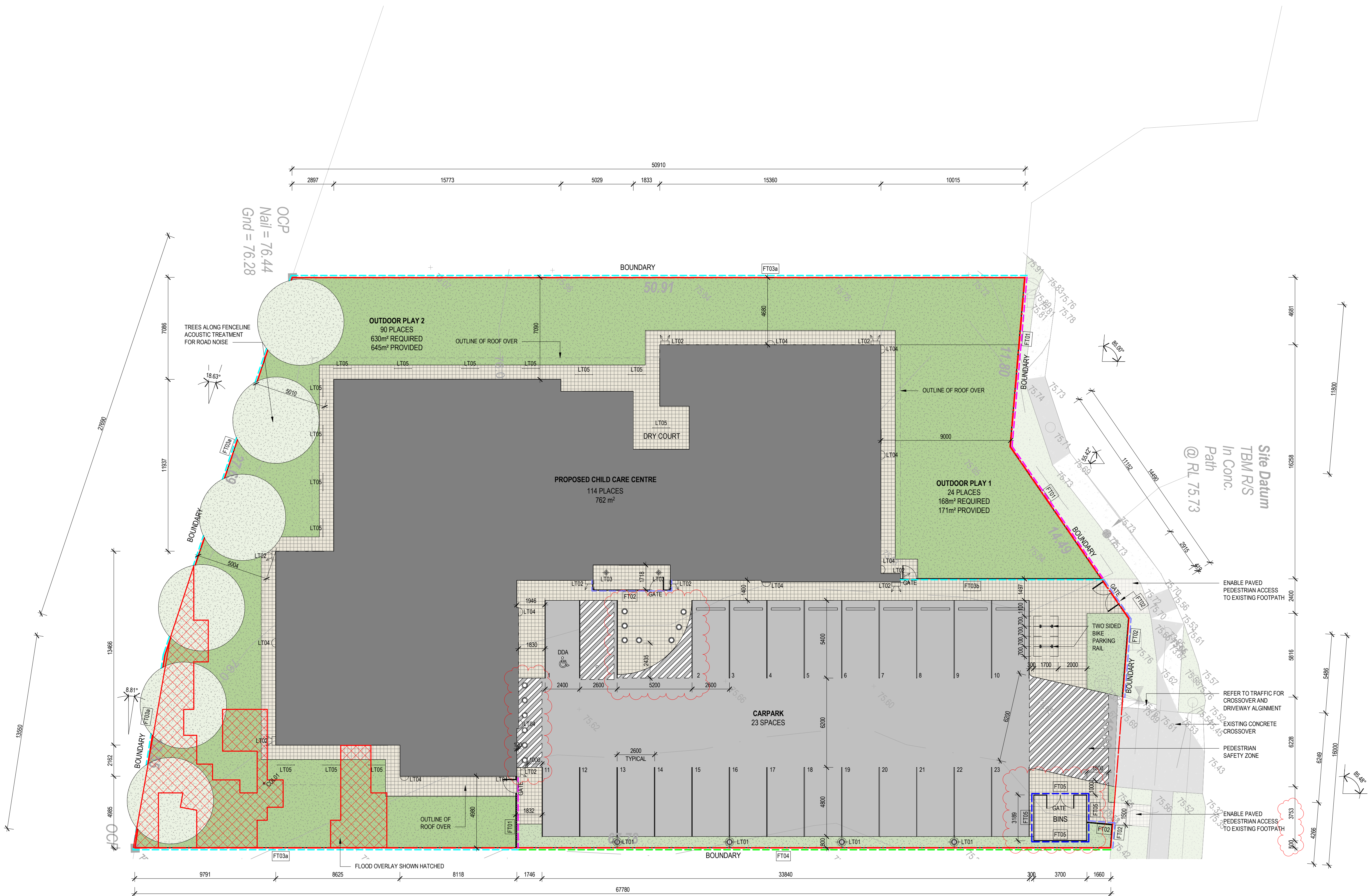
CONTEXT & SITE ANALYSIS



## DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL

Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23
3	DA SET UPDATES	05/02/24
4	DA SET UPDATES	16/02/24



## SITE PLAN

1 : 150

## EXTERNAL LIGHTING LEGEND

- LT01 BOLLARD LIGHT
- LT02 FLOODLIGHT
- LT03 DOWNLIGHT
- LT04 WALL LIGHT
- LT05 BATTEN LIGHT

NOTE: LIGHTING TO BE CONFIRMED BY ELECTRICAL ENGINEER.

## FENCE LEGEND

- FT01 FENCE TYPE 01 DEFENDER PICKET STYLE FENCE 1800 MINIMUM HIGH COLOUR: WHITE
- FT02 FENCE TYPE 02 DEFENDER PICKET STYLE FENCE 1500 MINIMUM HIGH COLOUR: WHITE
- FT03 FENCE TYPE 03 12MM FIXED CEMENT SHEET ACOUSTIC FENCE 1800-2000 MINIMUM HIGH (REFER ACOUSTIC REPORT)
- FT03a COLOUR: MONUMENT
- FT03b COLOUR: WHITE
- FT04 FENCE TYPE 04 COLOURBOND FENCE 1800 MINIMUM HIGH COLOUR: MONUMENT
- FT05 FENCE TYPE 05 TIMBER FENCE 1800 MINIMUM HIGH

NOTE: REFER REPORT PREPARED BY ACOUSTIC ENGINEER.

## NUMBER OF PLACES

SITE AREA	2402m²
SITE AREA PER PLACE	21m²
BUILDING AREA	762m²
BUILDING AREA PER PLACE	6.6m²
OUTDOOR PLAY AREA	815m²
NUMBER OF CARPARKS	23

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## SITE PLAN

Scale As indicated  
Date 12/12/23  
BF Job/Site No. 2023095

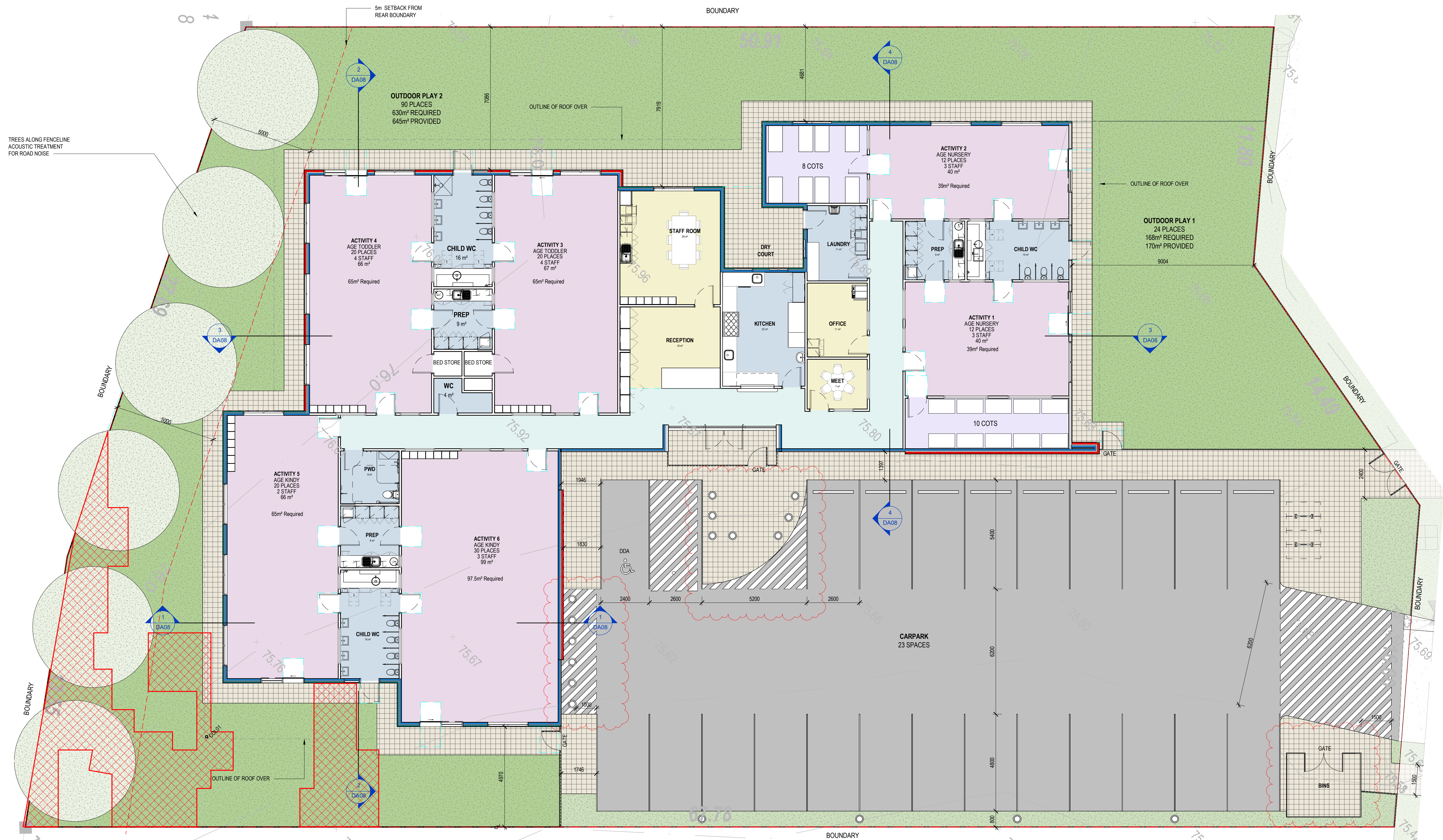
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ISSUED FOR DEVELOPMENT APPROVAL

Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23
3	DA SET UPDATES	05/02/24
4	DA SET UPDATES	16/02/24



FLOOR PLAN

1 : 100

NUMBER OF PLACES 114

SITE AREA 2402m²

SITE AREA PER PLACE 21m²

BUILDING AREA 762m²

BUILDING AREA PER PLACE 6.6m²

OUTDOOR PLAY AREA 815m²

NUMBER OF CARPARKS 23

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FLOOR PLAN

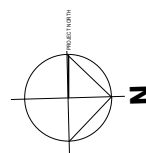
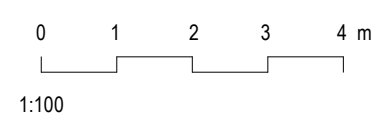
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Date 12/12/23

BF Job/Site No. 2023095

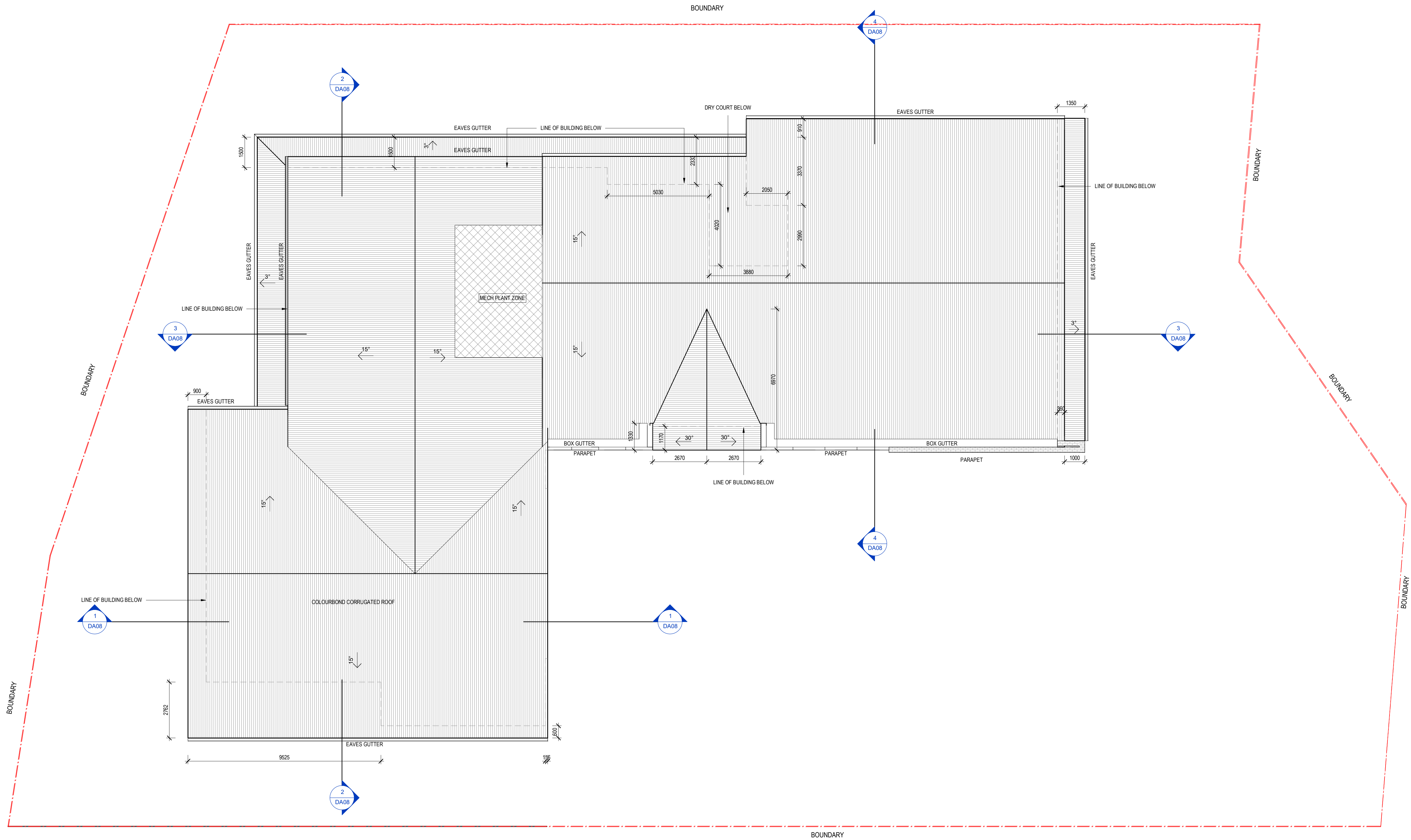
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Rev: 4 A1 SHEET





Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23
3	DA SET UPDATES	05/02/24



ROOF PLAN

1:100

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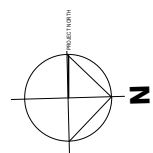
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ROOF PLAN

0 1 2 3 4 m  
1:100

Scale 1:100  
Date 12/12/23  
BF Job/Site No. 2023095

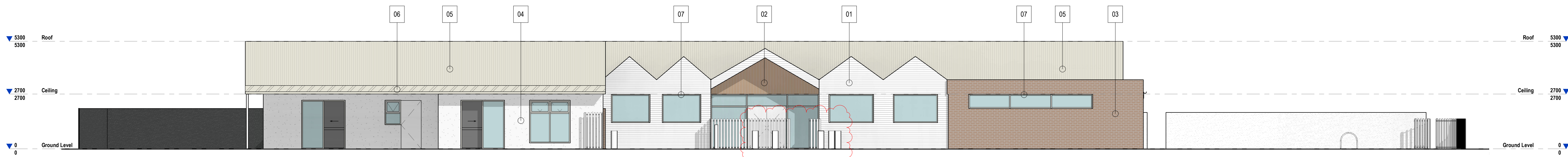
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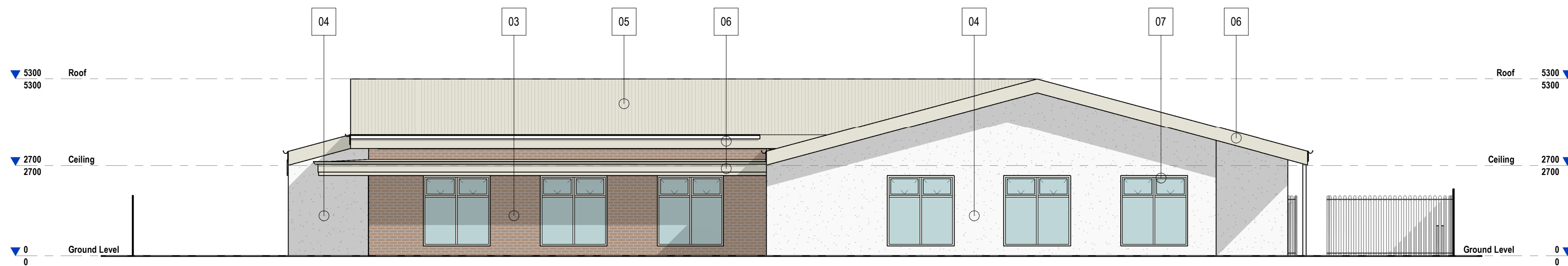
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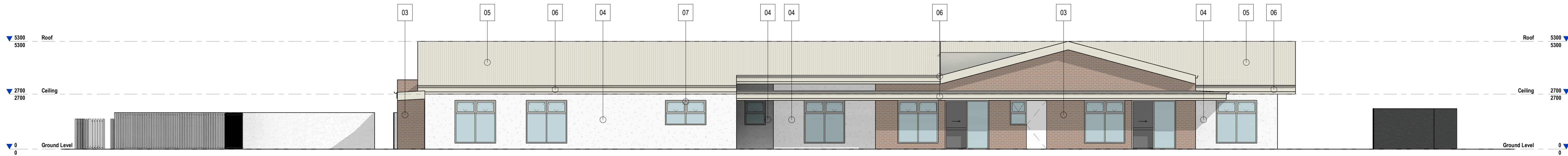
Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23
3	DA SET UPDATES	05/02/24
4	DA SET UPDATES	16/02/24



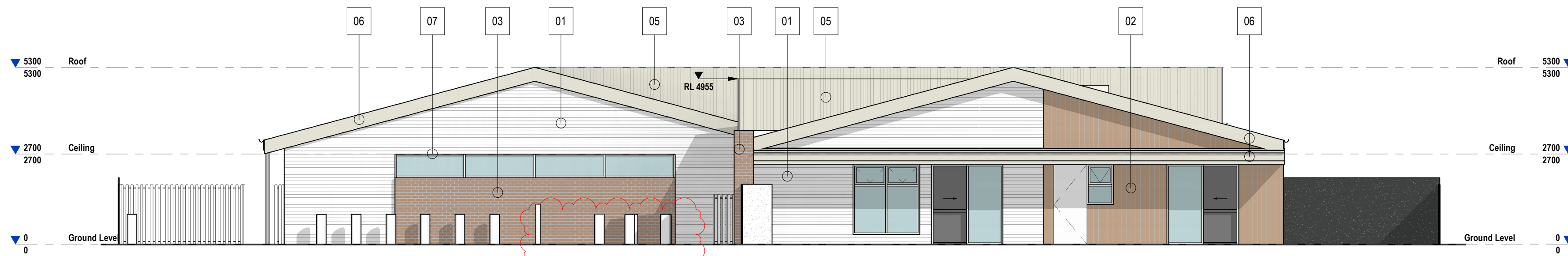
EAST ELEVATION



SOUTH ELEVATION



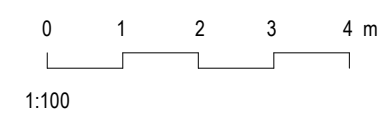
WEST ELEVATION



NORTH ELEVATION

## MATERIALS LEGEND

01 HORIZONTAL WEATHERBOARD CLADDING PAINTED GREY	02 TIMBER LOOK CFC	03 BRICK RANGE - YARRA COLOUR - TOORAK (AUSTRAL BRICKS)	04 RENDERED HEBEL PAINTED WHITE	05 COLOURBOND METAL ROOF IN SURFMIST	06 FASCIA & GUTTER PAINTED SURFMIST	07 ALUMINIUM DOOR & WINDOW FRAMES - WHITE



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## ELEVATIONS

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Date 12/12/23  
BF Job/Site No. 2023095

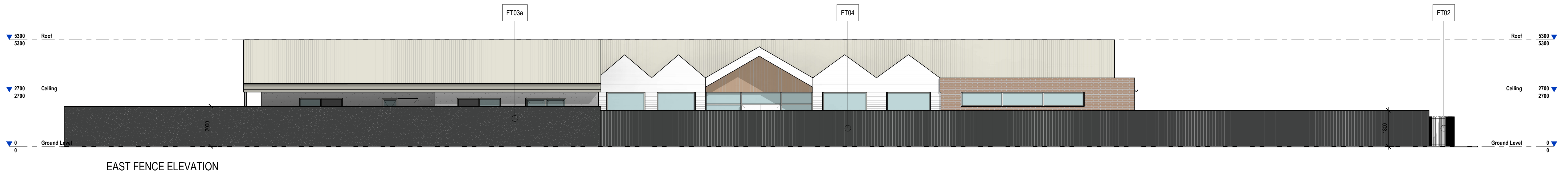
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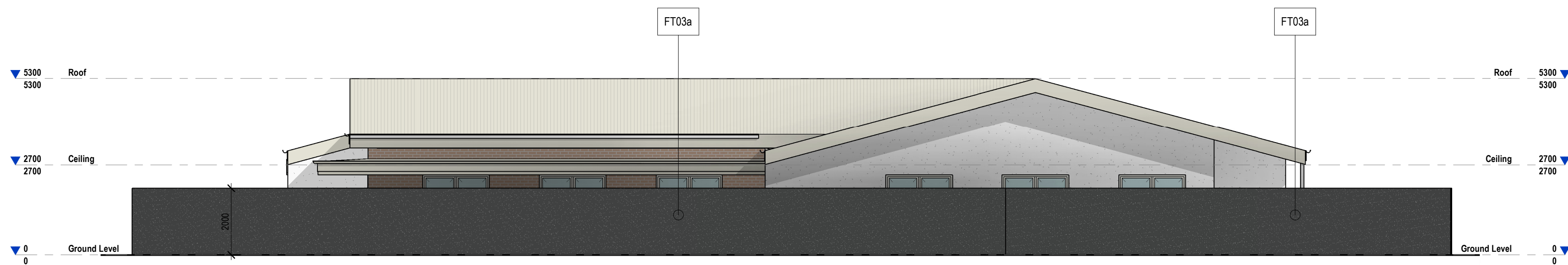
## DA ISSUE

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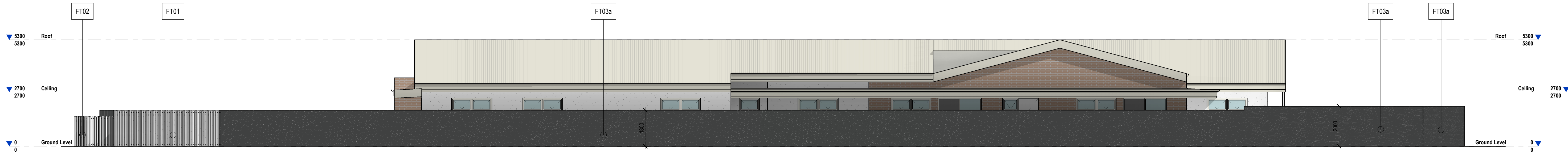
Rev.	Amendment	Date
1	DA SET UPDATES	05/02/24
2	DA SET UPDATES	16/02/24



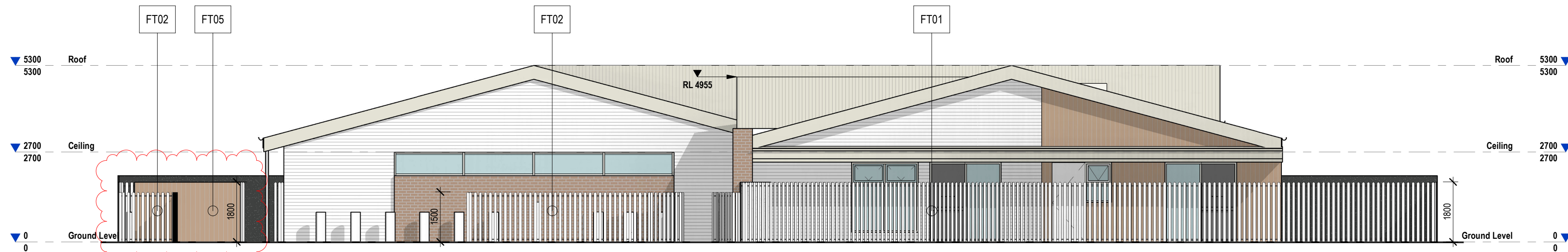
EAST FENCE ELEVATION



SOUTH FENCE ELEVATION



WEST FENCE ELEVATION



NORTH FENCE ELEVATION

## FENCE LEGEND

FT01 DEFENDER PICKET STYLE FENCE COLOUR: WHITE	FT03a 12MM FIXED CEMENT SHEET ACOUSTIC FENCE COLOUR: MONUMENT	FT03b 12MM FIXED CEMENT SHEET ACOUSTIC FENCE COLOUR: WHITE	FT04 COLOURBOND FENCE COLOUR: MONUMENT	FT05 TIMBER FENCE

## FENCE LEGEND

	FT01 FENCE TYPE 01 DEFENDER PICKET STYLE FENCE 1800 MINIMUM HIGH COLOUR: WHITE
	FT02 FENCE TYPE 02 DEFENDER PICKET STYLE FENCE 1500 MINIMUM HIGH COLOUR: WHITE
	FT03 FENCE TYPE 03 12MM FIXED CEMENT SHEET ACOUSTIC FENCE 1800-2000 MINIMUM HIGH (REFER ACOUSTIC REPORT)
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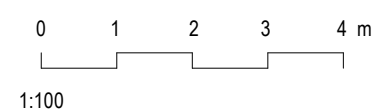
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## FENCE ELEVATIONS

Scale 1 : 100  
Date 12/12/23  
BF Job/Site No. 2023095  
Dwg No. DA07A Rev: 2 A1 SHEET

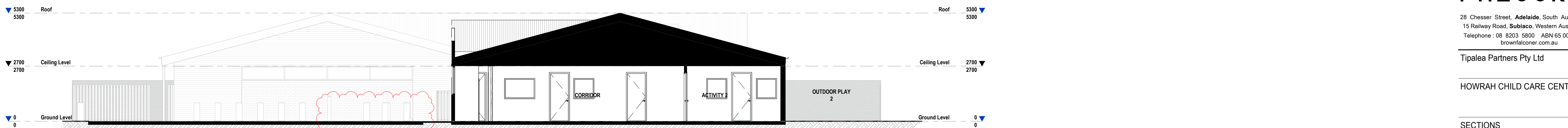
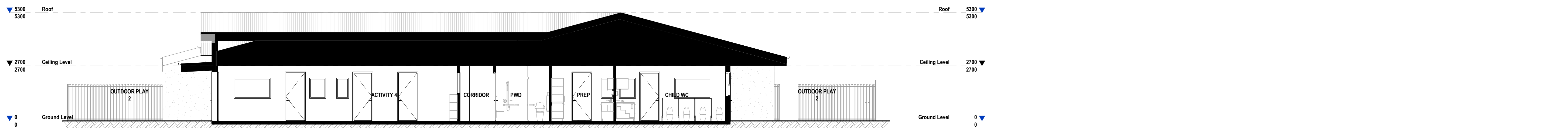
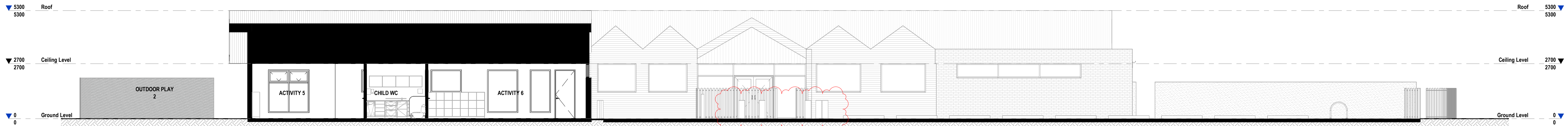




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Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23
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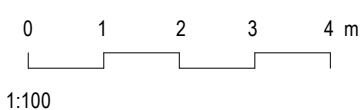
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HOWRAH CHILD CARE CENTRE

SECTIONS

Scale 1 : 100  
Date 12/12/23  
BF Job/Site No. 2023095

Dwg No. DA08 Rev: 4 A1 SHEET





DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL

Rev.	Amendment	Date
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3D IMAGES

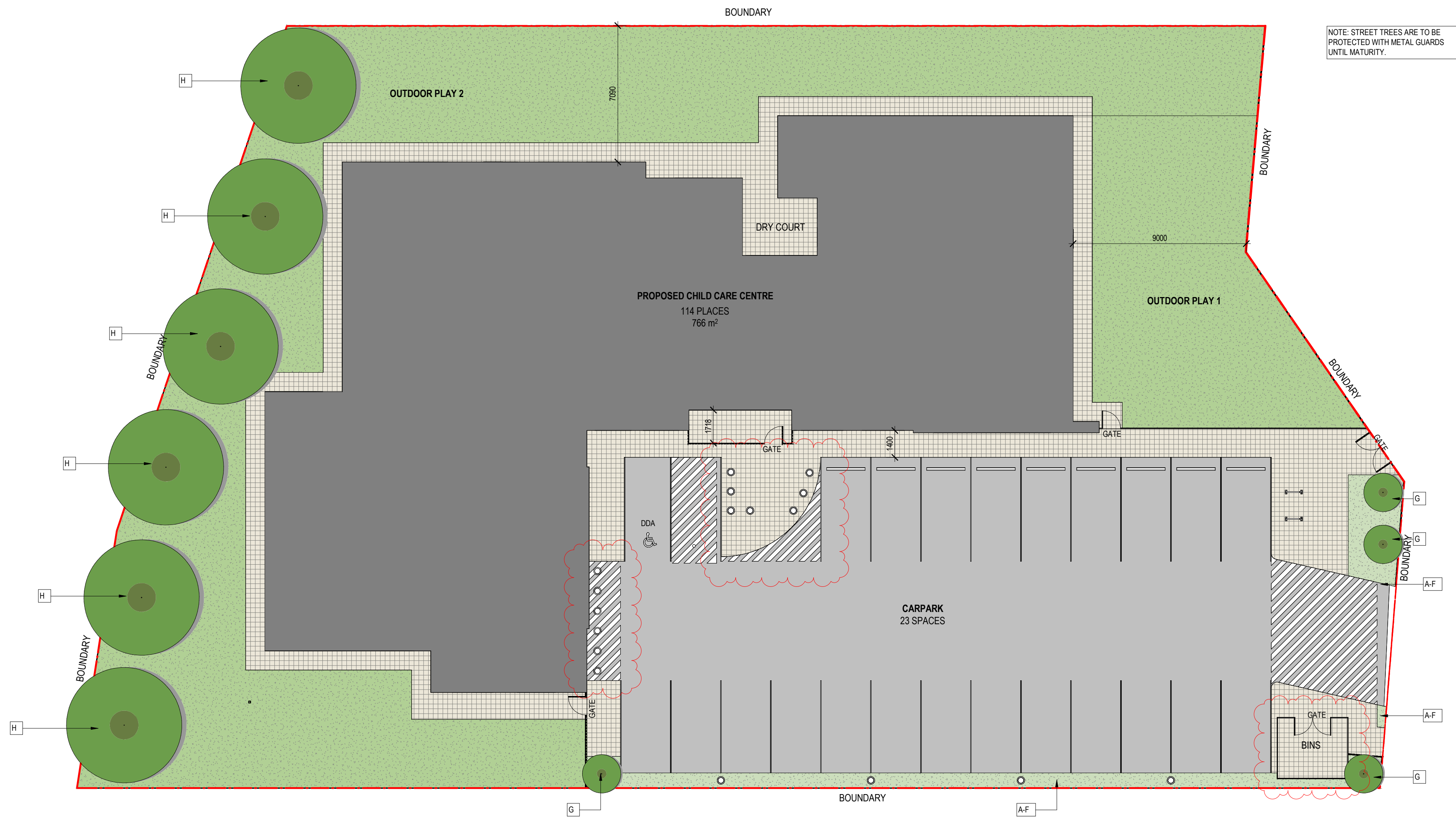
Scale  
Date 12/12/23  
BF Job/Site No. 2023095  
Dwg No. **DA09** Rev: **4** A1 SHEET



## DA ISSUE

ISSUED FOR DEVELOPMENT APPROVAL

Rev.	Amendment	Date
1	DRAFT DA SET	12/12/23
2	DA SET	19/12/23
3	DA SET UPDATES	05/02/24
4	DA SET UPDATES	16/02/24



## SITE PLAN - LANDSCAPE

1 : 150



A  
AGIZONANTHAS BUSH GEM  
'BUSH BITS'  
1m [H] 0.6m [W]



B  
DIANELLA REVOLUTA  
'BLACK FLAX LILY'  
0.4m [H] 0.4m [W]



C  
COREA DUSKY BELLS  
0.5m [H] 2.0m [W]



D  
DIANELLA 'TAS RED'  
0.45m [H] 0.45m [W]



E  
LOMANDRA TANIKA  
'DWARF MAT-RUSH'  
0.6m [H] 0.6m [W]



F  
DIANELLA LITTLE REV  
'DWARF FLAX LILY'  
0.6m [H] 0.6m [W]

G  
ORNAMENTAL PEAR TREE  
PYRUS CALLERYANA  
CAPITAL



H  
LEMON LIME WILLOW MYRTLE  
AGONIS FLEXUOSA

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Tipalea Partners Pty Ltd

HOWRAH CHILD CARE CENTRE

LANDSCAPE PLAN

Scale As indicated  
Date 12/12/23  
BF Job/Site No. 2023095

Dwg No. DA10 Rev: 4 A1 SHEET





**Tipalea**

**Howrah Childcare Centre**  
**Traffic Impact Assessment**

**February 2024**





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# 1. Introduction

## 1.1 Background

Midson Traffic were engaged by Tipalea to prepare a traffic impact assessment for a proposed childcare centre development at 21 Commerce Drive, Howrah.

## 1.2 Traffic Impact Assessment (TIA)

A traffic impact assessment (TIA) is a process of compiling and analysing information on the impacts that a specific development proposal is likely to have on the operation of roads and transport networks. A TIA should not only include general impacts relating to traffic management, but should also consider specific impacts on all road users, including on-road public transport, pedestrians, cyclists and heavy vehicles.

This TIA has been prepared in accordance with the Department of State Growth (DSG) publication, *Traffic Impact Assessment Guidelines*, August 2020. This TIA has also been prepared with reference to the Austroads publication, *Guide to Traffic Management*, Part 12: *Traffic Impacts of Developments*, 2019.

Land use developments generate traffic movements as people move to, from and within a development. Without a clear understanding of the type of traffic movements (including cars, pedestrians, trucks, etc), the scale of their movements, timing, duration and location, there is a risk that this traffic movement may contribute to safety issues, unforeseen congestion or other problems where the development connects to the road system or elsewhere on the road network. A TIA attempts to forecast these movements and their impact on the surrounding transport network.

A TIA is not a promotional exercise undertaken on behalf of a developer; a TIA must provide an impartial and objective description of the impacts and traffic effects of a proposed development. A full and detailed assessment of how vehicle and person movements to and from a development site might affect existing road and pedestrian networks is required. An objective consideration of the traffic impact of a proposal is vital to enable planning decisions to be based upon the principles of sustainable development.

This TIA also addresses the relevant clauses in C2.0, *Parking and Sustainable Transport Code*, and C3.0, *Road and Railway Assets Code*, of the Tasmanian Planning Scheme – Clarence, 2021.

## 1.3 Statement of Qualification and Experience

This TIA has been prepared by an experienced and qualified traffic engineer in accordance with the requirements of Council's Planning Scheme and The Department of State Growth's, *Traffic Impact Assessment Guidelines*, August 2020, as well as Council's requirements.

The TIA was prepared by Keith Midson. Keith's experience and qualifications are briefly outlined as follows:

- 28 years professional experience in traffic engineering and transport planning.
- Master of Transport, Monash University, 2006
- Master of Traffic, Monash University, 2004

- Bachelor of Civil Engineering, University of Tasmania, 1995
- Engineers Australia: Fellow (FIEAust); Chartered Professional Engineer (CPEng); Engineering Executive (EngExec); National Engineers Register (NER)

## **1.4 Project Scope**

The project scope of this TIA is outlined as follows:

- Review of the existing road environment in the vicinity of the site and the traffic conditions on the road network.
- Provision of information on the proposed development with regards to traffic movements and activity.
- Identification of the traffic generation potential of the proposal with respect to the surrounding road network in terms of road network capacity.
- Review of the parking requirements of the proposed development. Assessment of this parking supply with Planning Scheme requirements.
- Traffic implications of the proposal with respect to the external road network in terms of traffic efficiency and road safety.

## **1.5 Subject Site**

The subject site is located at 21 Commerce Drive, Howrah. The site is currently a vacant lot.

The subject site and surrounding road network is shown in Figure 1.

**Figure 1 Subject Site & Surrounding Road Network**

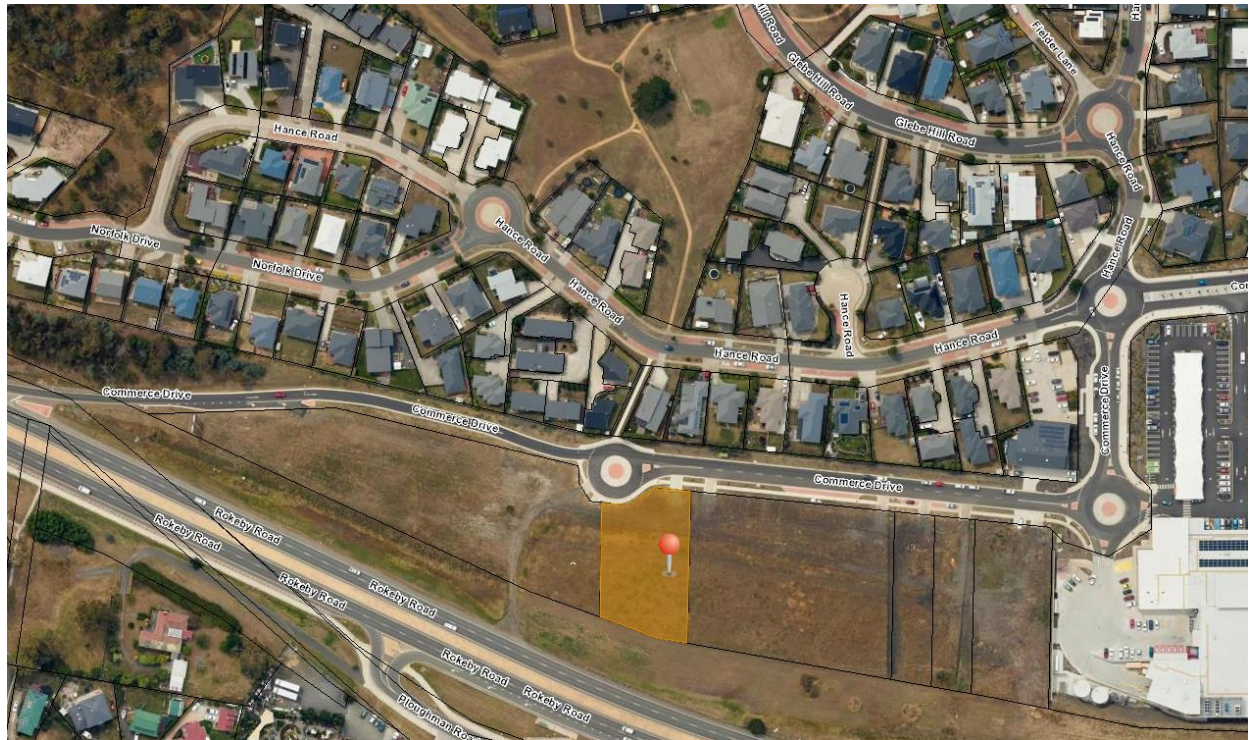


Image Source: LIST Map, DPIPW

## 1.6 Reference Resources

The following references were used in the preparation of this TIA:

- Tasmanian Planning Scheme - Clarence, 2021 (Planning Scheme)
- Austroads, *Guide to Traffic Management*, Part 12: *Traffic Impacts of Developments*, 2019
- Austroads, *Guide to Road Design*, Part 4A: Unsignalised and Signalised Intersections, 2021
- Department of State Growth, *Traffic Impact Assessment Guidelines*, 2020
- Roads and Maritime Services NSW, *Guide to Traffic Generating Developments*, 2002 (RMS Guide)
- Roads and Maritime Services NSW, *Updated Traffic Surveys*, 2013 (Updated RMS Guide)
- Australian Standards, AS2890.1, *Off-Street Parking*, 2004 (AS2890.1)

## 2. Existing Conditions

### 2.1 Transport Network

For the purposes of this report, the transport network consists of Rokeby Road and Commerce Drive only.

#### 2.1.1 Rokeby Road

Rokeby Road is a section of the South Arm Highway corridor through the urban areas of Howrah and Rokeby. Rokeby Road near the subject site is classified as a Category 3 under the Department of State Growth's road hierarchy. Regional Access Roads are defined as follows:

*Category 3 roads are of strategic importance to regional and local economies. While heavy freight vehicles use them, the level of use is less than that of Regional Freight Roads. Together with Regional Freight Roads, they provide safe and efficient access to Tasmania's regions. The traffic profile for 'Regional Access Roads' is described as roads that carry up to 300 trucks per day and annually carry between \$30m and \$70m of goods. Also, while the total traffic volume may vary, most have sections carrying a daily average of between 1,500 and 5,000 vehicles.*

Rokeby Main Road is a four-lane divided carriageway, with signalised intersections at Oceana Drive and Pass Road/ Tollard Drive to the east of the subject site. Rokeby Main Road carries approximately 24,400 vehicles per day near Pass Road<sup>1</sup>. The AM peak flow is approximately 2,100 vehicles per hour, and the PM peak hour flow is approximately 2,400 vehicles per hour. The posted speed limit of Rokeby Road near the Commerce Drive junction is 80-km/h.

#### 2.1.2 Commerce Drive

Commerce Drive connects between Rokeby Road and Pass Road, servicing the Glebe Hill region. It's connection with Rokeby Road is a one-way slip lane from Rokeby Road to an existing roundabout adjacent to the subject site. The balance of Commerce Drive consists of three links between roundabouts that provide connection to the Glebe Hill Shopping Centre, Hance Road and Pass Road.

The posted speed limit of Commerce Drive is 50-km/h. Traffic volumes are estimated to be 1,500 vehicles per day, with a dominant eastbound flow due to the slip lane from Rokeby Road.

Indented parking is provided on the southern side of Commerce Drive. Driveway accesses are only located on the southern side of the road noting that all lots connecting to Commerce Drive to the west of the shopping centre are currently vacant lots, including the subject site.

Commerce Drive near the subject site is shown in Figure 2.

---

<sup>1</sup> Department of State Growth traffic data, 2022 – west of Pass Rd/ Rokeby Rd signals



**Figure 2 Commerce Drive**



## 2.2 Road Safety Performance

Crash data can provide valuable information on the road safety performance of a road network. Existing road safety deficiencies can be highlighted through the examination of crash data, which can assist in determining whether traffic generation from the proposed development may exacerbate any identified issues.

Crash data was obtained from the Department of State Growth for a 5+ year period between 1<sup>st</sup> January 2018 and 31<sup>st</sup> October 2023 for Commerce Drive between Rokeby Road and Pass Road (excluding crashes at the Pass Road roundabout). No crashes were reported during this time.

## 3. Proposed Development

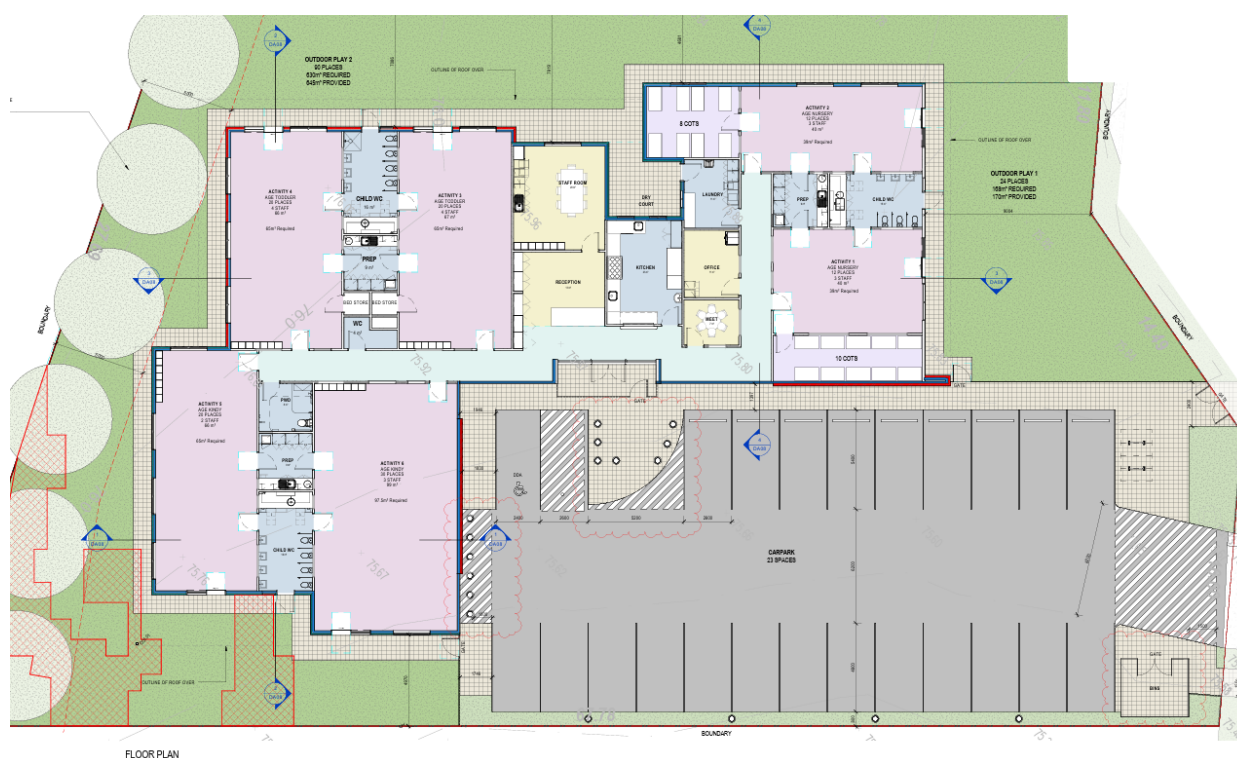
### 3.1 Development Proposal

The proposed development involves the construction of a new childcare centre catering for 114 children. Access to the site is via a driveway connecting to Commerce Drive immediately adjacent to a roundabout. On-site car parking is provided for 23 spaces, including 1 disabled space.

The hours of operation will be 6:30am to 6:30pm.

The proposed development is shown in Figure 3.

**Figure 3 Proposed Development Plans**





## 4. Traffic Impacts

### 4.1 Trip Generation

Trip generation rates were sourced from the RMS Guide. The RMS Guide traffic generation rates are summarised in Table 1.

**Table 1 RMS Traffic Generation**

Component	Quantity	Maximum Morning Peak Hour Traffic Generation	Maximum Afternoon Peak Hour Traffic Generation
Proposed childcare	114 children	1.4 trips per child 160 vehicles per hour	0.8 trips per child 91 vehicles per hour

The traffic generation will therefore be 160 and 91 vehicles per hour during the morning and afternoon peaks respectively. The daily traffic generation is likely to be in the order of 500 vehicles per day (assuming two trips per child places per drop-off and pick-up, plus staff vehicle trips).

### 4.2 Trip Assignment

It is likely that there will be a relatively even distribution of traffic arrivals from Rokeby Road and Commerce Drive. All traffic must exit the site to the east along Commerce Drive due to the one-way nature of Commerce Drive to the west of the roundabout adjacent to the subject site.

### 4.3 Access Impacts

The Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme states:

*"Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than: (a) the amounts in Table C3.1".*

Table C3.1 specifies a maximum increase of 20% of 40 vehicles per day, whichever is greater. In this case the existing access has no traffic generation as it accesses a vacant lot. The increase will therefore exceed the requirements of Table C3.1 and therefore does not comply with the requirements of Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme.

The Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme states:

*"Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:*

*(a) any increase in traffic caused by the use;*

- (b) the nature of the traffic generated by the use;*
- (c) the nature of the road;*
- (d) the speed limit and traffic flow of the road;*
- (e) any alternative access to a road;*
- (f) the need for the use;*
- (g) any traffic impact assessment; and*
- (h) any advice received from the rail or road authority”.*

The following is relevant with respect to the development proposal:

- a. Increase in traffic. The increase in traffic at the access will be 160 vehicles per hour during the morning peak and 91 vehicles per hour during the afternoon peak. This represents 2.7 vehicles each minute during the morning peak and 1.5 vehicles per minute during the afternoon peak period on average. The increase in traffic can be readily absorbed into the road network without any loss of efficiency noting that the access is located immediately adjacent to a roundabout, which provides traffic calming.
- b. Nature of traffic. The traffic will consist of cars, with timing that is normally associated with commuter and school peak periods. The access design is consistent with neighbouring accesses located along the frontage of Commerce Drive.
- c. Nature of road. Commerce Drive is a low volume collector road. The traffic generation and access conditions are compatible with the nature and function of the road.
- d. Speed limit and traffic flow of road. The speed limit of Commerce Drive is 50-km/h. The traffic flow is estimated to be in the order of 1,500 vehicles per day. The speed limit and traffic flow is compatible with the access conditions associated with the proposed development.
- e. Alternative access. No alternative access is considered necessary.
- f. Need for use. The access is required to provide access to the on-site car parking spaces.
- g. Traffic impact assessment. This report documents the findings of a traffic impact assessment.
- h. Road authority advice. Council (as road authority) require a TIA to be prepared for the development proposal.

Based on the above findings, the development complies with the requirements of Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme.

#### 4.4 Sight Distance

Australian Standards, AS2890.1, provide the sight distance requirements for commercial driveways. Sight distance requirements are lower for commercial driveways compared to road junctions.

The minimum sight distance requirements for an access driveway in a 50-km/h frontage road is 45 metres (the desirable sight distance is 69 metres). The available sight distance exceeds 200 metres to the east of the access and 150 metres to the west of the access.

#### 4.5 Pedestrian Impacts

The proposed development is likely to attract a relatively small amount of pedestrian movements in the surrounding network. The site is well connected to existing pedestrian infrastructure along Commerce Drive to the shopping centre and the nearby Glebe Hill residential catchment area.

The Acceptable Solution A1 of Clause C2.6.5 of the Planning Scheme states:

*"Uses that require 10 or more car parking spaces must:*

- (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:
 
  - (i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or*
  - (ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and**
- (b) be signed and line marked at points where pedestrians cross access ways or parking aisles".*

The car park has a pedestrian path that connects to the existing footpath of Commerce Drive which extends along the western, southern and northern sides of the car parking area. The path is not separated by a distance of 2.5 metres or protective bollards. No path is provided along the eastern side of the footpath. On this basis the pedestrian infrastructure within the car park does not comply with the requirements of Acceptable Solution A1 of Clause C2.6.5 of the Planning Scheme.

The Performance Criteria P1 of Clause C2.6.5 of the Planning Scheme states:

*"Safe and convenient pedestrian access must be provided within parking areas, having regard to:*

- (a) the characteristics of the site;*
- (b) the nature of the use;*
- (c) the number of parking spaces;*
- (d) the frequency of vehicle movements;*
- (e) the needs of persons with a disability;*

- (f) *the location and number of footpath crossings;*
- (g) *vehicle and pedestrian traffic safety;*
- (h) *the location of any access ways or parking aisles; and*
- (i) *any protective devices proposed for pedestrian safety”.*

The following is relevant with respect to the development:

- a. Characteristics of site. The site is a childcare development. The movement of cars and pedestrians only relates to activity associated with the childcare centre and would be expected by all road users. Vehicle speeds will be very low by virtue of the small size of the car park. The car park is relatively small and provides a very low speed environment, with a footpath available on three sides of the car parking area.
- b. Nature of the use. The use is a childcare centre. The size of the car park is relatively small, resulting in low vehicle speeds and relatively low traffic volumes (peak generation of 160 vehicles per hour during the morning peak period).
- c. Number of parking spaces. A total of 23 on-site parking spaces are proposed, accessed via a single driveway/ internal access.
- d. Frequency of vehicle movements. The peak traffic generation will be 160 vehicles per hour during the morning peak. The relatively low traffic generation coupled with the low vehicle speeds will result in an acceptable safety environment for shared use between pedestrians and cars.
- e. Needs of persons with a disability. Not applicable.
- f. Location and number of footpath crossings. Not applicable.
- g. Vehicle and pedestrian safety. The car park will be a 'shared zone' where vehicles and pedestrians share the space with pedestrians having priority. As noted in d above, the low traffic generation coupled with the low vehicle speeds will result in an acceptable safety environment for shared use between pedestrians and cars. This is typical of a small car park design that services a small commercial development.
- h. Location of access ways or parking aisles. The development has a relatively simple layout consisting a single access and central aisle. Parking is accessed at 90-degrees to the aisle.
- i. Protective devices. No pedestrian protective devices are included in the design.

Based on the above assessment, the development meets the requirements of Performance Criteria P1 of Clause C2.6.5 of the Planning Scheme.

## 4.6 Commercial Vehicles

The development will require food deliveries 1 to 2 times per week. Deliveries will be undertaken using a van (B85 or B99 vehicle size) utilising the car park outside of normal peak periods.

#### **4.7 Road Safety Impacts**

There are no significant detrimental road safety impacts foreseen for the proposed development. This is based on the following:

- The existing road safety performance of the Commerce Drive does not indicate that there are any current road safety deficiencies that might be exacerbated by the proposed development (noting that no crashes have been reported in the most recent five year period).
- The access is located immediately east of an existing roundabout. The roundabout provides traffic calming and a positive road safety environment for movements into and out of the access.
- Adequate sight distance is available at the proposed site access on Commerce Drive in relation to the prevailing vehicle speeds in accordance with Australian Standards requirements.
- The additional traffic generated by the proposed development (noting an estimated peak of up to 160 vehicles per hour, can be readily absorbed by the surrounding road network).

## 5. Parking Assessment

### 5.1 Parking Provision

The proposed development provides a total of 23 on-site car parking spaces. This includes 1 disabled parking space. The car parking layout is shown in Figure 3.

### 5.2 Planning Scheme Requirements

The Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme states:

*"The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:*

- (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;*
- (b) the site is contained within a parking precinct plan and subject to Clause C2.7;*
- (c) the site is subject to Clause C2.5.5; or*
- (d) it relates to an intensification of an existing use or development or a change of use where:*
  - (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or*
  - (ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:*

$$N = A + (C - B)$$

*N = Number of on-site car parking spaces required*

*A = Number of existing on site car parking spaces*

*B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1*

*C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1".*

In this case, sub-points (a), (b), (c), and (d) are not applicable. The car parking requirements in Table C2.1 for 'Educational and Occasional Care' is 1 space per employee and 1 space per 6 tertiary students.

The childcare centre ('occasional care') does not cater for tertiary students and therefore the requirements relate to staff parking only.

The proposed childcare centre will cater for up to 19 staff: the parking requirement is therefore 19 spaces. The parking provision of 23 spaces satisfies the requirements of Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme.

### 5.3 Car Parking Layout

The Acceptable Solution A1.1 of Clause C2.6.2 of the Planning Scheme states:

*"Parking, access ways, manoeuvring and circulation spaces must either:*

*(a) comply with the following:*

*(i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6;*

*(ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;*

*(iii) have an access width not less than the requirements in Table C2.2;*

*(iv) have car parking space dimensions which satisfy the requirements in Table C2.3;*

*(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;*

*(vi) have a vertical clearance of not less than 2.1m above the parking surface level; and*

*(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or*

*(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6".*

The car parking layout is assessed in the following sections:

#### 5.3.1 Gradient

##### Driveway Grade

Section 2.5.3(b) of AS2890.1 states the following regarding the maximum grade of straight ramps:

- i. Longer than 20 metres – 1 in 5 (20%) maximum.
- ii. Up to 20 metres long – 1 in 4 (25%) maximum. The allowable 20 m maximum length shall include any parts of the grade change transitions at each end that exceed 1 in 5 (20%).

The driveway grade is effectively level and therefore complies with AS2890.1 requirements.





- Aisle width 6.4 metres

The parking dimensions within the car park have the following dimensions:

- Space width 2.6 metres
- Space length 5.4 metres western aisle  
4.8 metres + 0.8 metres overhang = 5.6 metres<sup>2</sup> eastern aisle
- Aisle width 6.2 metres

The space length is greater than the minimum requirement and the aisle width is less than the minimum requirement, however these dimensions cancel each-other and effectively comply with the requirements of Table C2.3 (noting the total car park cross-section width exceeds the minimum requirements by 0.2 metres).

### 5.3.5 Vertical Clearance

The site is not constrained by vertical obstructions.

### 5.3.6 Assessment Summary

The proposed car parking design complies with the requirements of Acceptable Solution A1.1(a) of Clause C2.6.2 of the Planning Scheme.

## 5.4 Accessible Parking

The Acceptable Solution A1.2 of Clause C2.6.2 of the Planning Scheme states:

*"Parking spaces provided for use by persons with a disability must satisfy the following:*

*(a) be located as close as practicable to the main entry point to the building;*

*(b) be incorporated into the overall car park design; and*

*(c) be designed and constructed in accordance with Australian/New Zealand Standard AS/NZS 2890.6:2009 Parking facilities, Off-street parking for people with disabilities".*

The following is relevant with respect to the development proposal:

- a. Location. The disabled parking space is located immediately adjacent to the main entrance to the building.

<sup>2</sup> Australian Standards, AS2890.1 permits spaces to be 5.4m, or 4.8m + 0.6m overhang – the parking dimensions are consistent with AS2890.1 requirements.

- b. Car park design. The disabled parking space has been incorporated into the design of the car park.
- c. Disabled space design. The disabled parking space complies with the dimensional requirements in AS2890.6, including the adjacent shared area.

The National Construction Code provides the requirements for the number of disabled spaces. The Code classifies the building as a 'Class 9b' building. This requires 1 disabled parking space for every 100 car parking spaces. This is a requirement for 1 disabled space, which is provided.

Based on the above assessment, the disabled parking provision complies with the requirements of Acceptable Solution A1.2 of Clause C2.6.2 of the Planning Scheme.

## **5.5 Bicycle Parking**

The Acceptable Solution A1 of Clause C2.5.2 of the Planning Scheme states:

*"Bicycle parking spaces must:*

*(a) be provided on the site or within 50m of the site; and*

*(b) be no less than the number specified in Table C2.1".*

Bicycle parking is provided on-site for 4 bicycle spaces. Table C2.1 requires 1 bicycle space per 5 employees. This is a requirement for 4 bicycle spaces. The requirements of Acceptable Solution A1 of Clause C2.5.2 of the Planning Scheme are met.

## 6. Conclusions

This traffic impact assessment (TIA) investigated the traffic and parking impacts of a proposed childcare development at 21 Commerce Drive, Howrah.

The key findings of the TIA are summarised as follows:

- The childcare centre will cater for up to 114 children.
- The childcare centre currently generates approximately 160 vehicles per hour to and from the site during the morning peak period, and 91 vehicles per hour during the afternoon peak period.
- Access to the development will be via a driveway access immediately east of the existing roundabout on Commerce Drive. The access complies with the requirements of Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme.
- The proposed childcare centre provides a total of 23 on-site car parking spaces. The parking provision complies with the requirements of Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme.
- The car parking design complies with the requirements of Acceptable Solution A1.1(a) of Clause C2.6.2 of the Planning Scheme.

Based on the findings of this report the proposed development is supported on traffic grounds.

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**Document Status**

Revision	Author	Review	Date
0	Keith Midson	Zara Kacic-Midson	12 December 2023
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22 February 2024

Reference: 2324-030

Amanda Beyer  
Senior Planner  
Clarence City Council

By email: [abeyer@ccc.tas.gov.au](mailto:abeyer@ccc.tas.gov.au); [cityplanning@ccc.tas.gov.au](mailto:cityplanning@ccc.tas.gov.au)

Dear Amanda,

**21 COMMERCE DRIVE, HOWRAH – PDPLANPMTD-2024/041321  
RESPONSE TO FURTHER INFORMATION REQUEST**

ERA Planning and Environment continue to act on behalf of Tipalea Partners in relation to the proposed use and development of a childcare centre at 21 Commerce Drive, Howrah.

Please find below our response to the City of Clarence's request for additional information in relation to this development application.

Item no.	Council request	ERA response
<b>General matters</b>		
1	Revised proposal plans referencing the property address	The property address, 21 Commerce Drive, Howrah, is included on the front page of the proposal plans.
<b>Glebe Hill Neighbourhood Centre Specific Area Plan</b>		
2	Details of the public art proposed to be incorporated into the development to determine compliance with clause S13.7.4 P1 of the Glebe Hill Neighbourhood Specific Area Plan.  For assistance, a copy of Council's Public Art Policy is available at <a href="#">Public-Art- Policy-2013.pdf</a> ( <a href="http://ccc.tas.gov.au">ccc.tas.gov.au</a> ).	As discussed at the recent meeting between ERA, Tipalea Partners and Clarence City Council; Council is to include a condition on the permit requiring the provision of public art, that satisfies clause S13.7.4 and to the satisfaction of the planning manager. This would enable time for our client and Council to collaborate on an appropriate piece of art for the site, whilst ensuring the planning scheme requirements have been satisfied.
<b>General Business Zone</b>		
3	Revised site and elevation plans showing the proposed frontage fencing for all elevations and acoustic barriers required to satisfy the recommendations made in Section 5 of the Noise Impact Assessment including location, height, transparency, and materials to determine compliance with clause 15.4.4 A1/P1 of the General Business Zone.	On both the attached site plan sheet (DA04) and the fence elevations sheet (DA07a) the architects have incorporated the recommendations of the Noise Impact Assessment (NIA). These relate to fence type 03, which is a 12 mm fixed cement sheet acoustic fence that varies in height between 1.8 m and 2.0 m, per the acoustic recommendations. As required by the NIA, the fence will be solid.  Clause 15.4.4 P1 has been addressed in the supporting planning report.

Item no.	Council request	ERA response
4	Revised proposal plans clearly showing the intended height, materials, and transparency of the bin enclosure to determine compliance with clause 15.4.5 P1 of the General Business Zone. Given the proximity of this structure to the Commerce Drive frontage, consideration should be given to the relocation of this facility behind the front building line of the childcare centre and for the conversion of this area to landscaping to activate this space and provide the appearance of landscaped public realm.	<p>The height, material and transparency of the fencing surrounding the bin enclosure is shown on the proposal plans, on both the site plan sheet (DA04) and the fence elevations sheet (DA07a). Per the plans, it is fence type 04, which is a colorbond fence in the monument colour, with a minimum height of 1.8m. This is to adequately screen the bins from passers-by. The bin area is fenced and therefore not visible from the road or public open space adjoining the site, and therefore satisfies clause 15.4.5 A1.</p> <p>The bin enclosure has been moved from its former location near the bicycle parking to the other side of the car park entrance, in the north-eastern corner of the site. This minimises conflict between uses on site, and places the bins away from the main pedestrian entrance to the site. This is a preferred location for the bin enclosure, resulting in a better street presentation for the development. It has also allowed for additional landscaping to be added to the site near the former bin enclosure location.</p>
5	The absence of landscaping along the full length of the frontage (particularly separating the play space from the frontage) would not serve to activate this space for the public realm as required by clause 15.4.2 P1(b) of the General Business Zone. Consideration ought to be given to the continuation of a landscape buffer along the full frontage with the play space fencing set behind the landscaped areas.	<p>As way of background, the <i>Early Childhood Centre and School Age Care Facilities Code 2012</i> provides technical requirements that each childcare centre must provide. These include, amongst other requirements:</p> <ul style="list-style-type: none"> <li>• At least 3.25 m<sup>2</sup> of unencumbered indoor space per child</li> <li>• At least 7 m<sup>2</sup> of unencumbered outdoor space per child</li> </ul> <p>The proposed childcare centre comprises 813 m<sup>2</sup> of outdoor space and 762 m<sup>2</sup> of indoor space. Both the indoor space and outdoor space must then meet specific requirements per the <i>Early Childhood Centre and School Age Care Facilities Code 2012</i>, and taking that into account, the proposed centre can accommodate 114 children in care.</p> <p>There is a slight excess of outdoor space proposed, however, some of the outdoor space doesn't meet the requirements due to corners which remove teacher site lines with students. Further, operators typically require more outdoor and indoor space than what is required by the code, favouring quality over quantity.</p> <p>If the outdoor space were to be reduced to accommodate additional landscaping, this would result in the number of children in the centre having to be reduced also. It is noted that across Greater Hobart there is currently a significant shortage of childcare for Tasmanian families, with many childcare centres having lengthy waitlists. Taking this into account, it is considered that the centre will provide a greater overall community benefit by providing more childcare spots rather than providing landscaping at the front of the site.</p> <p>Regardless, as detailed in the supporting planning report, it is considered that the proposal satisfies clause 15.4.2 P1, having regard to subclause (b,) by:</p>

Item no.	Council request	ERA response
		<ul style="list-style-type: none"> <li>providing variations in the building alignment;</li> <li>providing landscaping and active outdoor areas between the building alignment and the frontage;</li> <li>ensuring concealment spaces are avoided in this frontage area; and</li> <li>with the picket style fence on the Commerce Drive frontage providing opportunity for passive surveillance.</li> </ul> <p>Moreover, it would not be appropriate to provide space on site for the public to use given the nature of the use being both sensitive and vulnerable.</p>
6	A detailed external lighting plan to determine compliance with clause 15.3.1 P2 of the General Business Zone.	<p>The proposal satisfies clause 15.3.1 A2, as external lighting will not operate within the hours of 11pm and 6am, except for security lighting, which will not extend into residential properties.</p> <p>An assessment against the performance criteria, P2, and subsequently an external lighting plan, is not required.</p>
7	<p>Amended plans to determine compliance with clause 15.4.3 A1 of the General Business Zone showing:</p> <ul style="list-style-type: none"> <li>(a) the location of mechanical plant and other service infrastructure for each building and the proposed screening arrangement from the street and other public places on the site;</li> <li>(b) the location of any proposed roof top mechanical plant and service infrastructure; and</li> <li>(c) location and design of external lighting to illuminate external vehicle parking areas and pathways.</li> </ul>	<p>Per the <i>Clarence Local Provisions Schedule</i>, clause CLA S13.7.1 of the Glebe Hill Neighbourhood Centre Specific Area Plan is in substitution for General Business Zone clause 15.4.3 Design.</p> <p>As such, Clause 15.4.3 does not apply to the proposal.</p>
8	The location of proposed public art integrated into the building design (building facades facing the frontage and other public spaces) as required by clause 15.4.3 P2(c).	See response to item 2.
9	The location and design of storage areas as shown on the site and elevation plans to determine compliance with clause 15.4.5 A1 of the General Business Zone.	The location and design of the outdoor bin storage is shown on the proposal plans. Refer to the supporting planning report which details how the proposal satisfies clause 15.4.5 A1.
10	Demonstration of suitable lighting to vehicle parking areas and pathways for the safety and security of users to determine compliance with clause 15.4.3 P1(c) of the General Business Zone.	<p>Per the <i>Clarence Local Provisions Schedule</i>, clause CLA S13.7.1 of the Glebe Hill Neighbourhood Centre Specific Area Plan is in substitution for General Business Zone clause 15.4.3 Design.</p> <p>As such, Clause 15.4.3 does not apply to the proposal.</p>
<b>Parking and Sustainable Transport Code</b>		
11	Demonstration of carpark and driveway drainage as per clause C2.6.1 A1 (b) of the Parking and Sustainable Transport Code.	<p>The carpark is proposed to be constructed with a durable all weather pavement. This satisfies clause C2.6.1(a), and therefore meets the acceptable solution of the clause.</p> <p>Further, stormwater will be drained to Council's stormwater system combined with detaining stormwater onsite as required. Details in</p>

Item no.	Council request	ERA response
		accordance with the <i>Urban Drainage Act 2013</i> will be provided at the plumbing and building permit stage.
12	Details of parking and access way gradients and turning paths as per clause C2.6.2 A1 of the Parking and Sustainable Transport Code.	<p>The gradient of the parking area and driveway is addressed in section 5.3 of the Traffic Impact Assessment, including how the acceptable solution is met.</p> <p>Turning paths for the car park are provided in section 5.3.2 of the Traffic Impact Assessment, and further demonstrate compliance with clause C2.6.2 A1.</p>
13	Plans confirming whether the car parking lighting will comply with clause C2.6.4 A1 of the Parking and Sustainable Transport Code.	<p>Indicative lighting is shown on the site plan, sheet DA04.</p> <p>It is requested that a condition is included on the permit requiring lighting to be constructed and designed in accordance with Clause 3.1 "Basis of Design" and Clause 3.6 "Car Parks" in Australian Standard/New Zealand Standard AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting – Performance and design requirements.</p>
14	<p>It is noted that proposed pedestrian access does not comply with clause C2.6.5 A1 due to the insufficient width near the entrance to the building, insufficient connection with all car parking spaces and insufficient protection by means of bollards or any other physical means. We note the response provided in section 4.5 of the TIA however, council's engineers do not agree that the response has satisfactorily addressed the performance criteria to clause C2.6.5 for the following reasons:</p> <ul style="list-style-type: none"> <li>• Characteristics of site- the site is a childcare development and includes vulnerable users including young children therefore it is important for appropriate, safe and connected design of pedestrian pathway/footpaths within the development site.</li> <li>• Nature of the use- the childcare centre has elements of intense peak traffic flow which coincides with the high pedestrian flow therefore safe pedestrian facilities are paramount.</li> </ul> <p>In response, the car parking layout and associated pedestrian facilities will require the following modifications:</p> <ul style="list-style-type: none"> <li>• Access to the facility may often be used by persons with access limitations and this will need to be factored into the design.</li> <li>• In relation to the location and number of footpath crossings, the proposed car park splits into two parts therefore the provision of appropriate crossings and footpath to connect these two parts is required.</li> </ul>	<p>The carpark and pedestrian walkways have been amended to better respond to clause C2.6.5. A response to the clause is provided in the amended Traffic Impact Assessment, refer to section 4.5.</p>
<b>Engineering matters</b>		
15	Design details for the two-sided bike parking rails, to determine compliance with clause C2.6.7 A1 of the Parking and Sustainable Transport Code.	Per Table C2.1 of the Parking and Sustainable Transport Code, a rate of 1 bicycle space per 5 employees is required, which results in a requirement of 4 bicycle parking spaces. This is



Item no.	Council request	ERA response
		<p>provided on site in the form of two-sided bike parking rails.</p> <p>Clause C2.6.7 A1 only applies to uses that require 5 or more bicycle spaces, therefore it is not applicable to the proposal.</p>

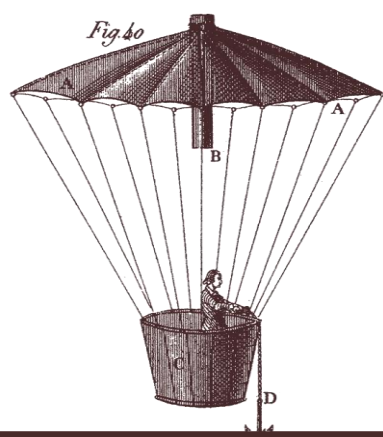
It is considered that the above adequately addresses the request for additional information, however, should you require anything additional please do not hesitate to contact me on 03 6135 0443 or at [monica@eraplanning.com.au](mailto:monica@eraplanning.com.au).

Yours sincerely,



Monica Cameron  
**Senior Planner**

*Attachments*      Revised proposal plans prepared by Brown Falconer, dated 16 February 2023  
Revised Traffic Impact Assessment prepared by Midson Traffic, dated 21 February 2023



# Howrah Childcare Centre: Concept Services Report

## Planning Scheme Compliance

21 Commerce Drive, Howrah, Tasmania, 7018  
for Tipalea Partners

08.12.2023

Version control

Revision	Description	Issue date	Issued by
1	Planning Approval	08.12.2023	Alice Curtain

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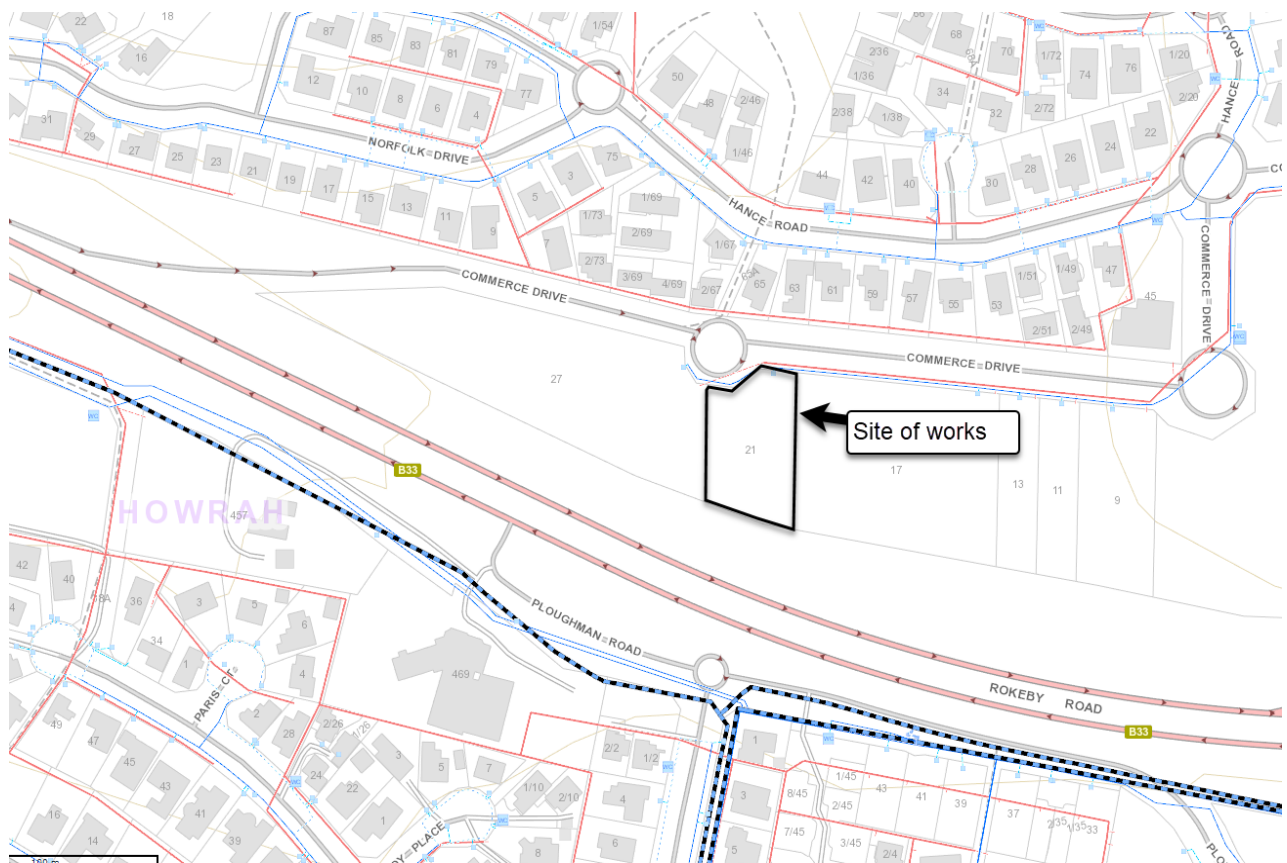
# 1 Context

Gandy and Roberts Consulting Engineers have been engaged by Tipalea Partners Pty. Ltd. to provide concept servicing documentation for a proposed childcare centre at 21 Commerce Drive, Howrah, Tasmania. This report has been prepared for the Planning Application submission process and will discuss the methodology in which the sites sewer, water and stormwater infrastructure will be delivered to existing TasWater and City of Clarence (CCC) owned infrastructure.

## 2 Development Locality

### 2.1 Development Site and Details

The proposed development is located at 21 Commerce Drive, Howrah, Tasmania and can be seen in **Figure 1** below. The current site is undeveloped and consists of only grassed areas.



**Figure 1: Site of Works (LISTMap Data)**

## 2.2 Affected Properties

It is not anticipated that any adjacent properties will be impacted by the proposed development.

## 3 Stormwater Management

### 3.1 Local Authority Requirements

The CCC's *Stormwater Management Procedure for New Development* requires that this development manages stormwater to specific requirements:

Section 7- Stormwater System Design Requirements states:

*'The minor stormwater drainage system in new developments shall be designed in accordance with the following levels of service:*

- *2% AEP for light industrial and general industrial zones.*
- *5% AEP for all other zones.'*

The site is identified as a general business zone; therefore, stormwater analysis is designed to cater for 5% AEP levels of service.

Section 7- Stormwater Quality Management Requirements states:

*'Unless exempt, all other new developments must incorporate water sensitive urban design principles for the treatment and disposal of stormwater to meet the Water Quality Treatment Target set by council as follows:*

- *90% reduction in the average annual load of litter/gross pollutants based on typical urban stormwater concentrations; AND*
- *80% reduction in the average annual load of total suspended solids (TSS) based on typical urban stormwater TSS concentrations; AND*
- *45% reduction in the average annual load of total phosphorus (TP) based on typical urban stormwater TP concentrations; AND*
- *45% reduction in the annual load of total nitrogen (TN) based on typical urban stormwater TN concentrations.'*

As per Section 7 -Stormwater Quality Management Requirements, the proposed site is not exempt to these requirements. Therefore, stormwater treatment is to be provided for the site.

Section 7- Stormwater Disposal Method Requirements states:

*'Stormwater must be disposed of by gravity to the council stormwater system where possible.'*

All stormwater runoff will be discharging via gravity to an existing site connection on Commerce Drive.

Section 7- Stormwater Quantity Management Requirements states:

*'Any increase in stormwater runoff must be accommodated within:*

- *An existing public water system to the satisfaction of the council; or*
- *Public infrastructure upgraded by the developer as part of the development construction to the satisfaction of the council; or*
- *On-site detention designed to offset the increase in stormwater runoff caused by the development, to the satisfaction of the Group Manager Engineering Services.'*

This development will result in an increase in impervious area and stormwater runoff for the site, therefore quantity management is required. On-site detention will be provided with orifice-controlled outlet to limit the stormwater quantity entering the CCC infrastructure from the site.

### 3.2 Pre-Development Stormwater Runoff

The pre-development site is undeveloped and consists of entirely pervious grass and dirt. The total area of the site is 2405 m<sup>2</sup>. In accordance with the CCC's *Stormwater Management Procedure for New Development*, the permissible site discharge (PSD) was calculated using the Rational Method from AS/NZS 3500.3 (2018) for the 5% AEP design event with the following results:

AEP:	5%
Duration:	5 minutes
Intensity:	86 mm/hr
Permissible Site Discharge (PSD):	11.7 L/s

### 3.3 Post-Development Stormwater Runoff

The post-development site will include roofs, areas of impervious hardstand and areas of pervious landscaping, as summarised in **Table 1** below.

Catchment and Runoff Coefficient as per AS3500.3 (2018)	Area (m <sup>2</sup> )
<i>Hardstand (C=0.9)</i>	713
<i>Impervious roofs (C=1.0)</i>	775
<i>Landscape areas (C=0.2)</i>	917
<i>Total Site</i>	2405

**Table 1: Summary of impervious and pervious post-development site areas and runoff coefficients.**

The post-development peak run-off was calculated using the Rational Method from AS/NZS 3500.3 (2018) to be 23.4 L/s for the 15-minute storm duration. Clearly this value exceeds the PSD, therefore a detention system will be required to control discharge flow from the developed site.

### 3.4 Stormwater Runoff Mitigation Proposal

To control the developed site discharge to the identified PSD, an onsite detention system is proposed with an orifice regulated discharge limiting flow offsite. To comply with the CCC's *Stormwater Management Procedure for New Development*, the detention system volume is designed to cater for the 5% AEP design event across a range of storm durations from 1-90 minutes.

Detention requirements were calculated using Boyd's Formula and result in a detention volume of 10522 L, as shown in **Table 2** below.

Duration (min)	Intensity (mm/hr)	Peak Runoff (L/s)	Runoff Volume (L)	Detention Requirement (L)
1	140	62.4	3743	3038
2	110	49.0	5882	4472
3	99.4	44.3	7973	5858
4	91.8	40.9	9818	6998
5	85.6	38.1	11443	7919
10	64.7	28.8	17299	10250
15	52.6	23.4	21095	10522
20	44.7	19.9	23903	9805
25	39.2	17.5	26202	8581
30	35.1	15.6	28154	7008
45	27.4	12.2	32966	1248
60	22.9	10.2	36736	-5555
90	18	8.0	43313	-20124

**Table 2: Detention requirements for various storm durations.**

It is proposed that all runoff from the new impervious hardstand and building roof are captured by on-site detention. The runoff from the pervious landscape areas will be discharged from the site undetained. Roof runoff will be collected and detained in two above-ground slimline tanks providing a



combined detention volume of 6000 L, refer to Civil Drawing C040. The detained roof runoff will be released at a controlled rate of 3.24 L/s by a 35mm orifice to the site stormwater system.

Hardstand runoff will be collected by a series of pits and directed to a DN450 pipe of 30 metres length graded at 0.5% providing a detention volume of 4769 L, refer to Civil Drawing C040. The detained hardstand runoff will be released at a controlled rate of 3.55 L/s by a 51mm orifice to the site stormwater system.

Analysis using the Rational Method from AS/NZS 3500.3 (2018) shows that the combined detained and un-detained runoff can be limited to 11.19 L/s and does not exceed the PSD. Thus, the CCC's *Stormwater Management Procedure for New Development* quantity and design requirements are satisfied.

### **3.5 Water Sensitive Urban Design**

#### **3.5.1 Stormwater System Concept**

A management system for the proposed development will incorporate the following design elements, as shown on Civil Drawing C040:

- Four Atlan 600x600 Stormsacks, and
- One Atlan Ecoceptor (1500 Series).

#### **3.5.2 MUSIC Modelling**

MUSIC V6.3.0 was used to model the performance of the proposed stormwater system. The model predicted the following performance outcomes:

- Total Suspended Solids reduction of 80.2%,
- Total Phosphorus reduction of 71.2%,
- Total Nitrogen reduction of 54.3%,
- Gross Pollutants reduction of 100%.

These reduction percentages satisfy the requirements of the CCC's *Stormwater Management Procedure for New Development*.

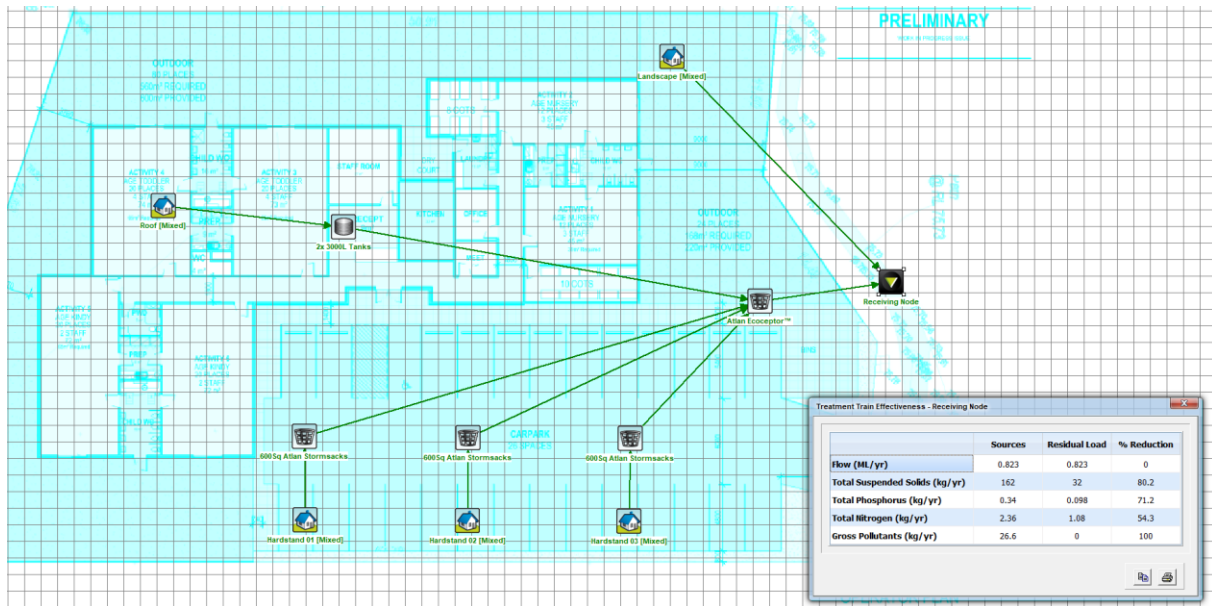


Figure 2: MUSIC model and performance percentages.

## 4 Sewer and Water Service

### 4.1 Existing Infrastructure

#### 4.1.1 Sewer

The current sewer connection is in the north-eastern corner of the site and discharges to an existing TasWater DN150 PVC-U sewer main in Commerce Drive, refer to Civil Drawing C050 for details.

#### 4.1.2 Water

The site currently has a DN50 property connection with a DN20 below-ground meter that is serviced by a TasWater DN150 PVC-O main in Commerce Drive, refer to Civil Drawing C060 for details.

### 4.2 Service Requirements for Proposed Development

#### 4.2.1 Sewer

Sewer flows have been calculated using both AS/NZS 3500.2 (2021) as well as the TasWater Supplement to the WSA 02-2014-3.1 the WSAA Gravity Sewerage Code of Australia. Development flows for the proposed site are shown in **Table 3** below.

The existing sewer connection is to be used to service the new development.

Design Parameter	Unit	Notes
Fixture Units (AS3500.2:2021)	114	
Equivalent Tenements	12.2	CF01 Childcare centre/ Pre-school
Average Dry Weather Flow (ADWF)	0.065 L/s	
Peaking Factor 'd'	10.99	
Peak Dry Weather Flow (PDWF)	0.712 L/s	

**Table 3: Development Sewerage Flows**

*Note: The listed sewage flows require the installation of DN100 private sewer drainage at a minimum grade of 1.65% in accordance with AS3500.2:2021.*

#### 4.2.2 Water

Domestic water pipework has been sized in accordance with AS3500.1 (2021) for probable simultaneous demand.

The proposed development has a classification as Class 9b by the National Construction Code (NCC). The building is not proposed to be sprinkler protected and so in accordance with Table 2.2.5 (B) of AS 2419.1 (2021), one fire hydrant is required to service the building. The existing TasWater fire hydrant adjacent to the property on Commerce Drive is suitable to provide the required coverage. Development flows are shown in **Table 4**.

Design Flow	Unit	Notes
Domestic Water (AS3500.1:2021)	0.94 L/s @ 500 kPa (7.32 ET's)	Domestic Water (AS3500.1:2021)
Fire Services (Hydrants)	NA	Fire Services (Hydrants)
Fire Services (Sprinklers)	NA	Fire Services (Sprinklers)

**Table 4: Development Water Flows**

*Note: The above listed domestic water flows require the installation of DN32 private water pipework in accordance with AS3500.1:2021.*

### 4.3 Conclusion

The development can be adequately serviced by the existing TasWater infrastructure.

## 5 Appendix A: Concept Servicing Plans

The following drawings shall be read in conjunction with this report for a more detailed overview of the proposed development:

- C020 – Concept Site Plan
- C040 – Concept Stormwater Plan
- C050 – Concept Sewer Plan
- C060 – Concept Water Plan
- C070 – Concept Details Plan

NOTES

1. LOCATION OF SOME EXISTING SERVICES BASED ON DIAL BEFORE YOU DIG INFORMATION CONTRACTOR TO CONFIRM ACTUAL LOCATION ON SITE PRIOR TO COMMENCING WORKS.
2. DRIVEWAY LAYOUT DESIGN BY THE ARCHITECT (BROWN FALCONER)
3. LINE MARKING, WHERE INDICATED, DELINEATING PARKING SPACES TO BE 80mm WIDE USING YELLOW WATER BASED ACRYLIC LINE MARKING PAINT.
4. PARKING SPACE NUMBERING OR VISITOR PARKING DESIGNATION, WHERE INDICATED, TO BE MIN. 200mm HIGH TEXT USING YELLOW WATER BASED ACRYLIC LINE MARKING PAINT.
5. PROVIDE WHEEL STOPS, WHERE INDICATED, TO AS/NZS 2890.1.
6. JOINTS ON CURVES TO BE PERPENDICULAR TO EDGE AND MINIMUM 450 LONG BEFORE CHANGE OF DIRECTION.
7. ALL PITS TO BE FINISHED FLUSH WITH SURFACE LEVEL.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

DISABLED PARKING SPACE TO DETAIL ON C070.

FLUSH KERB TO LGAT TSD-R14-v3

PROVIDE 1650 x 150 x 100 WHEEL STOPS AS SHOWN.  
REFER TO C070 FOR SET OUT.

PROVIDE LINE MARKING TO BAYS.  
REFER NOTE 4.

EXS DRIVEWAY CROSSOVER AND APRON  
TO BE MAINTAINED.  
PROVIDE SMOOTH TRANSITION BETWEEN  
NEW AND EXS CROSSOVER SURFACE.

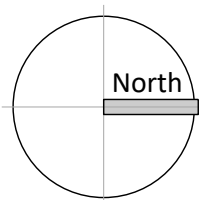
ASPHALT PAVEMENT TO DETAIL ON C070

600mm PARKING OVERHANG FROM FOK

TYPE KC KERB AND CHANNEL TO LGAT TSD-R14-v3

17 COMMERCE DRIVE

THIS DRAWING HAS NOT BEEN APPROVED  
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HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT SITEWORKS PLAN

0 50mm		SCALE 1:100@A1
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PROJECT 23.0407	DRAWING C020	REVISION A

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Engineers

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NOTES

1. ALL PRIVATE STORMWATER INFRASTRUCTURE SHALL COMPLY WITH AS/NZS 3500.3, THE NCC AND ANY OTHER AUTHORITY HAVING JURISDICTION OVER THE INSTALLATION
2. THE LOCATION OF SOME EXISTING SERVICES IS BASED ON DIAL BEFORE YOU DIG INFORMATION. CONTRACTOR TO CONFIRM ACTUAL LOCATION ON SITE PRIOR TO COMMENCING WORKS.
3. CONFIRM INVERT LEVEL OF ALL CONNECTION POINTS PRIOR TO COMMENCING ANY TRENCHING WORKS.
4. INSPECTION SHAFTS WITHIN THE DRIVEWAY ARE TO BE PROVIDED WITH A TRAFFICABLE COVER BOX FINISHED FLUSH WITH THE FINISHED SURFACE.
5. ALL PITS ARE TO BE FINISHED FLUSH WITH THE FINISHED SURFACE.
6. PROVIDE CLEAN OUT (CO) POINTS ON ALL SUB-SOIL DRAIN UPSTREAM ENDS AND DIRECTION CHANGES. RAISE CLEAN OUT POINTS TO FLUSH WITH FINISHED SURFACE LEVELS WITH SCREW CAP LOCATED WITHIN PATH APPROVED BOX.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS SW MH  
LID: 75.71

EXS SW CONNECTION  
LID: 75.53,  
IL: 74.26  
  
ATLAN ECOCEPTOR SERIES 1500  
STORMWATER TREATMENT DEVICE TO  
MANUFACTURER'S SPECIFICATIONS  
LID: 75.50  
IL: 74.27

EXS SW MH  
LID: 75.54

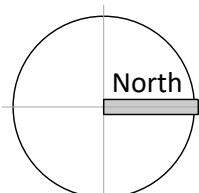
COMMERCE DRIVE

17 COMMERCE DRIVE

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FOR CONSTRUCTION



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HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT STORMWATER PLAN

0	50mm	SCALE 1:100@A1
DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C040	REVISION A

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NOTES

1. PUBLIC SEWER INFRASTRUCTURE WORKS SHALL COMPLY WITH THE REQUIREMENTS DETAILED ON DRG C001.
2. PRIVATE SEWER INFRASTRUCTURE WORKS SHALL COMPLY WITH AS/NZS 3500.2, THE NCC AND ANY OTHER AUTHORITY HAVING JURISDICTION OVER THE INSTALLATION.
3. THE LOCATION OF SOME EXISTING SERVICES IS BASED ON DIAL BEFORE YOU DIG INFORMATION. THE CONTRACTOR IS TO CONFIRM THE ACTUAL LOCATION ON SITE PRIOR TO COMMENCING WORKS.
4. CONFIRM INVERT LEVELS OF ALL CONNECTION POINTS PRIOR TO COMMENCING ANY TRENCHING WORKS.
5. INSPECTION SHAFTS WITHIN THE DRIVEWAY ARE TO BE PROVIDED WITH A TRAFFICABLE COVER BOX FINISHED FLUSH WITH THE SURFACE.
6. ALL ACCESS COVERS, INCLUDING COVER BOXES, SHALL COMPLY WITH AS 3996.2019.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS SEWER MH.  
LID: 75.66,  
IL IN: 73.87,  
IL OUT: 73.95.

LOCATE AND CONNECT TO EXS SEWER CONNECTION  
WITH INSPECTION OPENING RAISED TO SURFACE IN  
ACCORDANCE WITH MRWA-S-302  
LID: 75.51,  
IL: 73.58

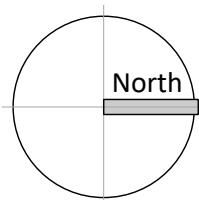
COMMERCE DRIVE

17 COMMERCE DRIVE

REV	DESCRIPTION	APP'D	DATE
A	DEVELOPMENT APPROVAL	AC	11.12.2023

REV	DESCRIPTION	APP'D	DATE

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mail@gandyandroberts.com.au  
ph 03 6223 8877

HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT SEWER PLAN

DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C050	REVISION A



NOTE

1. PUBLIC WATER INFRASTRUCTURE WORKS SHALL COMPLY WITH THE REQUIREMENTS DETAILED ON DRG C001.
2. PRIVATE WATER INFRASTRUCTURE SHALL COMPLY WITH AS/NZS 3500.1, THE NCC AND ANY OTHER AUTHORITY HAVING JURISDICTION OVER THE INSTALLATION.
3. ALL ACCESS COVERS, INCLUDING COVER BOXES, SHALL COMPLY WITH AS 3996:2019.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS FH  
LID: 75.76

EXS SV  
LID: 75.69

LOCATE AND CONNECT TO EXS DN50  
PROPERTY CONNECTION AND DN20 METER  
LID: 75.67

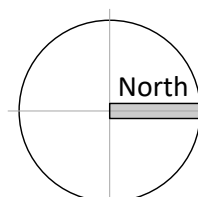
DN32 (ID 26.5) PE SDR 11 WATER PIPE

DN50 PVC-O TASMAT WATER MAIN

COMMERCE DRIVE

17 COMMERCE DRIVE

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ph 03 6223 8877

HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA, 7018  
DRAWING TITLE  
CONCEPT WATER PLAN

0 50mm			SCALE 1:100@A1
DESIGNED ALC	DRAWN ALC	CHECKED AC	
PROJECT 23.0407	DRAWING C060	REVISION A	

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TYPICAL SHARED SPACE  
IN GROUND BOLLARD  
SCALE 1:20

## UNDERGROUND DETENTION GENERAL ARRANGEMENT

NOTE:

- 1 THE SLIP RESISTANCE OF DISABLED PARKING SPACE LINE MARKING SHALL BE A MINIMUM OF CLASS P4 (WET PENDULUM TEST) AS SPECIFIED IN AS/NZS 4586.
- 2 ASPHALT SURFACES SHALL BE FULLY CURED BEFORE APPLYING PAINT.
- 3 ALL WATER BASED ACRYLIC LINE MARKING PAINT SHALL BE APPLIED TO ACHIEVE 300 MICRON DFT.
- 4 REPAPLY PAINT 4 WEEKS AFTER PRACTICAL COMPLETION IS REACHED IT SHALL BE ASSUMED THAT THE CAR PARK IS OPERATIONAL AND ALL ALLOWANCES NECESSARY TO UNDERTAKE THIS WORK SHALL BE INCLUDED IN THE CONTRACT SUM

## WHEEL STOP SET OUT

## TYPICAL ASPHALT PAVEMENT DETAIL

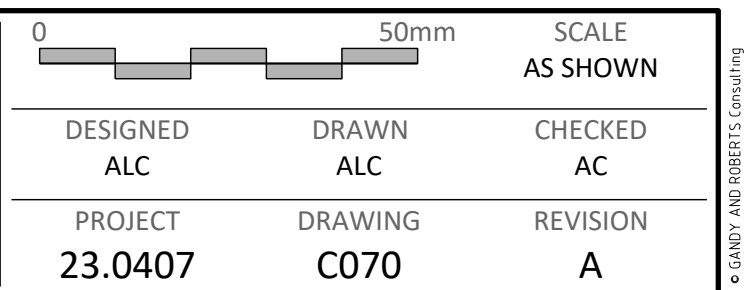
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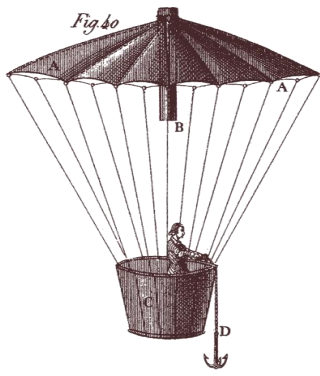
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ph 03 6223 8877

HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT DETAILS





NOTES

1. LOCATION OF SOME EXISTING SERVICES BASED ON DIAL BEFORE YOU DIG INFORMATION CONTRACTOR TO CONFIRM ACTUAL LOCATION ON SITE PRIOR TO COMMENCING WORKS.
2. DRIVEWAY LAYOUT DESIGN BY THE ARCHITECT (BROWN FALCONER)
3. LINE MARKING, WHERE INDICATED, DELINEATING PARKING SPACES TO BE 80mm WIDE USING YELLOW WATER BASED ACRYLIC LINE MARKING PAINT.
4. PARKING SPACE NUMBERING OR VISITOR PARKING DESIGNATION, WHERE INDICATED, TO BE MIN. 200mm HIGH TEXT USING YELLOW WATER BASED ACRYLIC LINE MARKING PAINT.
5. PROVIDE WHEEL STOPS, WHERE INDICATED, TO AS/NZS 2890.1.
6. JOINTS ON CURVES TO BE PERPENDICULAR TO EDGE AND MINIMUM 450 LONG BEFORE CHANGE OF DIRECTION.
7. ALL PITS TO BE FINISHED FLUSH WITH SURFACE LEVEL.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

DISABLED PARKING SPACE TO DETAIL ON C070.

FLUSH KERB TO LGAT TSD-R14-v3

PROVIDE 1650 x 150 x 100 WHEEL STOPS AS SHOWN.  
REFER TO C070 FOR SET OUT.

PROVIDE LINE MARKING TO BAYS.  
REFER NOTE 4.

EXS DRIVEWAY CROSSOVER AND APRON  
TO BE MAINTAINED.  
PROVIDE SMOOTH TRANSITION BETWEEN  
NEW AND EXS CROSSOVER SURFACE.

ASPHALT PAVEMENT TO DETAIL ON C070

600mm PARKING OVERHANG FROM FOK

TYPE KC KERB AND CHANNEL TO LGAT TSD-R14-v3

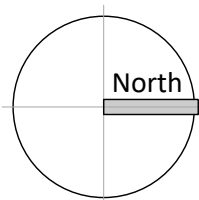
17 COMMERCE DRIVE

COMMERCE DRIVE

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ph 03 6223 8877

HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT SITEWORKS PLAN

0 50mm SCALE 1:100@A1		
DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C020	REVISION A

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Engineers



NOTES

1. ALL PRIVATE STORMWATER INFRASTRUCTURE SHALL COMPLY WITH AS/NZS 3500.3, THE NCC AND ANY OTHER AUTHORITY HAVING JURISDICTION OVER THE INSTALLATION
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5. ALL PITS ARE TO BE FINISHED FLUSH WITH THE FINISHED SURFACE.
6. PROVIDE CLEAN OUT (CO) POINTS ON ALL SUB-SOIL DRAIN UPSTREAM ENDS AND DIRECTION CHANGES. RAISE CLEAN OUT POINTS TO FLUSH WITH FINISHED SURFACE LEVELS WITH SCREW CAP LOCATED WITHIN PATH APPROVED BOX.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS SW MH  
LID: 75.71

EXS SW CONNECTION  
LID: 75.53,  
IL: 74.26  
  
ATLAN ECOCEPTOR SERIES 1500  
STORMWATER TREATMENT DEVICE TO  
MANUFACTURER'S SPECIFICATIONS  
LID: 75.50  
IL: 74.27

EXS SW MH  
LID: 75.54

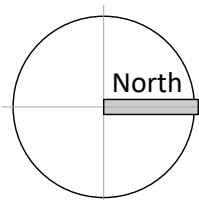
COMMERCE DRIVE

17 COMMERCE DRIVE

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ph 03 6223 8877

HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT STORMWATER PLAN

0	50mm	SCALE 1:100@A1
DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C040	REVISION A

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NOTES

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27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS SEWER MH  
LID: 75.66,  
IL IN: 73.87,  
IL OUT: 73.95.

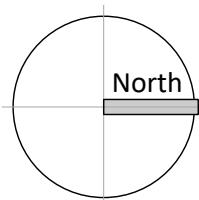
LOCATE AND CONNECT TO EXS SEWER CONNECTION  
WITH INSPECTION OPENING RAISED TO SURFACE IN  
ACCORDANCE WITH MRWA-S-302  
LID: 75.51,  
IL: 73.58

COMMERCE DRIVE

17 COMMERCE DRIVE

REV	DESCRIPTION	APP'D	DATE	REV	DESCRIPTION	APP'D	DATE
A	DEVELOPMENT APPROVAL	AC	11.12.2023				

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ph 03 6223 8877

HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT SEWER PLAN

DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C050	REVISION A

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NOTE

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27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS FH  
LID: 75.76

EXS SV  
LID: 75.69

LOCATE AND CONNECT TO EXS DN50  
PROPERTY CONNECTION AND DN20 METER  
LID: 75.67

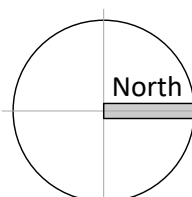
DN32 (ID 26.5) PE SDR 11 WATER PIPE

DN50 PVC-O TASMAT WATER MAIN

COMMERCE DRIVE

17 COMMERCE DRIVE

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FOR CONSTRUCTION



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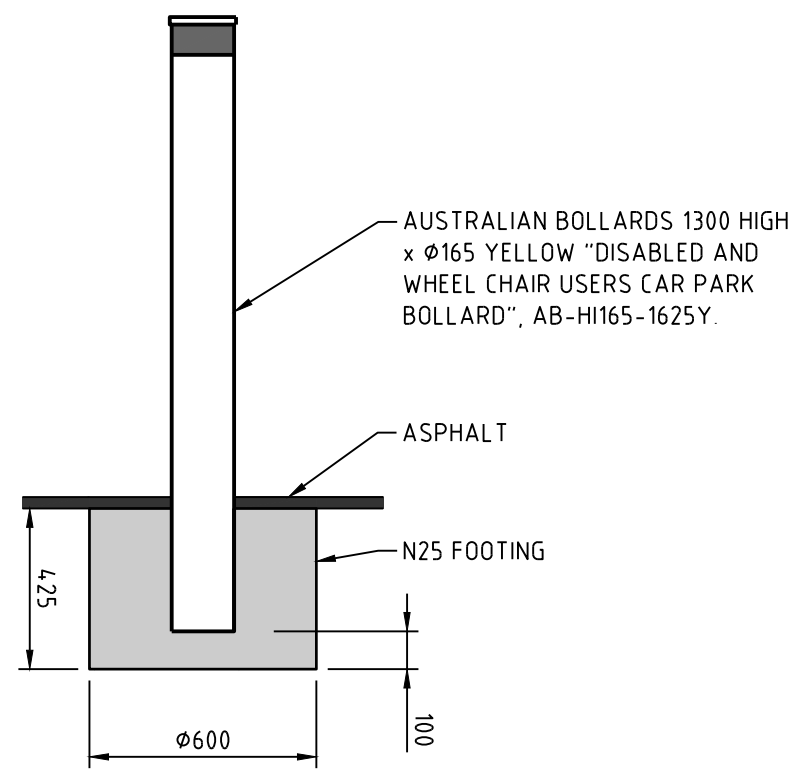
159 DAVEY ST, HOBART  
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ph 03 6223 8877

HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA, 7018  
DRAWING TITLE  
CONCEPT WATER PLAN

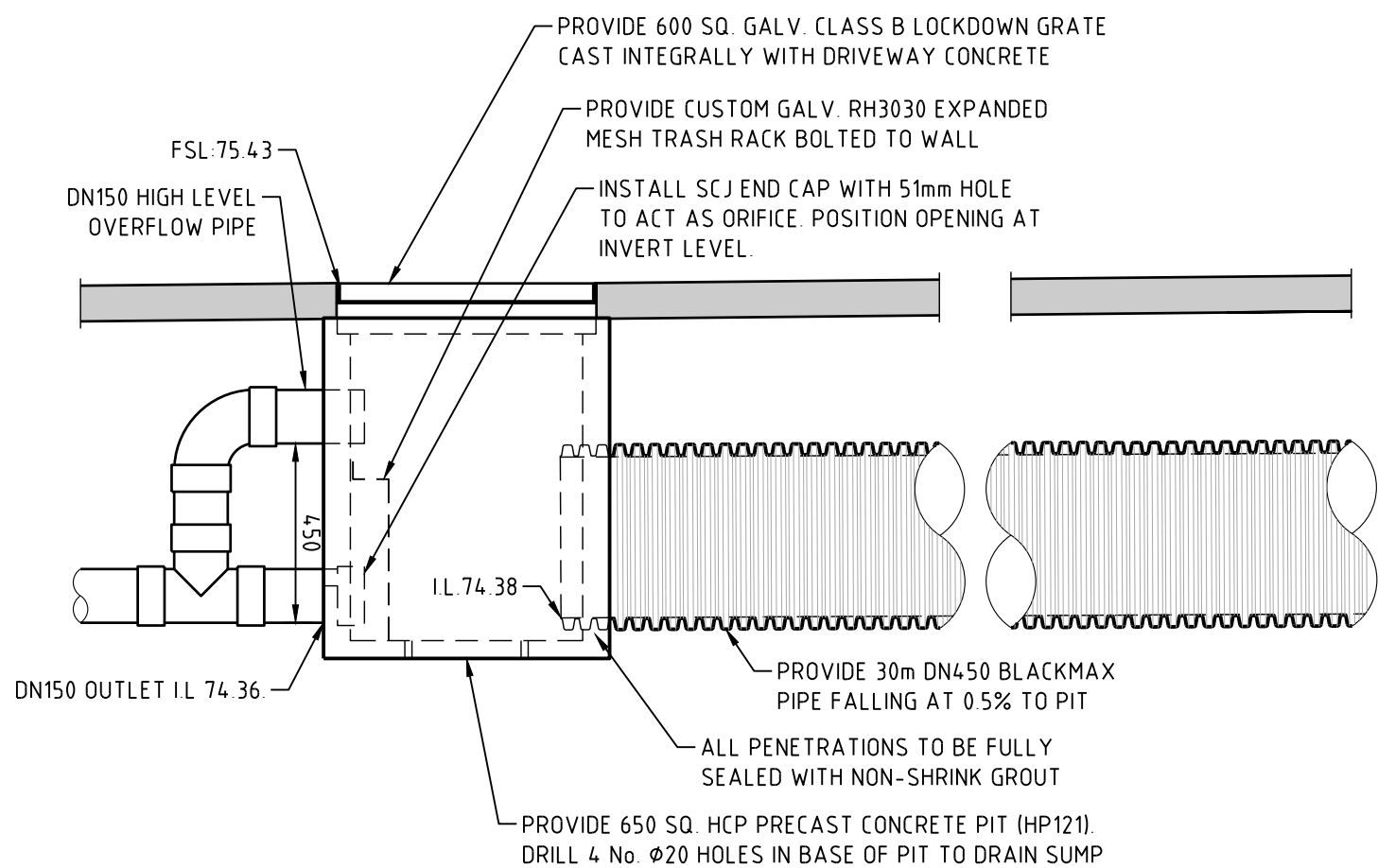
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DESIGNED ALC	DRAWN ALC	CHECKED AC	
PROJECT 23.0407	DRAWING C060	REVISION A	

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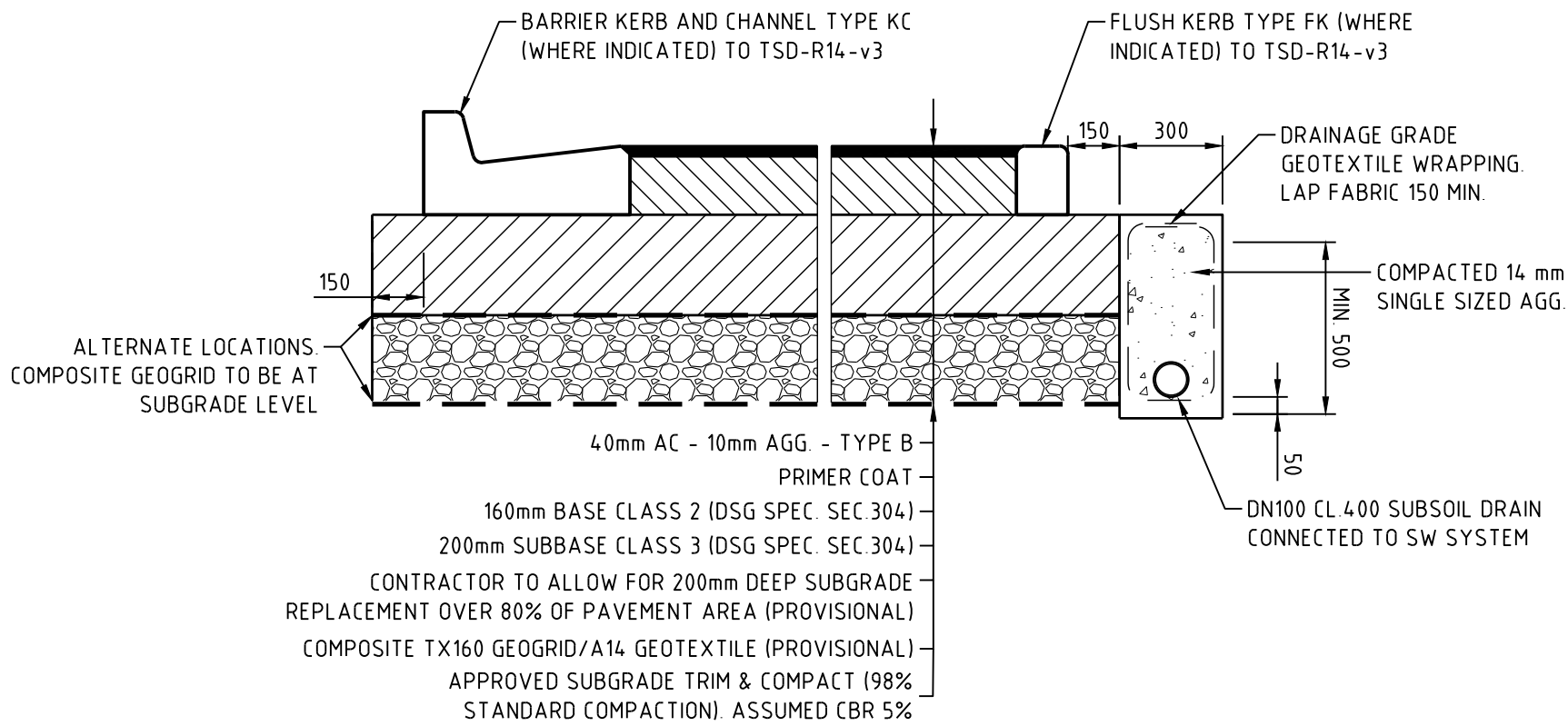
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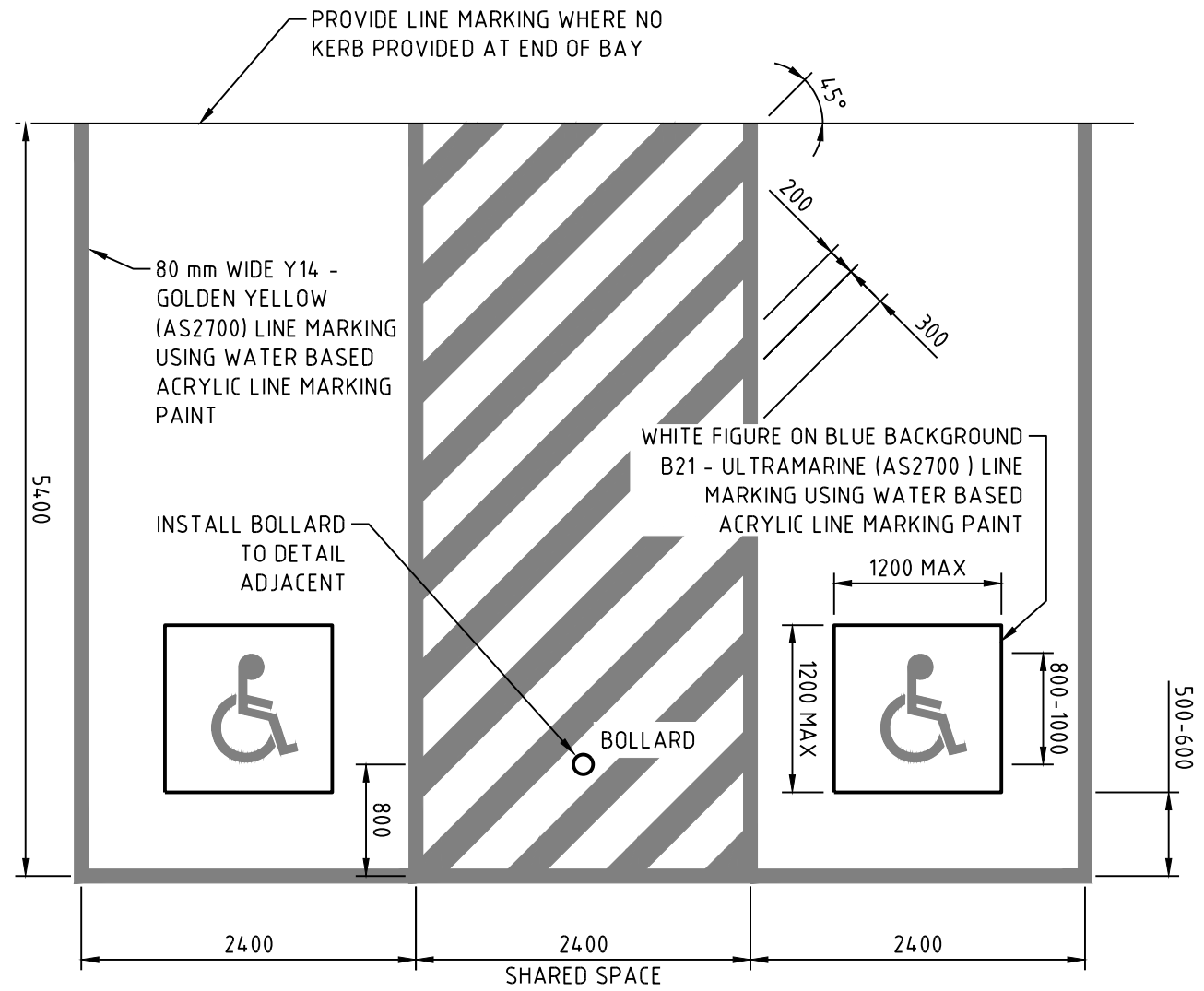
**TYPICAL SHARED SPACE  
IN GROUND BOLLARD**  
SCALE 120



**UNDERGROUND DETENTION GENERAL ARRANGEMENT**  
SCALE 120

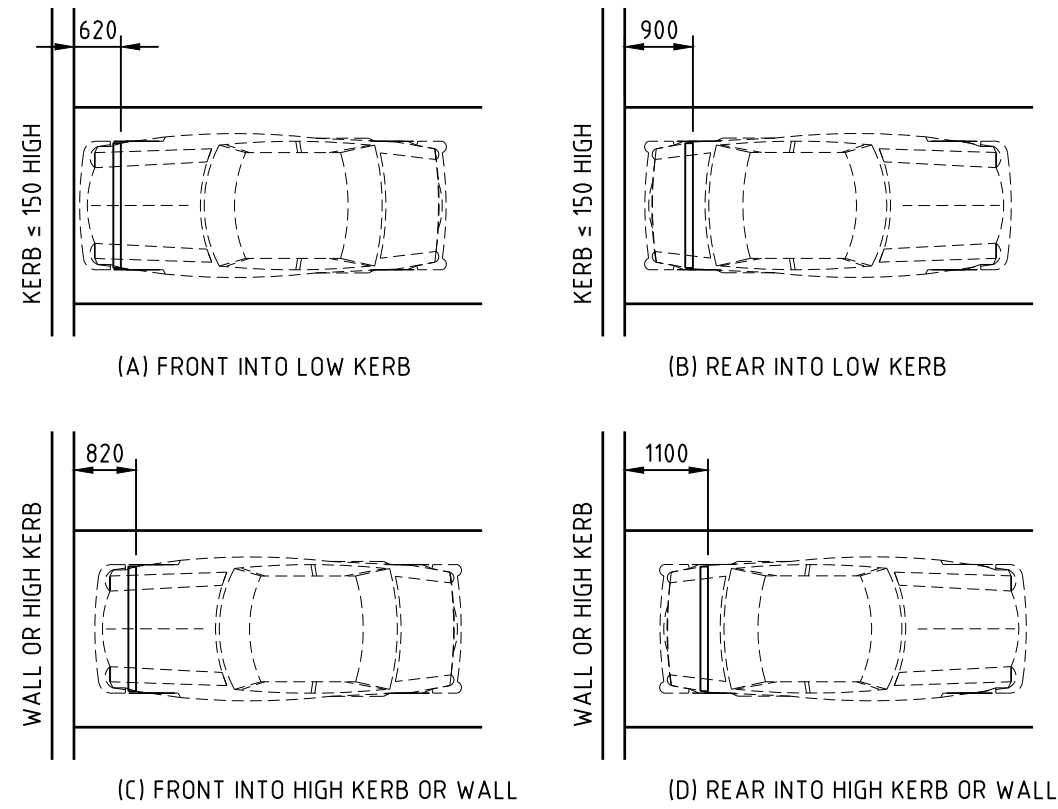


**TYPICAL ASPHALT PAVEMENT DETAIL**  
SCALE 120



- NOTE:
1. THE SLIP RESISTANCE OF DISABLED PARKING SPACE LINE MARKING SHALL BE A MINIMUM OF CLASS P4 (WET PENDULUM TEST) AS SPECIFIED IN AS/NZS 4586.
  2. ASPHALT SURFACES SHALL BE FULLY CURED BEFORE APPLYING PAINT.
  3. ALL WATER BASED ACRYLIC LINE MARKING PAINT SHALL BE APPLIED TO ACHIEVE 300 MICRON DFT.
  4. REAPPLY PAINT 4 WEEKS AFTER PRACTICAL COMPLETION IS REACHED. IT SHALL BE ASSUMED THAT THE CAR PARK IS OPERATIONAL AND ALL ALLOWANCES NECESSARY TO UNDERTAKE THIS WORK SHALL BE INCLUDED IN THE CONTRACT SUM.

**DISABLED PARKING SPACE LINE MARKING**  
SCALE 150



**WHEEL STOP SET OUT**  
NTS

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HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT DETAILS

SCALE AS SHOWN		
DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C070	REVISION A

NOTES

1. LOCATION OF SOME EXISTING SERVICES BASED ON DIAL BEFORE YOU DIG INFORMATION CONTRACTOR TO CONFIRM ACTUAL LOCATION ON SITE PRIOR TO COMMENCING WORKS.
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6. JOINTS ON CURVES TO BE PERPENDICULAR TO EDGE AND MINIMUM 450 LONG BEFORE CHANGE OF DIRECTION.
7. ALL PITS TO BE FINISHED FLUSH WITH SURFACE LEVEL.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

DISABLED PARKING SPACE TO DETAIL ON C070.

FLUSH KERB TO LGAT TSD-R14-v3

PROVIDE 1650 x 150 x 100 WHEEL STOPS AS SHOWN.  
REFER TO C070 FOR SET OUT.

PROVIDE LINE MARKING TO BAYS.  
REFER NOTE 4.

EXS DRIVEWAY CROSSOVER AND APRON  
TO BE MAINTAINED.  
PROVIDE SMOOTH TRANSITION BETWEEN  
NEW AND EXS CROSSOVER SURFACE.

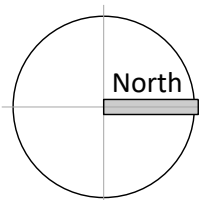
ASPHALT PAVEMENT TO DETAIL ON C070

600mm PARKING OVERHANG FROM FOK

TYPE KC KERB AND CHANNEL TO LGAT TSD-R14-v3

17 COMMERCE DRIVE

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HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT SITEWORKS PLAN

0 50mm

SCALE  
1:100@A1

DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C020	REVISION A

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NOTES

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27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS SW MH  
LID: 75.71

EXS SW CONNECTION  
LID: 75.53,  
IL: 74.26  
  
ATLAN ECOCEPTER SERIES 1500  
STORMWATER TREATMENT DEVICE TO  
MANUFACTURER'S SPECIFICATIONS  
LID: 75.50  
IL: 74.27

EXS SW MH  
LID: 75.54

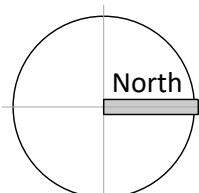
COMMERCE DRIVE

17 COMMERCE DRIVE

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A	DEVELOPMENT APPROVAL	AC	11.12.2023				

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HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT STORMWATER PLAN

0	50mm	SCALE 1:100@A1
DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C040	REVISION A

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Engineers

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CHILDCARE CENTRE  
FFL: TBC

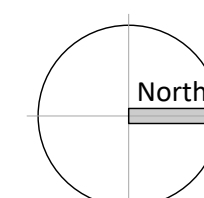
EXS SEWER MH.  
LID: 75.66,  
IL IN: 73.87,  
IL OUT: 73.95.

DN100 PVC SEWER PIPE AT MIN 1.65%

LOCATE AND CONNECT TO EXS SEWER CONNECTION-  
WITH INSPECTION OPENING RAISED TO SURFACE IN  
ACCORDANCE WITH MRWA-S-302.  
LID: 75.51,  
IL: 73.58.

CUMMERCE DRIVE

17 COMMERCE DRIVE



**GANDY AND  
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HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA 7018  
DRAWING TITLE  
CONCEPT SEWER PLAN



DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C050	REVISION A

Engineers.

02/11/2023 9:56:09 AM S:\Projects\2023\23-04-07 Glebe Hill Childcare15 - GR drawings, model\23-04-07.dwg



NOTE

1. PUBLIC WATER INFRASTRUCTURE WORKS SHALL COMPLY WITH THE REQUIREMENTS DETAILED ON DRG C001.
2. PRIVATE WATER INFRASTRUCTURE SHALL COMPLY WITH AS/NZS 3500.1, THE NCC AND ANY OTHER AUTHORITY HAVING JURISDICTION OVER THE INSTALLATION.
3. ALL ACCESS COVERS, INCLUDING COVER BOXES, SHALL COMPLY WITH AS 3996:2019.

27 COMMERCE DRIVE

CHILDCARE CENTRE  
FFL: TBC

EXS FH  
LID: 75.76

EXS SV  
LID: 75.69

LOCATE AND CONNECT TO EXS DN50  
PROPERTY CONNECTION AND DN20 METER  
LID: 75.67

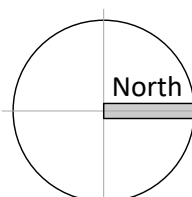
DN32 (ID 26.5) PE SDR 11 WATER PIPE

DN50 PVC-O TASMAT WATER MAIN

COMMERCE DRIVE

17 COMMERCE DRIVE

THIS DRAWING HAS NOT BEEN APPROVED  
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ENGINEERS

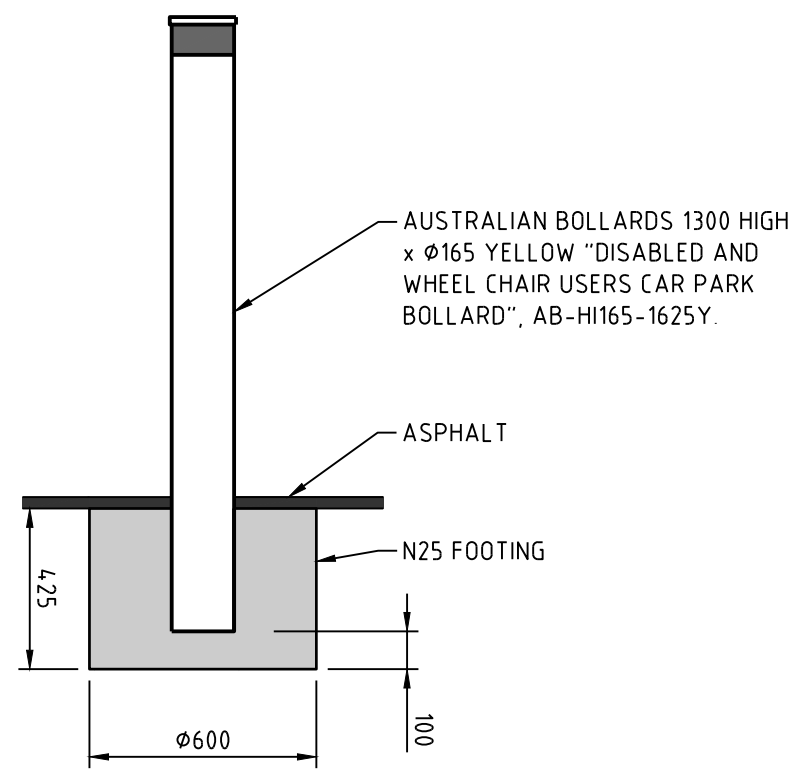
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HOWRAH CHILDCARE CENTRE  
21 COMMERCE DRIVE, HOWRAH  
TASMANIA, 7018  
DRAWING TITLE  
CONCEPT WATER PLAN

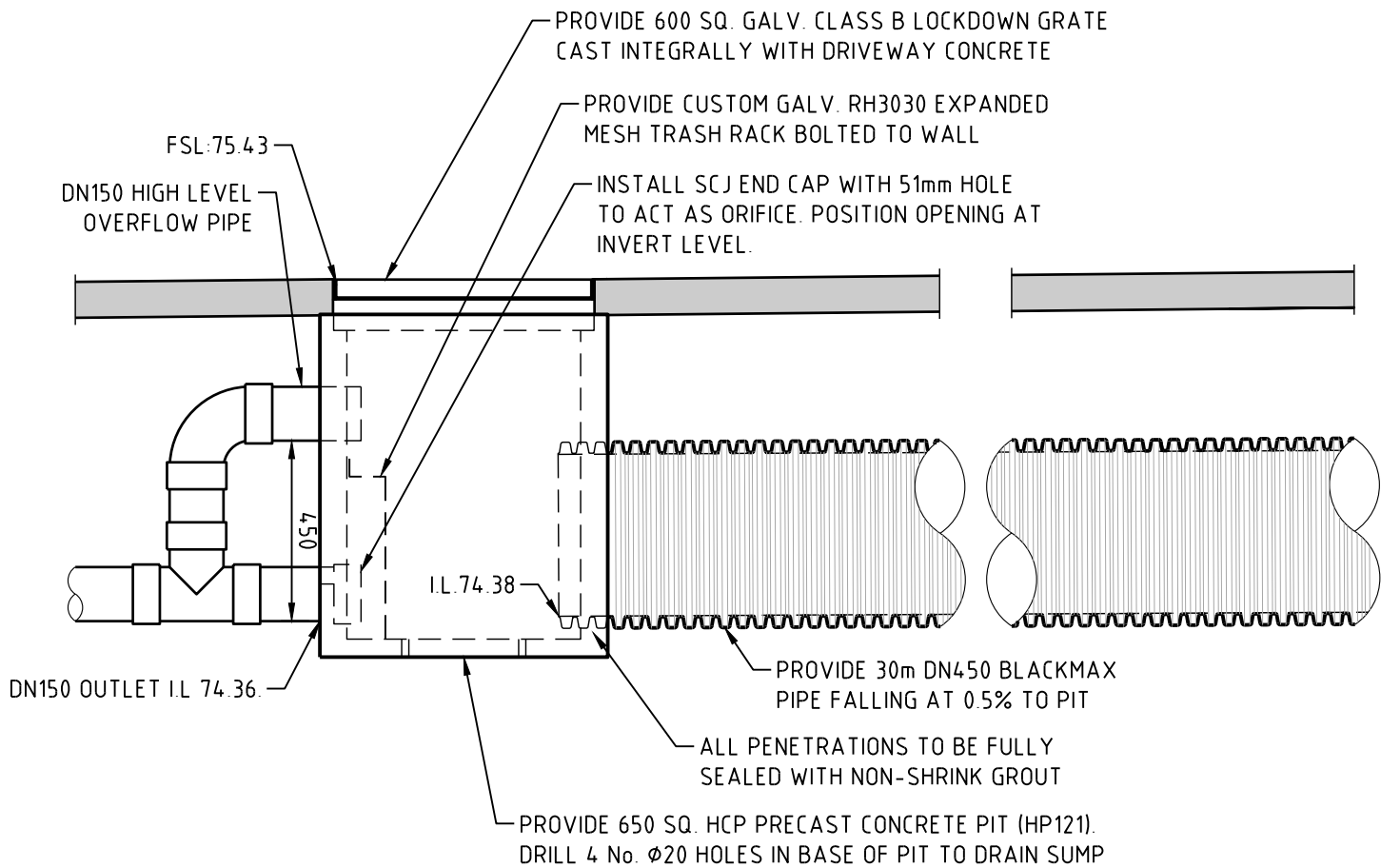
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PROJECT 23.0407	DRAWING C060	REVISION A

GANDY AND ROBERTS Consulting  
Engineers

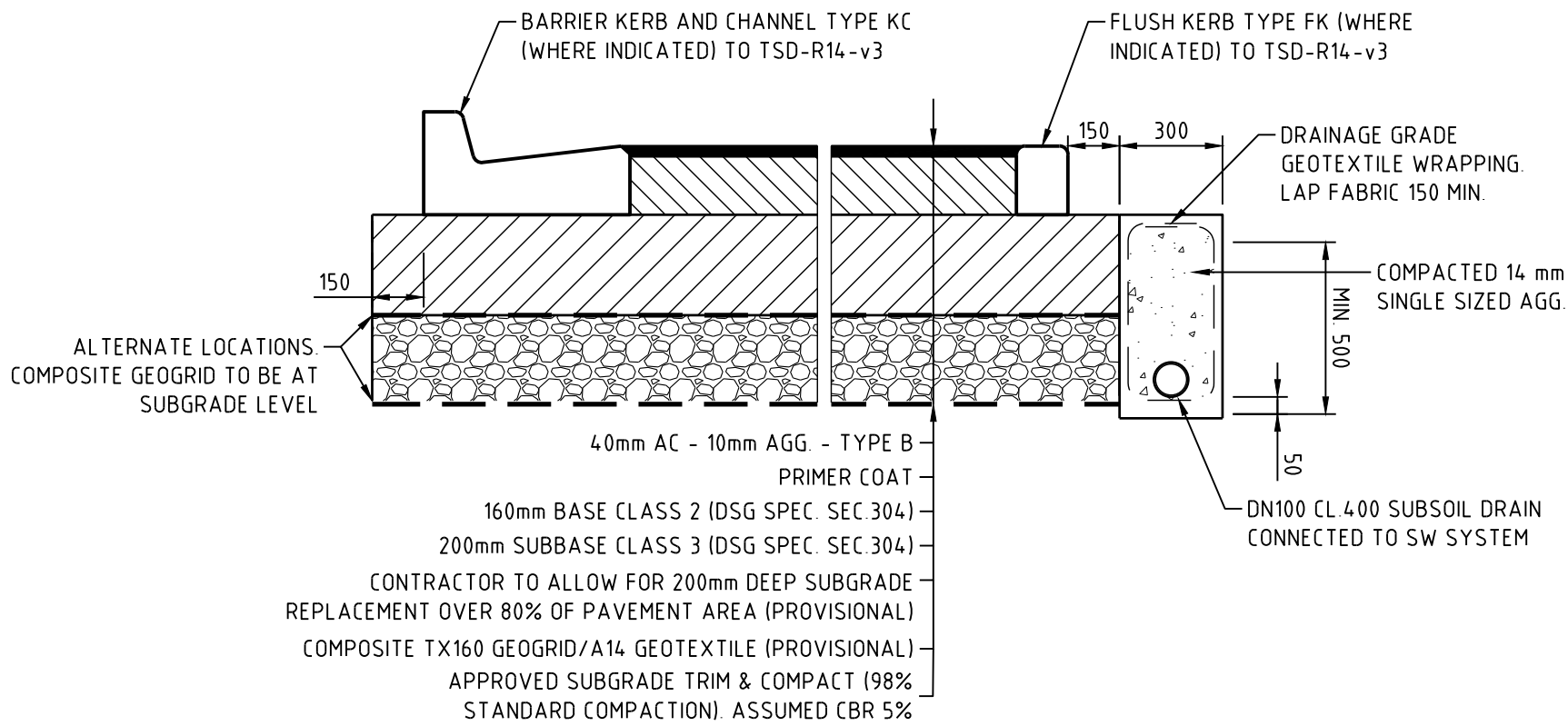
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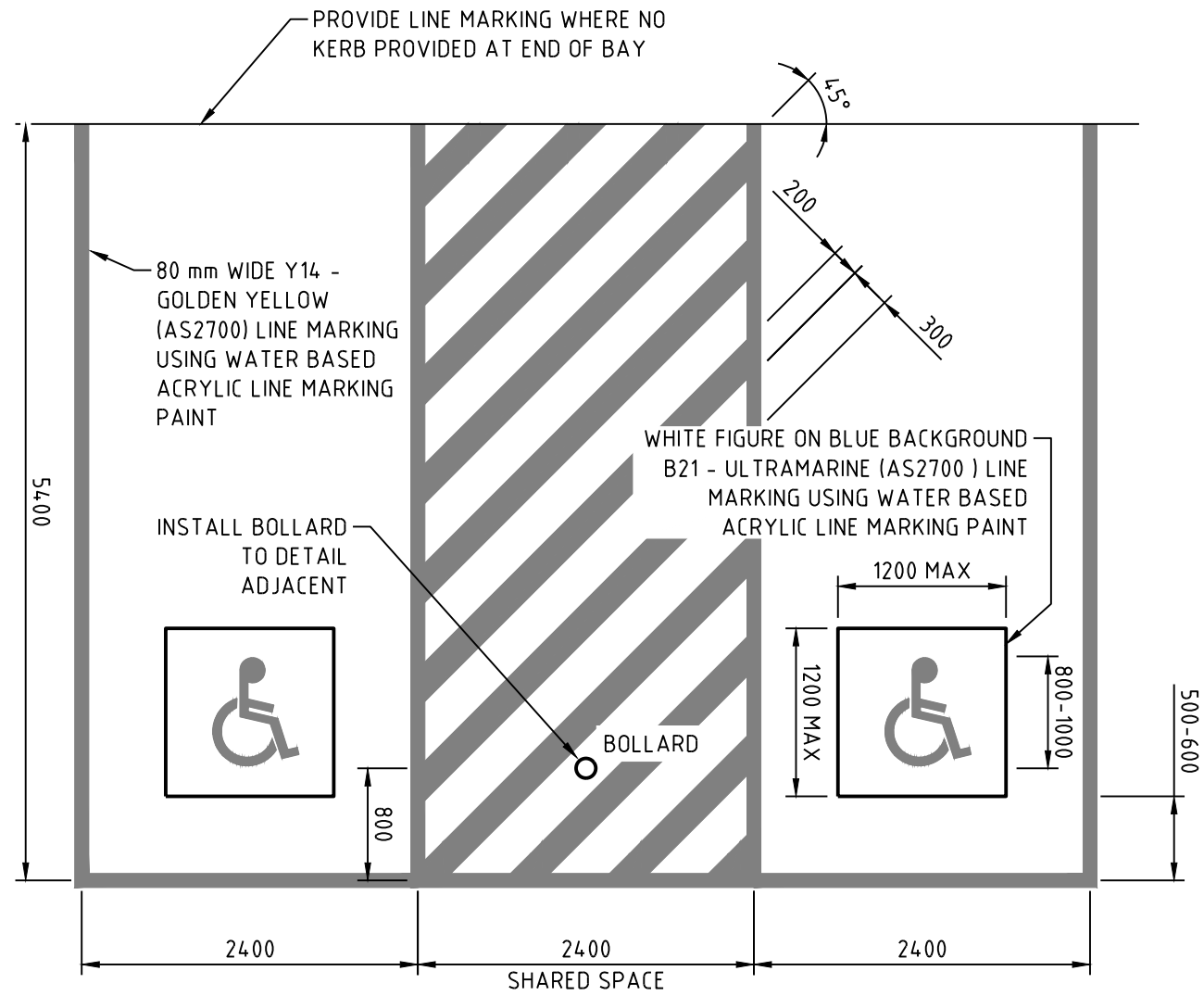
TYPICAL SHARED SPACE  
IN GROUND BOLLARD  
SCALE 120



UNDERGROUND DETENTION GENERAL ARRANGEMENT  
SCALE 120

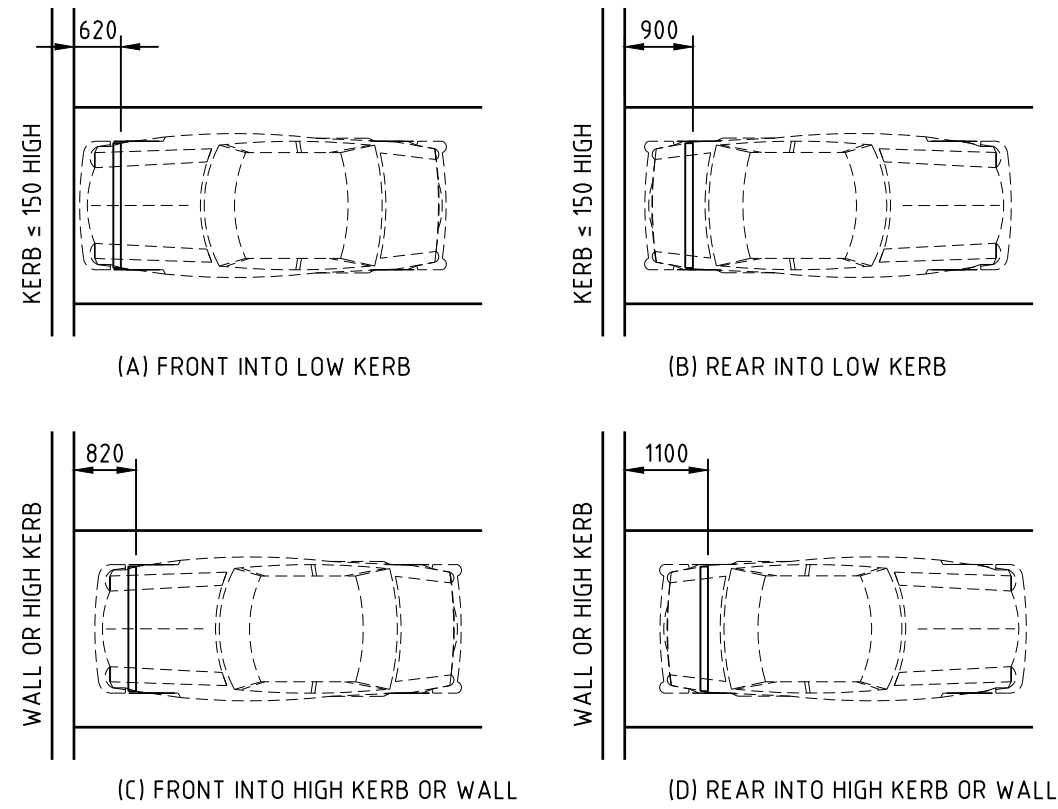


TYPICAL ASPHALT PAVEMENT DETAIL  
SCALE 120



- NOTE:
1. THE SLIP RESISTANCE OF DISABLED PARKING SPACE LINE MARKING SHALL BE A MINIMUM OF CLASS P4 (WET PENDULUM TEST) AS SPECIFIED IN AS/NZS 4586.
  2. ASPHALT SURFACES SHALL BE FULLY CURED BEFORE APPLYING PAINT.
  3. ALL WATER BASED ACRYLIC LINE MARKING PAINT SHALL BE APPLIED TO ACHIEVE 300 MICRON DFT.
  4. REAPPLY PAINT 4 WEEKS AFTER PRACTICAL COMPLETION IS REACHED. IT SHALL BE ASSUMED THAT THE CAR PARK IS OPERATIONAL AND ALL ALLOWANCES NECESSARY TO UNDERTAKE THIS WORK SHALL BE INCLUDED IN THE CONTRACT SUM.

DISABLED PARKING SPACE LINE MARKING  
SCALE 150



WHEEL STOP SET OUT  
NTS

REV	DESCRIPTION	APP'D	DATE	REV	DESCRIPTION	APP'D	DATE
A	DEVELOPMENT APPROVAL	AC	11.12.2023				

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CONCEPT DETAILS

DESIGNED ALC	DRAWN ALC	CHECKED AC
PROJECT 23.0407	DRAWING C070	REVISION A

Attention: Monica Cameron

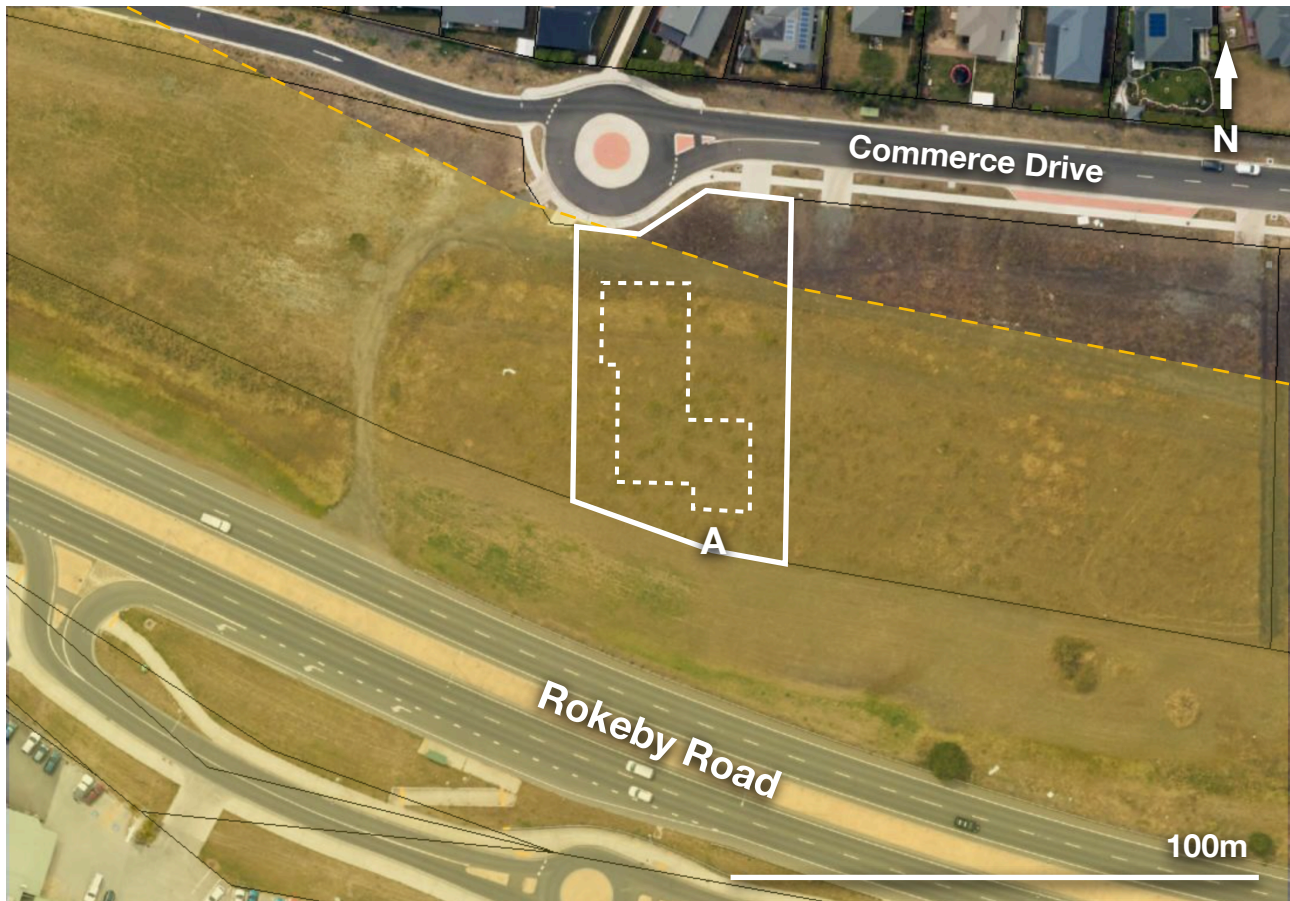
## 21 COMMERCE DRIVE — NOISE IMPACT ASSESSMENT

A childcare centre is proposed for construction at 21 Commerce Drive, Howrah. The site is located within a Road and Railway Attenuation zone under the Planning Scheme, and thus a noise assessment is required to ensure that the site and building design is appropriate for its proposed use. This letter presents the results of such a noise assessment, completed by NVC in December 2023.

### 1. BACKGROUND

The proposed site is an existing vacant block, located at 21 Commerce Drive, Howrah, and is shown by the solid white outline in Figure 1.1. The proposal includes a building (approximate footprint shown by the broken white outline) comprising activity / care rooms, amenities, a reception area, administrative areas, outdoor learning areas, and parking spaces for nominally 24 vehicles. The yellow overlay shown in Figure 1.1 indicates the Road and Railway Attenuation zone.

The site is on a relatively flat block, with a gentle gradient down toward the east. Rokeby Road is located to the south, nominally 30m from the site boundary. Note that Rokeby Road is relatively level, and thus due to the gradient of site, the height of the roadway increases relative to the site as the road continues to the south-east. The sign-posted speed limit Rokeby Road immediately adjacent site is 80km/h, with a large controlled intersection to the south-east of site.



**FIGURE 1.1: SITE AND SURROUNDINGS**



Figure 1.2, below, shows the proposed layout of the childcare centre. It is noted that the outdoor space extends around the entirety of the building, and thus is exposed to noise emissions from vehicles on Rokeby Road.



**FIGURE 1.2: PROPOSED SITE LAYOUT**



## 2. CRITERIA

The Tasmanian Planning Scheme - Clarence (the Scheme) contains, under section C3.0, the Road and Railways Assets Code. Specifically relevant is clause C3.6.1 which is shown below:

### C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area

Objective:	To minimise the effects of noise, vibration, light and air emissions on sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.	
Acceptable Solutions	Performance Criteria	
<p><b>A1</b></p> <p>Unless within a building area on a sealed plan approved under this planning scheme, habitable buildings for a sensitive use within a road or railway attenuation area, must be:</p> <ul style="list-style-type: none"> <li>(a) within a row of existing habitable buildings for sensitive uses and no closer to the existing or future major road or rail network than the adjoining habitable building;</li> <li>(b) an extension which extends no closer to the existing or future major road or rail network than: <ul style="list-style-type: none"> <li>(i) the existing habitable building; or</li> <li>(ii) an adjoining habitable building for a sensitive use; or</li> </ul> </li> <li>(c) located or designed so that external noise levels are not more than the level in Table C3.2 measured in accordance with Part D of the <i>Noise Measurement Procedures Manual</i>, 2<sup>nd</sup> edition, July 2008.</li> </ul>	<p><b>P1</b></p> <p>Habitable buildings for sensitive uses within a road or railway attenuation area, must be sited, designed or screened to minimise adverse effects of noise, vibration, light and air emissions from the existing or future major road or rail network, having regard to:</p> <ul style="list-style-type: none"> <li>(a) the topography of the site;</li> <li>(b) the proposed setback;</li> <li>(c) any buffers created by natural or other features;</li> <li>(d) the location of existing or proposed buildings on the site;</li> <li>(e) the frequency of use of the rail network;</li> <li>(f) the speed limit and traffic volume of the road;</li> <li>(g) any noise, vibration, light and air emissions from the rail network or road;</li> <li>(h) the nature of the road;</li> <li>(i) the nature of the development;</li> <li>(j) the need for the development;</li> <li>(k) any traffic impact assessment;</li> <li>(l) any mitigating measures proposed;</li> <li>(m) any recommendations from a suitably qualified person for mitigation of noise; and</li> <li>(n) any advice received from the rail or road authority.</li> </ul>	

Table C3.2 of the Scheme specifies acceptable noise levels within a road or railway attenuation area as follows:

Roads	Railways
The arithmetic average of the A-weighted L10 sound pressure levels for each of the one-hour periods between 6:00am and midnight on any day [L10 (18-hour)] of 63 dB(A).	A 24-hour Leq and Lmax noise level of 65 dB(A) and 87dB(A) Lmax assessed as a single event maximum sound pressure level.

Therefore, the relevant criterion for compliance with the Acceptable Solution under clause C3.6.1-A1 of the Scheme is as follows:

**63 dBA L10<sub>(18-hour)</sub> at the site boundary.**

Whilst achieving compliance with the above will result in compliance with the Scheme, additional relevant standards are deemed prudent to refer to, to ensure the design provides a high level of acoustic amenity for future occupants.

The Association of Australasian Acoustical Consultants (AAAC) publish a Guideline for Child Care Centre Acoustic Assessment<sup>1</sup> (the AAAC Guidelines) which outlines design sound levels for noise emissions from road traffic, rail or industry. Specifically, section 5.1 states the following:

*"The  $L_{Aeq,1hr}$  noise level from road traffic, rail or industry at any location within the outdoor play or activity area during the hours when the centre is operating should not exceed 55 dB(A).*

*The  $L_{Aeq,1hr}$  noise level from road traffic, rail or industry at any location within the indoor activity or sleeping areas of the Centre during the hours when the centre is operating shall be capable (ie doors and / or windows closed) of achieving 40 dB(A) within indoor activity areas and 35 dB(A) in sleeping areas."*

As such, the following design criteria are proposed to ensure that the building design provides a high level of acoustic amenity:

**55 dBA Leq at outdoor activity areas.**

**40 dBA Leq at indoor activity areas.**

**35 dBA Leq at sleeping areas.**

**3.**

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<sup>1</sup> 'Guideline for Child Care Centre Acoustic Assessment (V3.0)', AAAC, September 2020

## 4. NOISE MEASUREMENTS

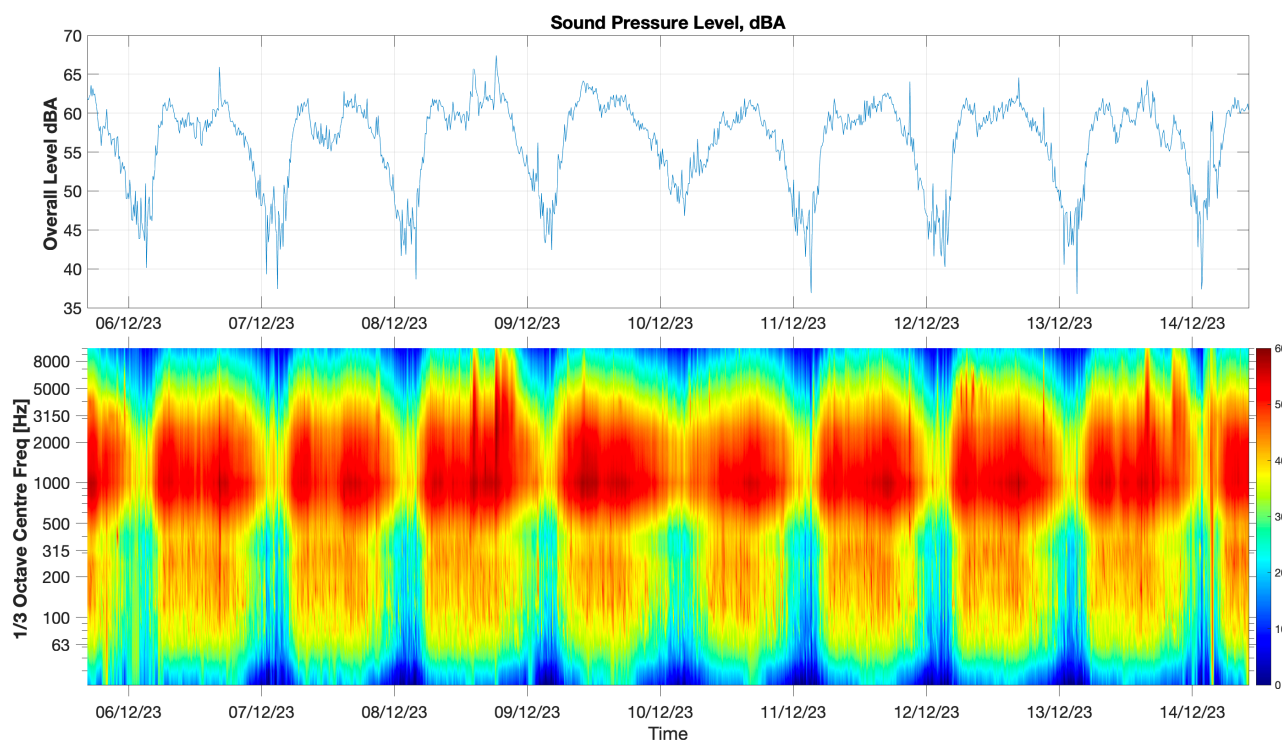
Unattended noise measurements were made on site between the 5<sup>th</sup> and the 14<sup>th</sup> December 2023 to quantify noise emissions from traffic on Rokeby Road. The measurements were made at location A (see Figure 1.1), which is the nearest boundary to Rokeby Road, approximately 27m from the road verge. Measurements used a Svan Type 1 sound level meter, logging in A-weighted decibels with a *Fast* response time. The data set comprised overall levels, one-third octave spectra and full statistical data at 10-minute intervals, with spectra and overall level data also recorded at 1s intervals.

The day time (6AM to 10PM) noise levels and L10<sub>(18-hour)</sub> across the measurement period are summarised in Table 3.1.

**TABLE 3.1: SUMMARY OF MEASURED NOISE LEVELS**

	Sound Pressure Level, dBA			
	Day Time (6AM to 10PM)			L10 <sub>(18-hour)</sub>
	L10	L90	Leq	
<b>Location A</b>	63	51	59	65

Figure 3.1, below, shows the measured noise trend at location A. Note that during the day time, noise was predominantly broad-band mid to high frequency noise, typical of high-speed vehicle tyre noise. There is also notable intermittent low-frequency noise throughout the day time, typical of heavy vehicles passing by.



**FIGURE 3.1: MEASURED NOISE SPECTROGRAM - LOCATION A**

The following observations made whilst on site are deemed relevant:

- Rokeby Road was the dominant noise source and was perceived as primarily broad-band noise, typical of high-speed vehicle tyre noise.
- Some low-frequency engine noise was audible when heavy vehicles passed by directly adjacent site on Rokeby Road.
- Some noise from vehicles accelerating was audible from the controlled intersection to the south-east of site.





## 6. ASSESSMENT

Following the implementation of the above recommendations, calculations demonstrate outdoor levels are expected to be below 55 dBA Leq, and indoor levels below 35 dBA Leq. As such, the design, with the inclusion of these recommendations, is predicted to comply with the best-practise design levels under the AAAC Guidelines, and comfortably comply with the requirements under clause C3.6.1-A1 of the Scheme.

Thus, the proposed childcare centre at 21 Commerce Drive, Howrah, demonstrates compliance with clause C3.6.1-A1 of the Tasmanian Planning Scheme - Clarence.

Should you have any queries, please do not hesitate to contact me directly.

Kind regards,



**Jaye Parry**