

COUNCIL MEETING
MONDAY 9 OCTOBER 2023

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BUSINESS TO BE CONDUCTED AT THIS MEETING IS TO BE CONDUCTED IN THE ORDER IN WHICH IT IS SET OUT IN THIS AGENDA UNLESS THE COUNCIL BY ABSOLUTE MAJORITY DETERMINES OTHERWISE

COUNCIL MEETINGS, NOT INCLUDING CLOSED MEETING, ARE AUDIO-VISUALLY RECORDED AND PUBLISHED TO COUNCIL'S WEBSITE

1. ACKNOWLEDGEMENT OF COUNTRY

The Mayor will:

- make the following statement:

“Before proceeding, I pay my respects to the Mumirimina people as the traditional and original custodians of the lands on which we meet, and I acknowledge the continuing connection of the Tasmanian Aboriginal people to the skies, land and waterways.

I pay respect to Elders past and present.”

- invite those present to pause for a moment of quiet reflection and respect before commencing the council meeting.
- advise the Meeting and members of the public that Council Meetings, not including Closed Meeting, are livestreamed, audio-visually recorded and published to Council’s website. The meeting is not protected by privilege. A link to the Agenda is available via Council’s website.

2. APOLOGIES

Nil.

3. DECLARATIONS OF INTERESTS OF COUNCILLORS OR CLOSE ASSOCIATE

In accordance with Regulation 8 of the Local Government (Meeting Procedures) Regulations 2015 and Council’s adopted Code of Conduct, the Mayor requests Councillors to indicate whether they have, or are likely to have a pecuniary interest (any pecuniary benefits or pecuniary detriment) or conflict of interest in any item on the Agenda.

4. OMNIBUS ITEMS**4.1 CONFIRMATION OF MINUTES****RECOMMENDATION:**

That the Minutes of the Council Meeting held on 18 September 2023, as circulated, be taken as read and confirmed.

4.2 MAYOR'S COMMUNICATION**4.3 COUNCIL WORKSHOPS**

In addition to the Councillor's Meeting Briefing (workshop) conducted on Friday immediately preceding the Council Meeting the following workshops were conducted by Council since its last ordinary Council Meeting:

PURPOSE

Little Howrah Beach Master Plan
AFL High Performance Centre
Parking Sensors
LGAT Motions

DATE

25 September

RECOMMENDATION:

That Council notes the workshops conducted.

4.4. TABLING OF PETITIONS

(Note: Petitions received by Councillors are to be forwarded to the Chief Executive Officer within seven days after receiving the petition).

Petitions are not to be tabled if they do not comply with Section 57(2) of the Local Government Act, or are defamatory, or the proposed actions are unlawful.

4.5 REPORTS FROM OUTSIDE BODIES

This agenda item is listed to facilitate the receipt of both informal and formal reporting from various outside bodies upon which Council has a representative involvement.

REPORTS FROM SINGLE AND JOINT AUTHORITIES

Provision is made for reports from Single and Joint Authorities if required.

Council is a participant in the following Single and Joint Authorities. These Authorities are required to provide quarterly reports to participating Councils, and these will be listed under this segment as and when received.

- **COPPING REFUSE DISPOSAL SITE JOINT AUTHORITY**

Representative: Cr James Walker

Quarterly Reports

September Quarterly Report pending.

Representative Reporting

- **SOUTHERN TASMANIAN REGIONAL WASTE AUTHORITY**

Representative: Cr Warren (Mayor's nominee)
Cr Hunter (Proxy)

- **TASWATER CORPORATION**

- **GREATER HOBART COMMITTEE**

REPORTS FROM COUNCIL AND SPECIAL COMMITTEES AND OTHER REPRESENTATIVE BODIES

4.6 WEEKLY BRIEFING REPORTS

The Weekly Briefing Reports of 18 and 25 September and 2 October 2023 have been circulated to Councillors.

RECOMMENDATION:

That the information contained in the Weekly Briefing Reports of 18 and 25 September and 2 October 2023 be noted.

5. PUBLIC QUESTION TIME

Public question time at ordinary Council meetings will not exceed 15 minutes. An individual may ask questions at the meeting. Questions may be submitted to Council in writing on the Friday 10 days before the meeting or may be raised from the Public Gallery during this segment of the meeting.

The Chairman may request a Councillor or Council officer to answer a question. No debate is permitted on any questions or answers. Questions and answers are to be kept as brief as possible.

5.1 PUBLIC QUESTIONS ON NOTICE

(Seven days before an ordinary Meeting, a member of the public may give written notice to the Chief Executive Officer of a question to be asked at the meeting). A maximum of two questions may be submitted in writing before the meeting.

Nil.

5.2 ANSWERS TO QUESTIONS ON NOTICE

The Mayor may address Questions on Notice submitted by members of the public.

Nil.

5.3 ANSWERS TO PREVIOUS QUESTIONS TAKEN ON NOTICE

Nil.

5.4 QUESTIONS WITHOUT NOTICE

The Chairperson may invite members of the public present to ask questions without notice.

Questions are to relate to the activities of the Council. Questions without notice will be dependent on available time at the meeting.

Council Policy provides that the Chairperson may refuse to allow a question on notice to be listed or refuse to respond to a question put at a meeting without notice that relates to any item listed on the agenda for the Council meeting (note: this ground for refusal is in order to avoid any procedural fairness concerns arising in respect to any matter to be determined on the Council Meeting Agenda.

When dealing with Questions without Notice that require research and a more detailed response the Chairman may require that the question be put on notice and in writing. Wherever possible, answers will be provided at the next ordinary Council Meeting.

Council's Public Question Time Policy can be found on Council's website at [Public Question Time - City of Clarence : City of Clarence \(ccc.tas.gov.au\)](https://www.ccc.tas.gov.au/Public-Question-Time)

6. DEPUTATIONS BY MEMBERS OF THE PUBLIC

(In accordance with Regulation 38 of the Local Government (Meeting Procedures) Regulations 2015 and in accordance with Council Policy, deputation requests are invited to address the Meeting and make statements or deliver reports to Council)

7 PLANNING AUTHORITY MATTERS

In accordance with Regulation 25 (1) of the Local Government (Meeting Procedures) Regulations 2015, the Mayor advises that the Council intends to act as a Planning Authority under the Land Use Planning and Approvals Act 1993, to deal with the following items:

7.1 DEVELOPMENT APPLICATION PDPLANPMTD-2023/037817 – 981 CAMBRIDGE ROAD, CAMBRIDGE (CT223817/1 AND CT166751/1) – 9 MULTIPLE DWELLINGS (ONE EXISTING AND EIGHT NEW)**EXECUTIVE SUMMARY****PURPOSE**

The purpose of this report is to consider the application made for nine multiple dwellings (one existing and eight new) at 981 Cambridge Road, Cambridge (CT223817/1 and CT166751/1).

RELATION TO PLANNING PROVISIONS

The site is zoned General Residential and is subject to the Parking and Sustainable Transport Code, the Road and Railways Assets Code and the Safeguarding of Airports Code under the Tasmanian Planning Scheme - Clarence (the Scheme). In accordance with the Scheme, the proposal is a Discretionary development.

LEGISLATIVE REQUIREMENTS

The report on this item details the basis and reasons for the recommendation. Any alternative decision by Council will require a full statement of reasons to maintain the integrity of the Planning approval process and to comply with the requirements of the Judicial Review Act and the Local Government (Meeting Procedures) Regulations 2015.

Council is required to exercise a discretion within the statutory 42-day period which expires on 11 October 2023 as agreed with the applicant.

CONSULTATION

The proposal was advertised in accordance with statutory requirements and two representations were received raising the following issues:

- Boundary fencing;
- Infrastructure capacity;
- Traffic congestion and vehicle and pedestrian safety;
- Anticipated daily vehicle movements to and from the site;
- Headlight glare;
- Impact on school pedestrian crossing;
- Emergency vehicle access;
- Waste collection arrangements;
- Location of pedestrian and parking lighting;
- Incompatible density;
- Safe sight distance for dwellings; and
- Loss of privacy and vehicle noise intrusion.

RECOMMENDATION:

- A. That the Development Application for nine (one existing and eight new) multiple dwellings at 981 Cambridge Road (CT223817/1 and CT166751/1) (Cl Ref PDPLANPMTD-2023/037817) be refused because the proposal is contrary to the provisions of the Tasmanian Planning Scheme – Clarence for the following reason:

1. The proposal does not comply with clause 8.4.1 P1(b) because the proposal does not provide for a significant social or community benefit.
- B. That the details and conclusions included in the Associated Report be recorded as the reasons for Council's decision in respect of this matter.

ASSOCIATED REPORT

1. BACKGROUND

The two lots subject to the application were created by subdivision permit SD-2008/116. The subdivision permit resulted in the creation of two lots which have remained in the same ownership, and the property is known as 981 Cambridge Road.

Planning application PDPLANPMTD-2023/034251, for nine multiple dwellings (one existing and eight new) was refused by Council at its meeting held on 17 July 2023 for the following reasons:

1. The proposal does not comply with clause 8.4.1 P1(b) because the proposal does not provide for a significant social or community benefit.
2. The proposal does not comply with clause 8.4.2 P3(a)(iv) as the visual impacts caused by the overall apparent scale, bulk, and proportions of the proposal, would cause an unreasonable loss of amenity when viewed from adjoining dwellings. The proposed dwelling density would emphasise this.
3. The proposal does not comply with clause 8.4.2 P3(b) as the separation between the proposed units and dwellings on adjoining properties is inconsistent with that existing on established properties in the area.
4. The proposal does not comply with clause 8.4.4 P1 as the multiple dwellings have been designed and sited so as to cause an unreasonable loss of amenity by overshadowing the private open space of another dwelling on the same site; and
5. The proposal does not comply with C2.6.5 P1(a)-(i) as it would not provide for safe and convenient pedestrian access.

The planning application now the subject of this report, was made with a revised design attempting to overcome refusal grounds 2-5 listed above. The revised design results in the following improved outcomes:

- Conjoining Units 3 - 4 and Units 7 - 8, and reducing Units 4 and 7 to single storey, and increasing the setback of Units 3 - 8 from the eastern side and southern rear boundaries resulting in all buildings being located within the building envelope, as prescribed by clause 8.4.2 A3 of the General Residential Zone. Compliance with this clause has overcome refusal grounds 2 and 3 in relation to visual bulk and dwelling separation.
- The revised design results in a minimum of 50% of the private open space to each unit receiving in excess of three hours of sunlight between 9am and 3pm on 21 June, as required by clause 8.4.4 A1 of the General Residential Zone, therefore overcoming refusal ground 4.
- Provision of a 1m wide internal pedestrian pathway parallel with the western side boundary and extending in an easterly direction through the development site. The pathway will be a minimum of 1m wide, constructed with a hard-wearing concrete surface, and separated from the driveway with kerbing where possible. Where the pathway crosses the internal driveway, it will be delineated with road markings, defining the pathway and pedestrian crossing areas. To further enhance safety by moderating the operating speed of vehicles, a 10km/h shared zone sign will be installed at the beginning of the driveway. The internal pathway is considered by Council's development engineers as providing a good level of service for pedestrians and overcomes refusal ground 5.

In relation to refusal ground 1, the density of the revised proposal remains unchanged. During the initial phase of assessment of the application, it was identified that compliance with the applicable density standard would be challenging. At the earliest opportunity, the applicant was made aware, and it was suggested the application be revised to what is permissible in the zone (i.e., a total of eight dwellings as opposed to nine).

However, the application remains for the assessment of nine dwellings and has been assessed accordingly. Refusal ground 1 has not been overcome by the revised design.

2. STATUTORY IMPLICATIONS

2.1. The land is zoned General Residential under the Scheme.

2.2. The proposal is discretionary because it does not meet the Acceptable Solutions under the Scheme.

2.3. The relevant parts of the Planning Scheme are:

- Clause 5.6 – Compliance with Applicable Standards;
- Clause 6.10 – Determining Applications;
- Clause 8.0 – General Residential Zone;
- Clause C2.0 – Parking and Sustainable Transport Code;
- Clause C3.0 – Road and Railway Assets Code; and
- Clause C.16 – Safeguarding of Airports Code.

2.4. The proposal satisfies exemption C16.4.1(a) of the Safeguarding of Airports Code in that the maximum height of the development would not exceed the prescribed obstacle limitation surface level of 90m AHD.

2.5. Council's assessment of this proposal must consider the issues raised in any representations received, the outcomes of the State Policies and the objectives of Schedule 1 of the *Land Use Planning and Approvals Act, 1993* (LUPAA).

3. PROPOSAL IN DETAIL

3.1. The Site

The subject site consists of two titles, Certificate of Title references: CT223817/1 (1,419m²) and CT166751/1 (1,634m²), known as 981 Cambridge Road, Cambridge. The site has a combined area of 3,058m² (including the area contained within the access strips).

The site forms an internal lot accessed from the southern side of Cambridge Road via two access strips, with a combined frontage width of 8.14m.

Reciprocal rights-of-way are in place over the initial 71m of the access strip benefiting the lots forming part of the application, and the adjoining property at 983 Cambridge Road. The area of these reciprocal rights-of-way covers 430m². A sealed crossover has been constructed from Cambridge Road with an existing gravel driveway servicing the existing dwelling.

The site has a north facing aspect with a gentle grade and is currently developed with a dwelling contained within the boundaries of CT166751/1. The topography of the site rises from Cambridge Road at the north from 48m contour to the 53m contour at the rear boundary, resulting in gentle 5.2% gradient.

The site is serviced by reticulated sewer, water and stormwater infrastructure with a variable width pipeline easement running parallel to the western side boundary.

The site is within an established residential area typified by single storey detached dwellings and multiple dwellings.



Map 1: Subject Site and Surrounds

3.2 The Proposal

The proposal is for the construction of eight multiple dwellings (there is an existing single residence on the site) resulting in a total of nine dwellings on the site.

The dwellings would be located around a central driveway providing access to each dwelling. Table 1 shows the number of storeys and bedrooms per dwelling.

Unit	No of Storeys	No of Bedrooms
1	1	2
2	2	3
3	1	2
4	1	3
5	1	3
6	2	3
7	1	3
8	1	2
9 (existing dwelling)	1	3
Total		24

Table 1: No of storeys and bedrooms per unit.

The dwellings would have floor areas ranging from 93m² to 200m².

The dwellings would vary in height from 4.5m to 7.5m above existing ground level, with setbacks ranging from 1.8m to 9.3m from side and rear boundaries. The separation between the proposed dwellings ranges from 2.5m to 9.5m.

A front fence constructed of timber palings with a maximum height of 1.2m is proposed along part of the frontage to screen the proposed shared bin storage area.

A total of 22 car parking spaces has been provided for the proposed development, in accordance with the requirements of the Parking and Sustainable Transport Code.

The existing access is proposed to be upgraded to provide a 5.5m wide sealed access and driveway to service the dwellings, with a dedicated pedestrian pathway separated from the driveway by kerbing where possible.

4. PLANNING ASSESSMENT

4.1. Compliance with Applicable Standards Section 5.6

“5.6.1 A use or development must comply with each applicable standard in the State Planning Provisions and the Local Provisions Schedules.”

4.2. Determining Applications [Section 6.10]

“6.10.1 In determining an application for any permit for use or development the planning authority must, in addition to the matters required by section 51(2) of the Act, take into consideration:

- (a) all applicable standards and requirements in this planning scheme; and*
- (b) any representations received pursuant to and in conformity with section 57(5) of the Act, but in the case of the exercise of discretion, only insofar as each such matter is relevant to the particular discretion being exercised.”*

References to these principles are contained in the discussion below.

4.3. General Provisions

The Scheme contains a range of General Provisions relating to specific circumstances not controlled through the application of Zone, Code or Specific Area Plan provisions. There are no General Provisions relevant to the assessment of this proposal.

4.4. Compliance with Zone and Codes

8.2 Use Table

The proposal is for a residential use, which is permitted in the General Residential Zone under Table 8.2 of the Scheme.

General Residential Zone

- Clause 8.4.1 A1 (Residential density for multiple dwellings)** requires multiple dwellings to have site area per dwelling of not less than $1/325\text{m}^2$. The term “Site Area” is defined by the Scheme as: *“means the area of a site, excluding any access strip, divided by the number of dwellings on that site.”*

The two properties are burdened by reciprocal rights-of-way as shown in Figure 1.

1. These rights-of-way have a combined area of 429.79m^2 .

The term “Access Strip” is defined under the Scheme as:

means the narrow part of an internal lot to provide access to a road.

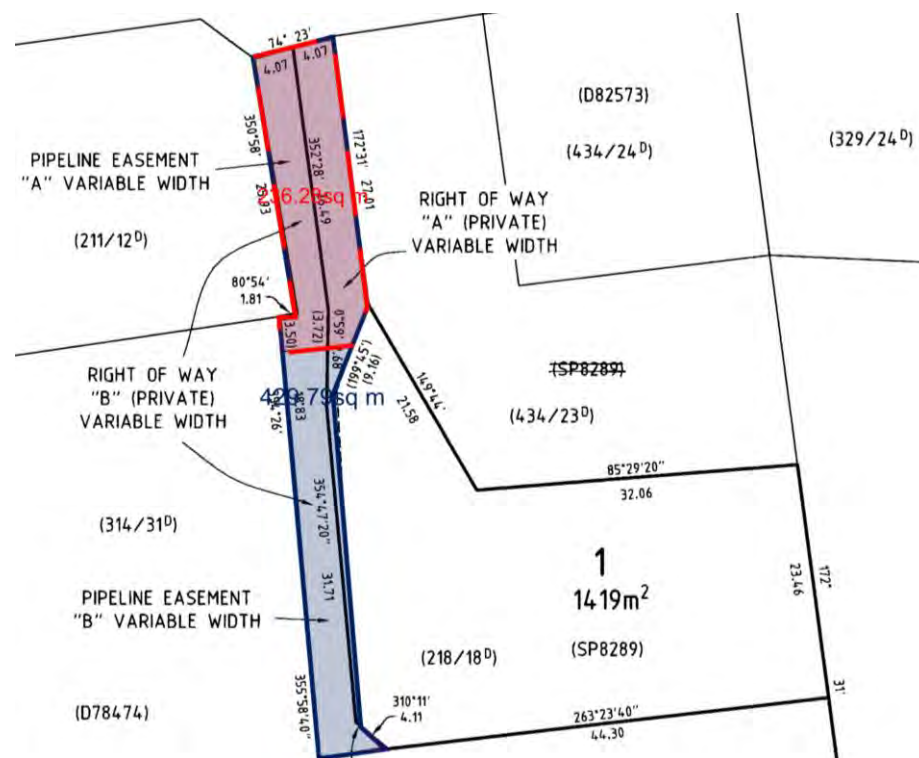


Figure 1: Plan showing reciprocal rights-of-way for access.

Site plan showing the proposed location of the Access Strip. The plan includes dimensions for the strip (e.g., 4.800, 4.900, 5.000) and existing features like the "Ex. grate drain and pit" and "Access to 883 Campground". A dashed line indicates the proposed strip boundary, and a solid line shows the existing road layout. A note "276.000 m" is present near the strip's end.

The application must be assessed against Performance Criteria (P1) of this standard as follows.

Clause	Performance Criteria	Assessment
8.4.1 P1	<i>“Multiple dwellings must only have a site area per dwelling that is less than 325m², if the development will not exceed the capacity of infrastructure services and:</i>	Clause 5.6.4 of the Scheme requires the planning authority to consider the relevant objective in an applicable standard to determine if a proposal satisfies the performance criterion for that standard. Objective (b) of clause 8.4.1 relates to the density of multiple dwellings through optimising the use of infrastructure and community services.

		<p>As detailed under the 2022 <i>Tasmanian Housing Strategy Discussion Paper</i>, infrastructure services also include appropriate access to services (including health and retail), employment and schools. Services within the township of Cambridge include a school, sporting facilities, community hall, playgrounds, service station and convenience store. Major retail, health, community support and employment services are located 5km away within Rosny Park or 10km away at Sorell.</p> <p>While a private medical centre is located within the Cambridge Industrial Park, no public health or community support services are available in the suburb.</p> <p>Housing supply must also align with the appropriate infrastructure, including sufficient capacity of roads, electricity, and water. It is considered there is sufficient capacity within existing reticulated services and the local road network to cater for the increased housing supply.</p> <p>The use of the conjunctive “and” at the end of the first sentence within P1 demonstrates that each limb of the performance criteria must be satisfied. That is, in addition to demonstrating the development will not exceed the capacity of infrastructure services, P1(a) <u>or</u> (b) must also be met to satisfy the performance criteria.</p>
	<p>(a) <i>is compatible with the density of existing development on established properties in the area; or</i></p>	<p>The site area is 2,777m² resulting in a site area per dwelling of 308m².</p>

		<p>The site consists of two internal lots with a combined access strip off Cambridge Road. The lot proper has been considered based on the removal of the area contained within the access strips. The compliant maximum density would be eight dwellings.</p> <p>The proposed density would be greater than that of any other multiple dwelling development within the surrounding area of the Cambridge township.</p> <p>“Compatibility” is not defined under the Scheme; however, the approach taken in the RMPAT decision <i>Henry Design and Consulting v Clarence City Council & Ors [2017] TASRMPAT 11</i>, and several other subsequent decisions, has been adopted by Council where compatibility is taken to be in broad correspondence or in harmony with the prevailing or predominant densities in the surrounding area.</p> <p>In <i>Clarence City Council v M Drury [2021] TASSC 5</i>, Justice Brett did not rule out statistical analysis being determinative of the compatibility of the proposed site area per dwelling with the density of the surrounding area.</p> <p>The proposed density of 308m² per dwelling is significantly higher than the average density of the surrounding area. In addition to the quantitative analysis, in <i>Clarence City Council v M Drury</i>, Justice Brett said at paragraph [22]:</p>
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		<p><i>“...The determination of compatibility requires an overall assessment of how two things relate to each other and is concerned with the ultimate result. It suggests the need for a qualitative comparison and assessment.”</i></p> <p>In undertaking a qualitative assessment of the proposed multiple dwelling development and whether it is in harmony with the density of the surrounding area, the following is considered relevant:</p> <ul style="list-style-type: none"> • Single dwellings are the predominant development pattern in the Cambridge township; • There are four other multiple dwelling developments within the surrounding area (defined as within 100m of the site) with densities of 1 per 475m², 1 per 565m² and 1 per 400m² and 1 per 450m²; and • Single dwellings are generally well setback from the street frontage and rear boundaries, have generous backyards, and have either no front fence or a low fence. <p>The proposal for nine multiple dwellings with limited rear and side setbacks and small private open spaces, would not maintain a pattern of built form compatible with that established in the surrounding area. For this reason, the proposed development would not exist in harmony with the built form in the surrounding area.</p>
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		<p>The proposal is not considered capable of complying with P1(a) and for the proposal to be approved it must be capable of satisfying 8.4.1 P1(b) as considered below.</p>
	<p>(b) <i>provides for a significant social or community benefit and is:</i></p> <p>(i) <i>the site is wholly or partially within 400m walking distance of a public transport stop.</i></p> <p>(ii) <i>the site is wholly or partially within 400m walking distance of a business, commercial, urban mixed use, village, or inner residential zone.”</i></p>	<p>The application contained a legal opinion, a letter of interest from Centacare Housing and an email from a planning consultant in support of the proposal providing a significant social or community benefit.</p> <p>Any provision of housing can provide social or community benefit. However, it is whether that benefit is significant.</p> <p>No assessment of any potential social or community benefit provided by the proposal was detailed in the application submission.</p> <p>However, the legal opinion stated:</p> <p><i>“1.A single supply of 9 new affordable dwellings in Clarence will be a significant benefit to the community in consequence of the current housing shortage and rental crisis.</i></p> <p><i>2. A single supply of 9 new affordable dwellings will offer a significant social and community benefit by leading to an increase in population in the Cambridge area, leading to increased economic activity, and likely social engagement given the proximity of the development to the nearby school and services.</i></p>

		<p>3. <i>9 new dwellings will generate a large increase in municipal rates which will significantly benefit the community.”</i></p> <p>The provision of nine multiple dwelling units on its own is assessed as not providing a significant social and community benefit and is not considered reason enough to provide for the increase in density proposed. If the assessment was undertaken using this logic, then Council could approve any multiple dwelling density proposal regardless of social or community benefit, simply based on the number of dwellings proposed.</p> <p>The letter from Centacare states:</p> <p><i>“I write to you today to confirm that Centacare Evolve Housing have an interest in this project being approved for the purpose of social housing.”</i></p> <p>There is nothing in the development application which provides any assurance that the development application would deliver community or social housing or that Centacare would be involved in its eventual operation. If Council were to approve the development application and wished for it to be used for community or social housing, it would appear difficult to condition the permit on the basis that the future owner and operational arrangements remain unknown.</p>
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		<p>The email from the planning consultant states:</p> <p><i>“In my assessment the proposal for community housing dwellings in a mixed tenure arrangement with the remaining dwellings to be held in private ownership will provide a significant social housing benefit. This is particularly so given the recognised preference and well document (sic) social benefits that result from social housing being mixed amongst privately owned dwellings.”</i></p> <p>As previously stated, there is nothing in the proposal which would guarantee that the dwellings would be used for social or community housing.</p> <p>No information is included in the proposal which demonstrates or calculates that it would provide a social or community benefit, other than it is for nine multiple dwellings, which is not considered to be a significant benefit.</p> <p>The proposal is discretionary as it seeks to increase the density above 1 per 325m². A proposal for eight multiple dwellings would comply with the density standards and it cannot be argued that one additional dwelling would provide any significantly greater social or community benefit.</p> <p>Clause 8.4.1(b) P1 in the alternative test to the acceptable solution where either a <u>significant</u> community or social benefit can be demonstrated by the applicant.</p>
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		<p>In <i>C J & J E Margetts v Burnie City Council</i> [2015] TASRMPAT 21, the Tribunal found:</p> <p><i>“The Tribunal considers that community benefit required to be demonstrated is the advancement of the general community interest. It requires the production (by way of evidence) of objectively defensible results based upon disclosed methods, producing analysis capable of being understood and tested.”</i></p> <p>In relation to the nature of the evidence to be provided, the Tribunal found:</p> <p><i>“The required social housing benefit must be more than a marginal impact on house prices through the release of a few extra lots onto the market and the requirement is not demonstrated by the testimonials of individual ‘not for profit’ housing providers. These are in the nature of assertions, they articulate conclusions, and they are not evidence which sets out any objectively ascertainable community benefit or social housing benefit.”</i></p> <p>In relation to “significant community benefit”, the Tribunal considered the meaning of this phrase in <i>Telstra Corporation Ltd v Meander Valley Council and Ors</i> [2019] TASRMPAT 11, and stated at paragraphs 71 and 72:</p> <p><i>“What constitutes a ‘significant community benefit’ is not defined in the Scheme, nor are the individual words defined. What constitutes the ‘community’, being a group of people living in one place, should not, in the Tribunal’s view, be interpreted narrowly.</i></p>
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		<p><i>In the context of the Site and the nature of the Proposal, the Tribunal's view is that 'community' would comprise at least the collection of people who live in Blackstone Heights and may well extend beyond that community.</i></p> <p><i>The assessment of 'benefit' is that which is 'for the good of a person or thing', or to 'gain advantage', or 'make improvement'. For benefit to be significant, it must be 'important' and 'of consequence'."</i></p> <p>The words "a significant social or community benefit" require that at the completion of a development, it provides an outcome which is for the good of a community or society that is important and of consequence. This test requires an assessment of the development as a whole to determine whether that high threshold is met. The need for a "significant social or community benefit" conveys a requirement that the development do more than simply increase population, housing supply and economic activity.</p> <p>The proposal has not demonstrated how a <u>significant</u> social or community benefit would be delivered by the proposal, accordingly it does not satisfy clause 8.4.1P1(b) of the standard and refusal is recommended.</p> <p>The site is located within 100m walking distance of the Local Business zoned land at 959-971 Cambridge Road to the west and therefore satisfies (b)(ii).</p>
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		The site is located within 100m of a bus stop located along Cambridge Road and therefore satisfies (b)(i).
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General Residential Zone

- **Clause 8.4.6 A3 (Privacy)** in that the following windows would not maintain the required separation or privacy treatment from the shared internal driveway:
- Unit 1, Bedroom 1 east elevation window maintains the required sill height; however, the window does not maintain the required 1m separation from the shared driveway, therefore A3(b) is not met.
- Unit 2, Bedroom 1, and Bedroom 2 windows maintain required sill height; however, they do not provide the required 1m separation, therefore A3(b) is not met.
- Unit 3, door to kitchen, although glazed, does not maintain the required 1m separation from the shared driveway, therefore A3(a) or (b) is not met.
- Unit 4, Bedroom 2 window provides a sill height of 1.7m; however, does not maintain the required 1m setback from the shared driveway, therefore A3(b) is not met.
- Unit 7, Kitchen and bedroom 2 and 3 windows do not maintain the required 1m separation, therefore A3(b) is not met. While the Bedroom 3 window provides the required sill height, the kitchen window does not provide the required sill height or screening (opaque glass proposed).

- Unit 9 (Existing dwelling) sunroom is located within 1m of the shared driveway. The kitchen window, although screened, would be within 1m of the shared driveway, therefore neither window meets A3(b).

The application must be assessed against Performance Criteria (P1) of this standard as follows.

Clause	Performance Criteria	Assessment
P8.4.6 P3	<i>“A shared driveway or parking space (excluding a parking space allocated to that dwelling), must be screened, or otherwise located or designed, to minimise unreasonable impact of vehicle noise or vehicle light intrusion to a habitable room of a multiple dwelling.”</i>	<p>In relation to Unit 1, the provision of a double-glazed highlight window to Bedroom 1 provides an adequate design response to minimise unreasonable impact of vehicle noise or light intrusion into the bedroom. No further modifications or treatment to this window are considered necessary.</p> <p>The window design for Unit 2 Bedroom 1 and 2 is considered reasonable for reasons applied to the Unit 1 discussions.</p> <p>The door to the kitchen of Unit 3 is proposed to be glazed with opaque glazing. This is considered a reasonable treatment to minimise the impact of vehicle light intrusion into the kitchen and the use of double glazing will minimise unreasonable impact of vehicle noise intrusion.</p> <p>The window design for Unit 4 Bedroom 2 includes a double-glazed highlight window, which is considered reasonable to minimise unreasonable impact of vehicle noise or light intrusion into the bedroom.</p>

		<p>In relation to Unit 7, while the Bedroom 3 window provides the required sill height, the kitchen window does not provide the required sill height or screening (opaque glass proposed). The use of double-glazing and opaque glazing is a reasonable treatment to minimise unreasonable impact of vehicle light and noise intrusion.</p> <p>Lastly, Unit 9 (existing dwelling) sunroom and kitchen windows would be suitably screened to reduce impact of vehicle light intrusion. In relation to vehicle noise intrusion, these windows are currently single glazed and likely to experience considerable noise intrusion resulting from the proximity to the initial section of the shared driveway. It is considered these rooms would benefit from the installation of double-glazed windows to further attenuate noise intrusion. If approved, a condition is recommended to this effect.</p> <p>Subject to minor modifications to the treatment of the windows to the sunroom and kitchen of Unit 9 (existing dwelling), the proposal is considered to satisfy the performance criteria and for this reason the standard is met.</p>
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Parking and Sustainable Transport Code

- **C2.6.5 A1 (Pedestrian access)** in that the proposal requires more than 10 parking spaces, which also triggers the provision of pedestrian access. The proposed pedestrian footpath would be 1m wide and is not continuous nor separated from the access ways or parking aisles by 2.5m or suitable protective devices.

The application must be assessed against Performance Criteria (P1) of this standard as follows.

Clause	Performance Criteria	Assessment
C2.6.5 P1	<i>“Safe and convenient pedestrian access must be provided within parking areas, having regard to:</i> <i>(a) the characteristics of the site;</i>	<p>Given the scale of the development, the inclusion of a continuous pedestrian pathway through most of the site is deemed necessary to ensure safe and convenient pedestrian access within the site.</p> <p>The proposed pedestrian access is deemed to provide safe and convenient pedestrian access given a 1m wide pedestrian pathway will be provided extending from the footpath along Cambridge Road to the end of Unit 6. The pedestrian pathway will have a concrete surface and be separated from the driveway with kerbing where possible.</p>
	<i>(b) the nature of the use;</i>	<p>The proposal is for nine multiple dwellings providing 24 bedrooms and accommodating a large number of residents, which will encourage a higher pedestrian usage.</p> <p>The pedestrian access layout is easily delineable and will provide clear physical buffers between car parking spaces and pedestrian access. It is considered the proposed pedestrian access arrangement will reduce conflict between users of the site and will provide for adequate pedestrian safety.</p>
	<i>(c) the number of parking spaces;</i>	A total of 22 car parking spaces is provided, which is sufficient to meet the requirements of the Parking and Sustainable Transport Code.

	<i>(d) the frequency of vehicle movements;</i>	In consideration of the number of dwellings on the site and associated vehicle frequency, the proposal includes a clearly marked pedestrian crossing and implementation of a 10km/h shared zone. Given there would be limited interaction between the pedestrian walkways and vehicle accessways, the frequency of vehicle movements would not impact on pedestrian safety.
	<i>(e) the needs of persons with a disability;</i>	Car parking spaces and pedestrian facilities for use by persons with a disability are provided with appropriate access to pedestrian facilities within the site.
	<i>(f) the location and number of footpath crossings;</i>	Pedestrian pathways and crossings are clearly defined through ground material, including painted markings and the implementation of a 10km/h shared zone sign to be provided at the beginning of the driveway. Council's development engineer supports the design and location of pedestrian crossings.
	<i>(g) vehicle and pedestrian traffic safety;</i>	The proposed pedestrian access arrangement will provide for an appropriate design, layout and delineation from the shared driveway and parking spaces and will provide for the required pedestrian safety.
	<i>(h) the location of any access ways or parking aisles; and</i>	The location of the pedestrian walkways is mostly separated from the shared driveway and a pedestrian crossing will be installed in accordance with Council requirements.
	<i>(i) any protective devices proposed for pedestrian safety."</i>	In addition to delineation of the shared driveway from the pedestrian pathway by kerbing, the Traffic Impact Assessment provided with the application has found the internal speed limit of 10km/h will further ensure pedestrian safety.

		For the above reasons, the proposed pedestrian access arrangement is considered safe and convenient when having regard to sub-clause (a)-(i) and is considered to satisfy the performance criteria and for this reason the standard is met.
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Road and Railway Assets Code

- **Clause C3.5.1 A.4 (Traffic generation at a vehicle crossing)** in that the proposal cannot meet the acceptable solution, given the proposal will increase vehicular traffic to and from the site by more than 20% than that which is currently generated by the existing single dwelling.

The application must be assessed against Performance Criteria (P1) of this standard as follows.

Clause	Performance Criteria	Assessment
C3.5.1 P1	<i>“Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:</i>	Vehicular traffic to and from the site is assessed as minimising any adverse effects on the safety of the junction, having regard to:
	<i>(a) any increase in traffic caused by the use;</i>	<p>The proposed development would generate 48 additional vehicle movements per day, as compared to the existing seven vehicle movements per day.</p> <p>The Traffic Impact Assessment submitted with the application finds the amount of traffic generated by the proposed development is low and there is sufficient capacity in the surrounding road network to absorb these movements.</p>

		<p>Additionally, the Traffic Impact Assessment found the additional vehicle movements are not expected to cause any adverse safety or traffic impact to the traffic operations associated with the nearby Cambridge Primary School.</p>
	<i>(b) the nature of the traffic generated by the use;</i>	<p>Most vehicular movements to and from the site will be smaller vehicles under 5.5m. The type of generated traffic movements associated with the proposal is compatible with the current vehicles using the surrounding public road network.</p>
	<i>(c) the nature of the road;</i>	<p>Vehicular traffic would enter and exit the site via Cambridge Road. Cambridge Road functions as a collector road within the surrounding network and can absorb the increase in traffic movements generated by the development.</p> <p>Specifically, with the road alignment offering straight sight lines in either direction and no notable vertical grade, drivers will be able to enter and exit the site with ease and a high level of road safety.</p> <p>The Traffic Impact Assessment indicates the existing access with Cambridge Road has sufficient sight distance enabling vehicles to enter and leave the development site in a safe and efficient manner, although it is noted the installation of a traffic mirror on the outside of the horizontal curve will assist with sight lines for opposing drivers.</p>
	<i>(d) the speed limit and traffic flow of the road;</i>	<p>Cambridge Road, at the location of the proposed access, has a 60km/hr speed limit which reduces to 40km/hr during school pick-up and drop off times.</p>

	<i>(e) any alternative access to a road;</i>	There is no alternative site access other than to and from Cambridge Road. The proposed access layout is the most practical to service the proposed development.
	<i>(f) the need for the use;</i>	Given the location and size of the site, it is reasonable to assume the site will be developed with multiple dwellings.
	<i>(g) any traffic impact assessment; and</i>	A Traffic Impact Assessment has been provided with the application. The assessment found from a traffic engineering and road safety perspective, additional traffic generated from the proposed development is not expected to cause any adverse safety or traffic efficiency issues and no recommendations are made. Council's development engineer supports the findings of the assessment.
	<i>(h) any advice received from the rail or road authority."</i>	Council's Manager Development has advised the road network can absorb the additional traffic without significant risk to road users and will provide for ease of access and egress to the site. The proposal is considered to satisfy the tests of this performance criterion and complies with the standard.

5. REPRESENTATION ISSUES

The proposal was advertised in accordance with statutory requirements and two representations were received. The following issues were raised by the representors.

5.1. Location of Pedestrian and Parking Lighting

Concern is raised in relation to the location and design of pedestrian and parking lighting within the site and the impact this may have on the amenity of adjoining residences.

- **Comment**

The lighting requirements prescribed by clause C2.6.4 A1 of the Parking and Sustainable Transport Code apply to carpark areas within the General Business Zone and Central Business Zone only. There is therefore no mechanism to control the requirement of pedestrian lighting within the site. It is noted there is no specific detail provided for lighting; however, any internal lighting will be required to meet relevant Australian Standards, which will ensure lighting is baffled so that direct light does not extend into the adjoining properties.

5.2. Waste Collection Arrangements

The representor has raised concern over the location of the communal waste storage facility, in terms of the impact this will have on the amenity of adjoining residential properties to the east and west. Concern is also raised as to how the bin storage area will be accessed via a waste collection vehicle.

- **Comment**

A private waste collection service will be required to service the proposed development, which will use a vehicle capable of hoisting larger bins over the front of the cabin. The vehicle will reverse into the driveway, empty the bins, and leave in a forward direction. This reversing manoeuvre is not expected to cause any adverse traffic or safety impacts, subject to a condition limiting the collection time to prior to 7am on the day of collection.

5.3. Emergency Vehicle Access

Concern is raised emergency vehicles (particularly fire trucks) will not be able to access the site.

- **Comment**

The Traffic Impact Assessment incorporates the sweep path of a medium rigid vehicle and has found the internal access arrangements will accommodate a medium rigid vehicle (heavy pump fire truck), meaning all units can be accessed by fire trucks.

Due to site constraints, the fire service will need to reverse out of the site, which is acceptable given the likelihood of this movement would be very low.

5.4. Boundary Fencing

The representor is concerned the reduction in the height of the western side boundary fence adjacent to 977 Cambridge Road will reduce the privacy, security, and amenity of the adjoining property.

- **Comment**

The initial 4.5m of the western side boundary fence is proposed to be lowered to 1.2m to enhance sight distance for vehicles and pedestrians entering and exiting the property, and to result in a development which satisfies the relevant exemption for front fencing. The reduction in the height of the side fence would apply to the initial 4.5m length of fencing only, with the remainder of the fence remaining a solid (not permeable) 1.8m high boundary fence.

The retention of the western side boundary fence in its current form to enhance the privacy of the adjoining property is a private matter regulated by the *Boundary Fences Act 1980*. This matter is therefore of no determining weight.

5.5. Infrastructure Capacity

The representor has raised concerns that two stormwater discharge points are insufficient to cope with the number of additional dwellings.

- **Comment**

The Scheme does not provide specific stormwater management requirements; however, the stormwater design will be assessed as part of the detailed engineering assessment to ensure there is no detrimental impact to downstream infrastructure. It is noted that the proposal includes a management strategy to address stormwater quality and quantity requirements by means of detention and a treatment system.

5.6. Traffic Congestion and Vehicle and Pedestrian Safety

The representor is concerned the proposal does not consider the safety of pedestrians crossing the property access on Cambridge Road.

- **Comment**

The Traffic Impact Assessment considers the impact of usage of the access on pedestrian safety at section 7.3 of the assessment. The Traffic Impact Assessment finds there are sufficient sight lines between drivers and pedestrians and complies with the relevant Australian Standard. This finding is supported by Council's development engineer.

5.7. Anticipated daily Vehicle Movements to and from the Site

The representor is concerned the Traffic Impact Assessment underestimates the anticipated daily traffic movements to and from the site. Specifically, the representor queries the projected number of seven vehicle movements per day associated with the use of the nine units.

- **Comment**

To determine the number of trips likely to be generated by the development, the Traffic Impact Assessment refers to the best practice guidance document, being the *RTA Guide to Traffic Generating Developments* (RTA Guide). The guide recommends for low density residential dwellings in regional areas daily vehicle trips of 7.4 per dwelling. Based on the RTA Guide, the Traffic Impact Assessment predicts the development will generate an additional 48 daily trips. It would appear that the representor may have misread the information contained within the Traffic Impact Assessment in relation to forecast daily vehicle movements.

5.8. Headlight Glare

The representor is concerned the location of the shared driveway will result in vehicle headlights shining directly into the neighbouring properties to the west.

- **Comment**

The concern relating to vehicle light intrusion upon adjoining residences to the west does not have any determining weight because the Scheme does not include standards addressing separation requirements from shared driveway to adjoining residences.

5.9. Impact on School Pedestrian Crossing

The representor is concerned the proximity of the access and increased traffic generated by the proposed development will impact upon the safety of the Cambridge Primary School crossing.

- **Comment**

Cambridge Road, in the location of the access is 60km/hr, which reduces to 40km/hr during school opening and finishing hours, with a manned crossing provided to the west of the proposed access during school hours. Council's development engineer has advised the increase in traffic is unlikely to result in an adverse impact to the safety of pedestrian or school users at the identified school crossing due to the separation offered and available sight distance.

Furthermore, the Traffic Impact Assessment has found the additional vehicle movements will not cause any adverse safety or traffic impact to the traffic operations associated with the local primary school.

5.10. Incompatible Density

Representors are concerned that the density of the development is inconsistent with the built neighbourhood character of the area.

- **Comment**

It has been established the proposed density does not comply with the development standard for residential density at clause 8.4.1 of the Scheme, and for this reason will not be consistent with the density of the surrounding area and does not provide for a significant social or community benefit. This concern has determining weight.

5.11. Safe Sight Distance for Vehicles

The representor has raised concern the access does not achieve the necessary sight distance due to the presence of a solid 1.8m high boundary fence and the presence of a power pole and vegetation.

- **Comment**

The Traffic Impact Assessment has been prepared by a suitably qualified traffic engineer and has found the layout of the driveway access will ensure there is sufficient sight lines between drivers and pedestrians, complying with section 3.2.4 and figure 3.3 of the *Australian Standard 2890.1:2004*. The access is therefore deemed appropriately designed and located to provide the required safe intersecting sight distance.

5.12. Loss of Privacy and Vehicle Noise Intrusion

Concern is raised in relation to the impact of the shared driveway upon the residential amenity of the adjacent residential properties adjoining the western boundary of the site. This concern relates specifically to loss of privacy and vehicle noise intrusion.

- **Comment**

The concerns relating to loss of privacy caused by use of the shared driveway would be satisfactorily ameliorated through the retention of the 1.8m high solid boundary fence. The concerns relating to vehicle noise intrusion upon adjoining residences to the west do not have any determining weight, as the Scheme does not include standards addressing separation requirements from shared driveway.

6. EXTERNAL REFERRALS

The proposal was referred to TasWater, which has provided a number of conditions to be included on the planning permit if granted.

The proposal was referred to TasNetworks, which has provided advice to be included on the planning permit if granted.

7. STATE POLICIES AND ACT OBJECTIVES

7.1. The proposal is consistent with the outcomes of the State Policies, including those of the State Coastal Policy.

7.2. The proposal is not consistent with the objectives of Schedule 1 of LUPAA, as it is not in accordance with Part 1 clause 1 (b) because it does not provide for the fair, orderly and sustainable use and development of air, land, and water by not satisfying the performance criteria and failing to meet the applicable standards.

8. COUNCIL STRATEGIC PLAN/POLICY IMPLICATIONS

There are no inconsistencies with Council's adopted Strategic Plan or any other relevant Council Policy.

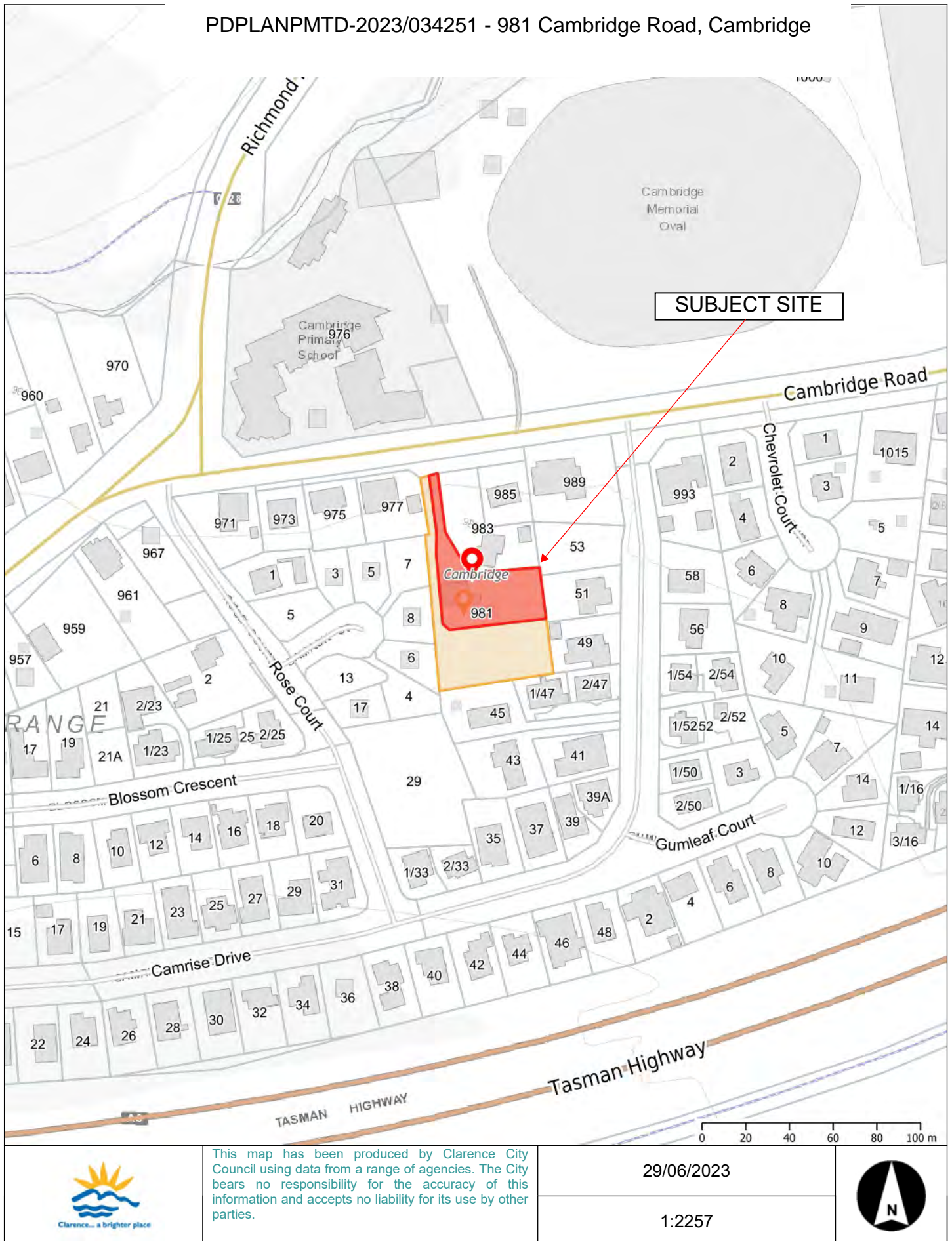
9. CONCLUSION

The proposal is recommended for refusal because the proposed development does not comply with clauses 8.4.1 P1(b) of the Scheme for reasons detailed within this report.

Attachments: 1. Location Plan (1)
2. Proposal Plan (32)
3. Site Photos (2)

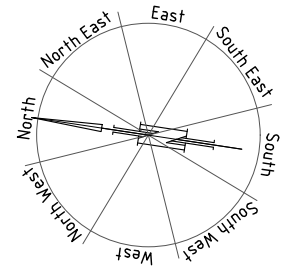
Robyn Olsen
ACTING HEAD CITY PLANNING

PDPLANPMTD-2023/034251 - 981 Cambridge Road, Cambridge



CONTENTS

1	Site Plan
2	Site Plan 2
3	Landscape Plan
4	Driveway Plan
5	Vehicle Turning Plans 1
6	Vehicle Turning Plans 2
7	Stormwater Plan
8	Stormwater Plan Upper Level
9	Plumbing Notes
10	Unit 1 Plan
11	Unit 1 Elevations
12	Unit 2 Plan
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18	Unit 5 Elevations
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20	Unit 6 Elevations
21	Unit 6 Elevations
22	Unit 7 & 8 Plan
23	Unit 7 & 8 Elevations
24	Unit 9 Plan, Existing
25	Unit 9 Elevations, Existing
26	Unit 9 plan, Proposed
27	Unit 9 Elevations, Proposed



Attachment 2



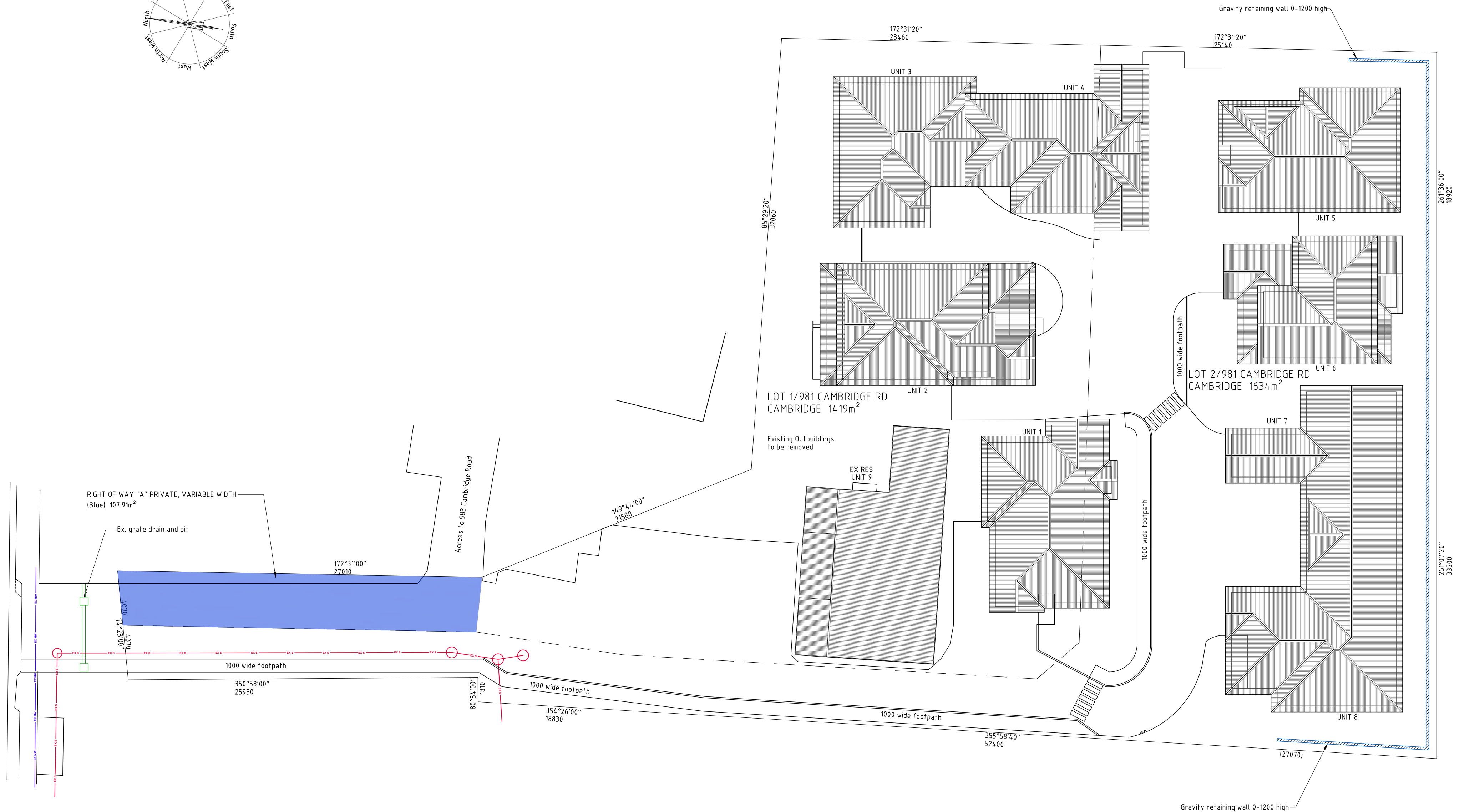
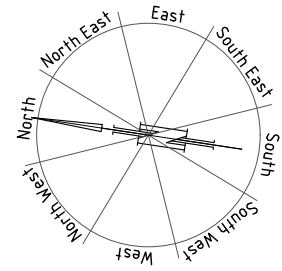
Lot 1 - 1419m² + Lot 2 - 1634m²
Total adhered land area = 3053m²
Private Right of Way "A" servicing
983 Cambridge Rd = 107.91m² (Blue)

Adhered lots total = 3053m² - 107.91m² (Right of way) = 2945.09m²
2945.09m² divided by 325m² (min lot size per unit)
= 9.06 Units, = 327.23m² per Unit.

DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION

NOTE: Titles to be adhered

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR SLIM PROPERTY DEVELOPMENTS PTY LTD AT 981 CAMBRIDGE ROAD CAMBRIDGE	SITE PLAN		DATE 29 / 08 / 23	DRAWN BY email: gilliea7@bigpond.com phone: ph 0400 671 582 Accreditation No. CC6204 copyright 2022 6922
	SCALE 1:200	AMENDED	DRAWING NO. 01 OF 27	



Lot 1 - 1419m² + Lot 2 - 1634m²
Total adhered land area = 3053m²
Private Right of Way "A" servicing
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DEVELOPMENT DRAWINGS ONLY
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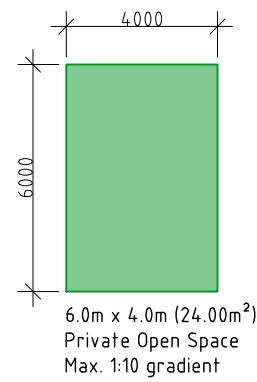
PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR SLIM PROPERTY DEVELOPMENTS PTY LTD AT 981 CAMBRIDGE ROAD CAMBRIDGE	SITE PLAN 2		DATE 29/08/23	DRAWN BY email: gilliea7@bigpond.com phone: ph 0400 671 582 Accreditation No. CC620H copyright 2022 6522
	SCALE 1:200	AMENDED	DRAWING NO. 02 OF 27	

- CL Freestanding
Retractable clothes line
F 1800 high paling fence
S 750x1500 shed
● B - Thorn Bollard lights
Safin black 1500-840 400K
24.5 watt LED
1400 x 230mm high
RW Gravity retaining wall 0-900 high

- Protea neriifolia
2-3m high
Leucadendron Red gem
1.5m high
Escallonia "Hedge edge" or similar
0.5m high

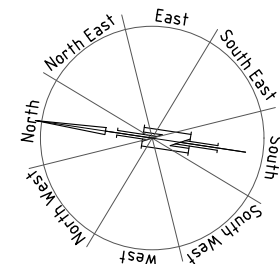
NOTE: Plant height stated is matured height
apart from the hedge which can be managed
to desired height.

Garden bed not to extend against
building, refer to CSIRO report for info



- Lawn
Decorative pebbles/gravel
Mulched garden

Unit 1 Private Open Space	87.50m²
Unit 2 Private Open Space	73.83m²
Unit 3 Private Open Space	104.53m²
Unit 4 Private Open Space	60.04m²
Unit 5 Private Open Space	98.17m²
Unit 6 Private Open Space	95.34m²
Unit 7 Private Open Space	85.64m²
Unit 8 Private Open Space	107.62m²
Unit 9 Private Open Space	128.10m²
Impervious surface	2110.27m² divide by 3053m²(1419+1634) = 69.12%
Pervious surface	942.73m² divide by 3053m² = 30.88%
Site Coverage (roofed area)	976.31m² divided by 3053m² = 31.97%

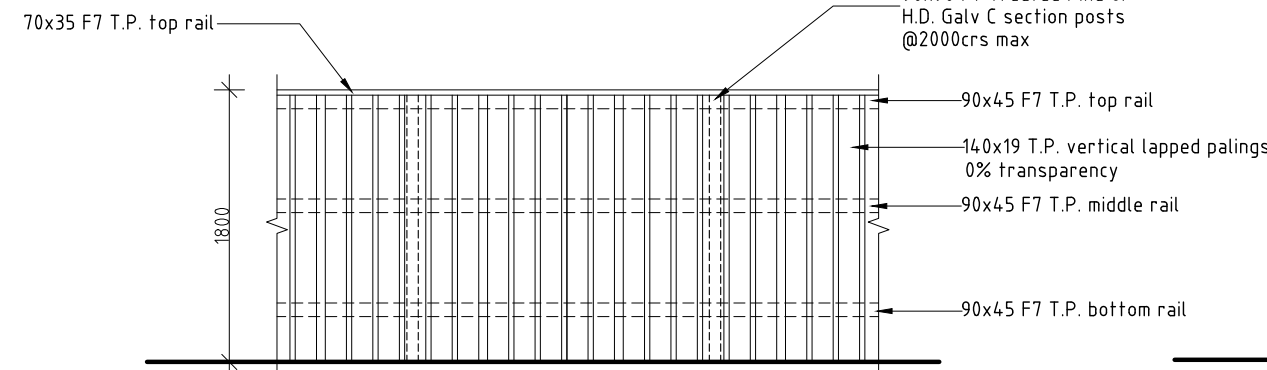
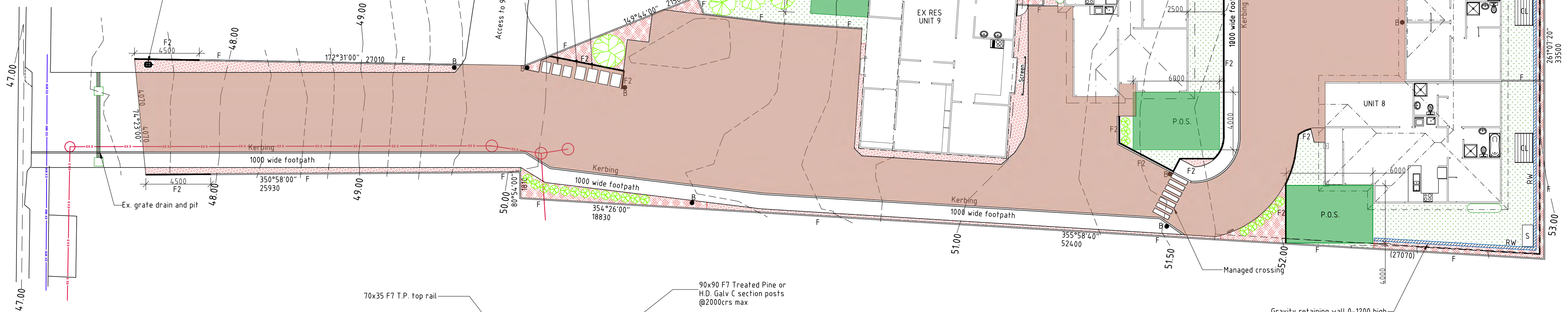


It is proposed that the bins will be
collected by a private bin collection
company, the truck is to have a front
loading hoist over the cabin, the truck
is to reverse onto the property, empty
the bins and exist in a forward direction

Shared 360 liter recycling bins
1 bin per 2 Units (4 bins required)
140 liters per Unit per week
Shared 240 liter garbage bins
1 bin per 3 Units (3 bins required)
80 liters per Unit per week

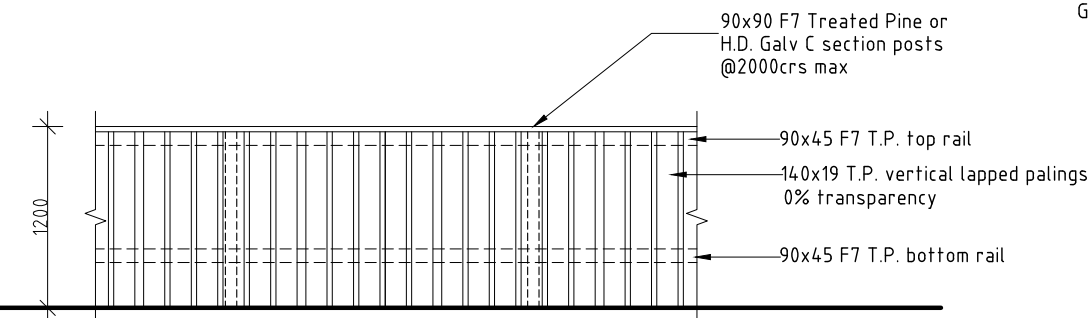
Existing Outbuildings
to be removed

Access to 983 Cambridge Road



F FENCE ELEVATION (Typical)
Scale 1:50

Fence may be solid to a height of 1200mm
A max transparency of 30% is required for
fencing over 1200mm high



F2 FENCE ELEVATION (Typical)
Scale 1:50

DEVELOPMENT DRAWINGS ONLY
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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SLIM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

LANDSCAPE PLAN

SCALE 1:200

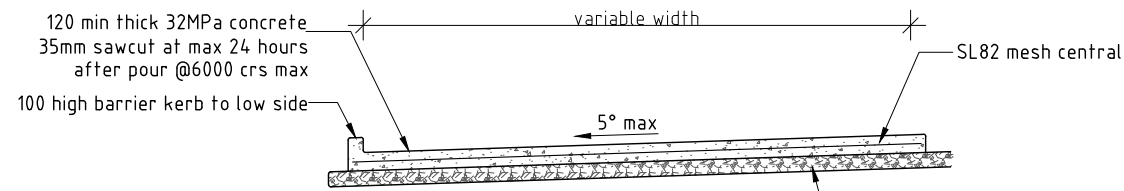
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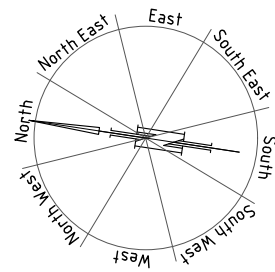
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03 OF 27

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phone: ph 0400 671 582

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DRIVEWAY CROSS SECTION (TYPICAL)
SCALE 1:50



5500 wide access as far as the Existing dwelling (Unit 9)
Driveway then reduces to min 3000 wide thereafter with
passing bays.
The number of vehicles past Unit 9 is 17, in accordance
with Table C2.2 of the Planning Scheme

DEVELOPMENT DRAWINGS ONLY
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Passing Bay: 5000 wide x 5500 long

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SLIM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

DRIVEWAY PLAN

SCALE 1:200

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DATE

29 / 08 / 23

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04 OF 27

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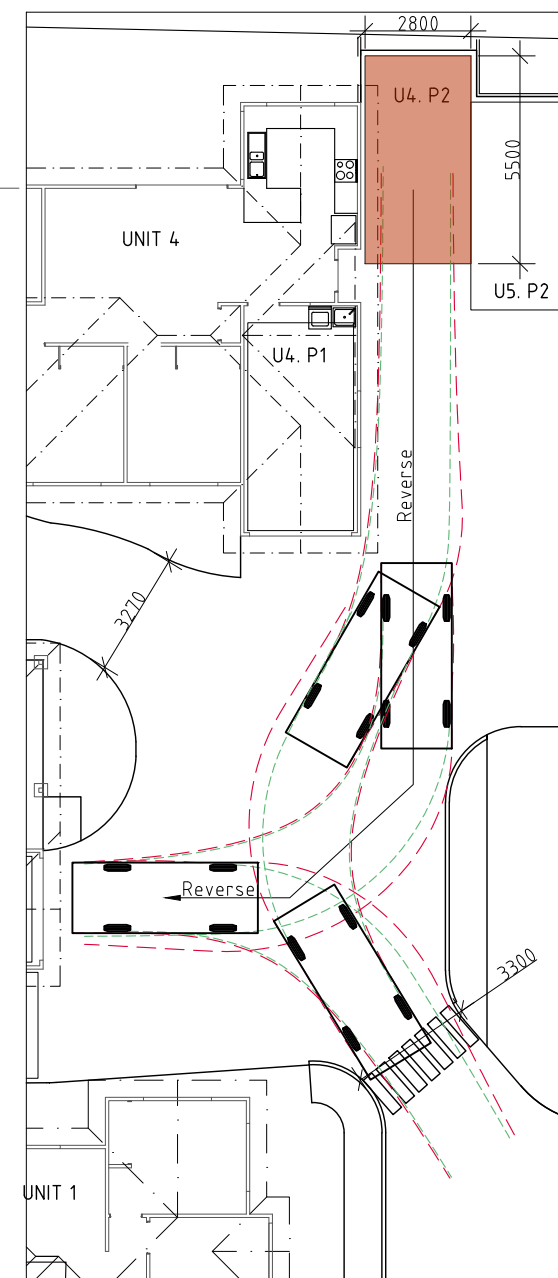
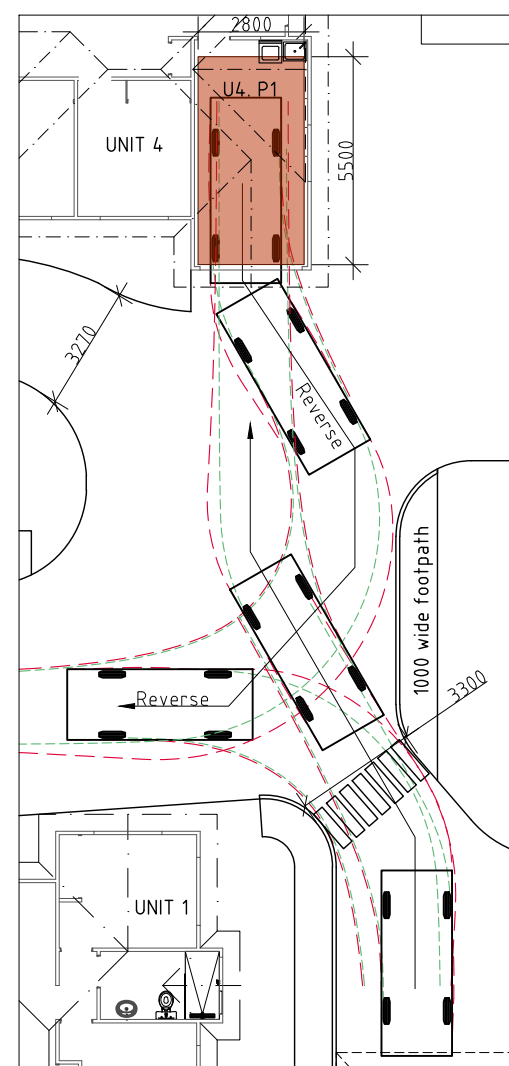
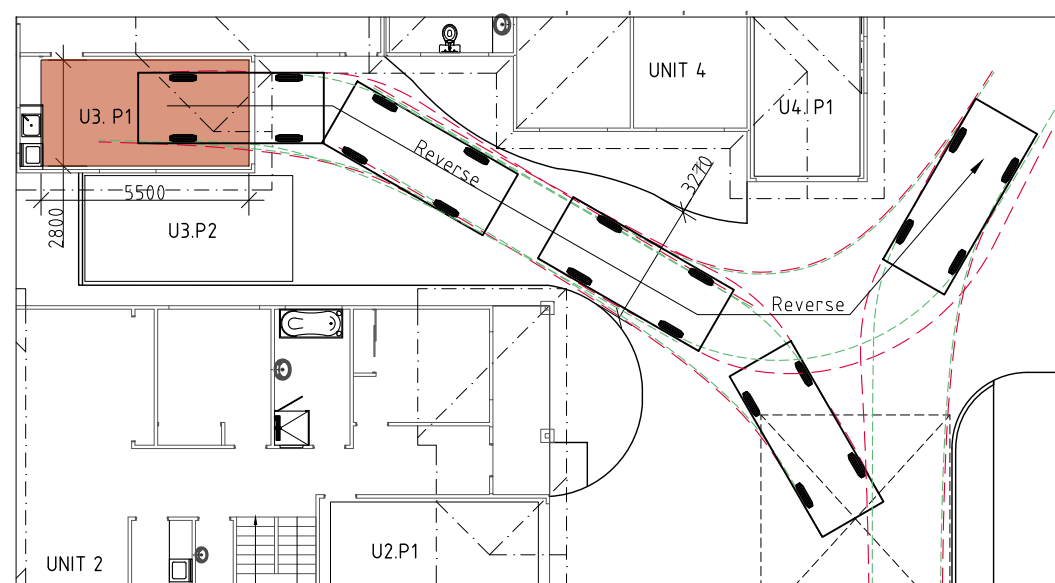
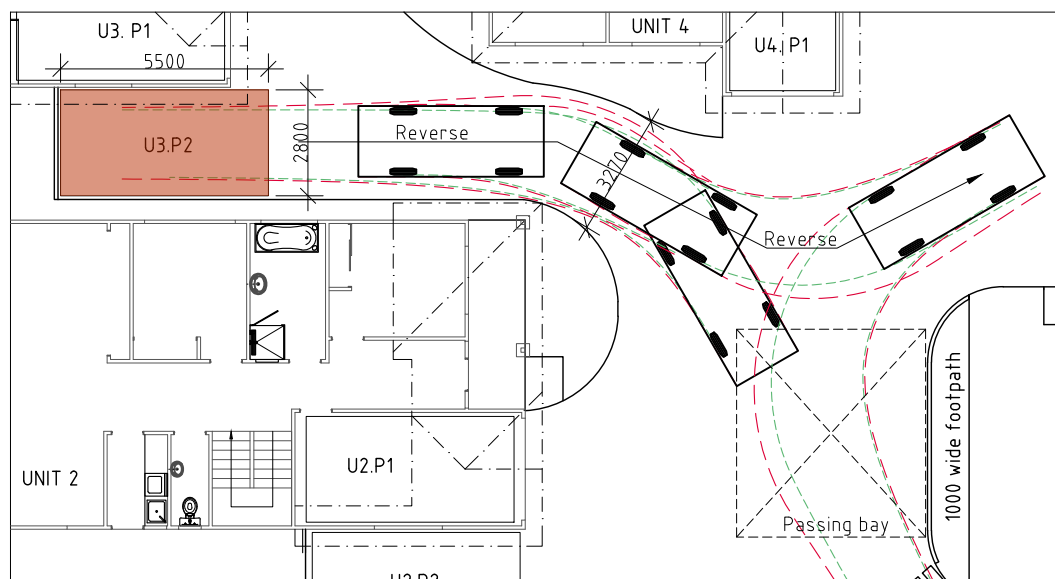
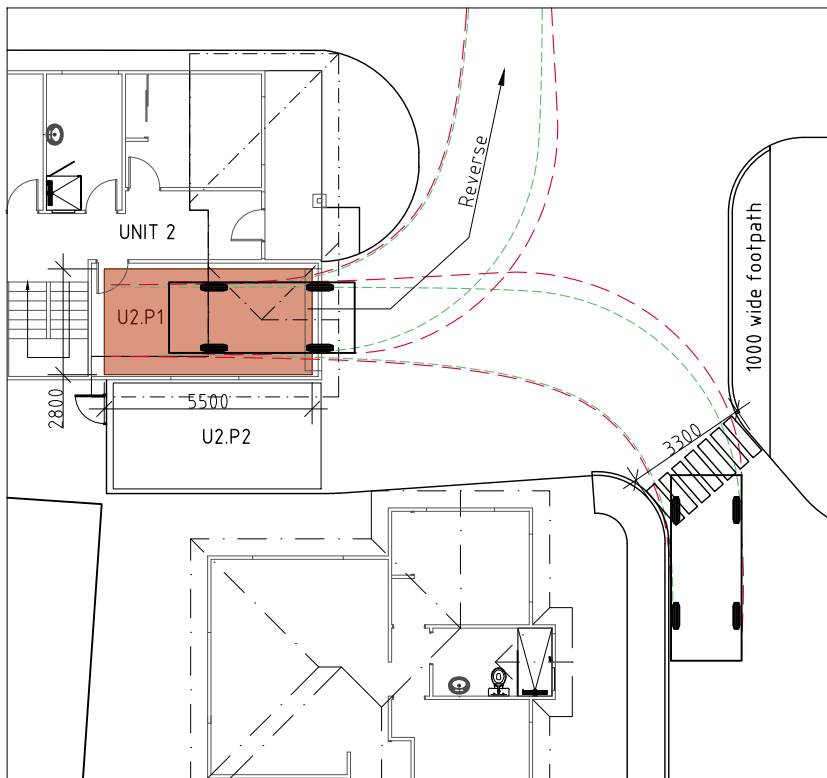
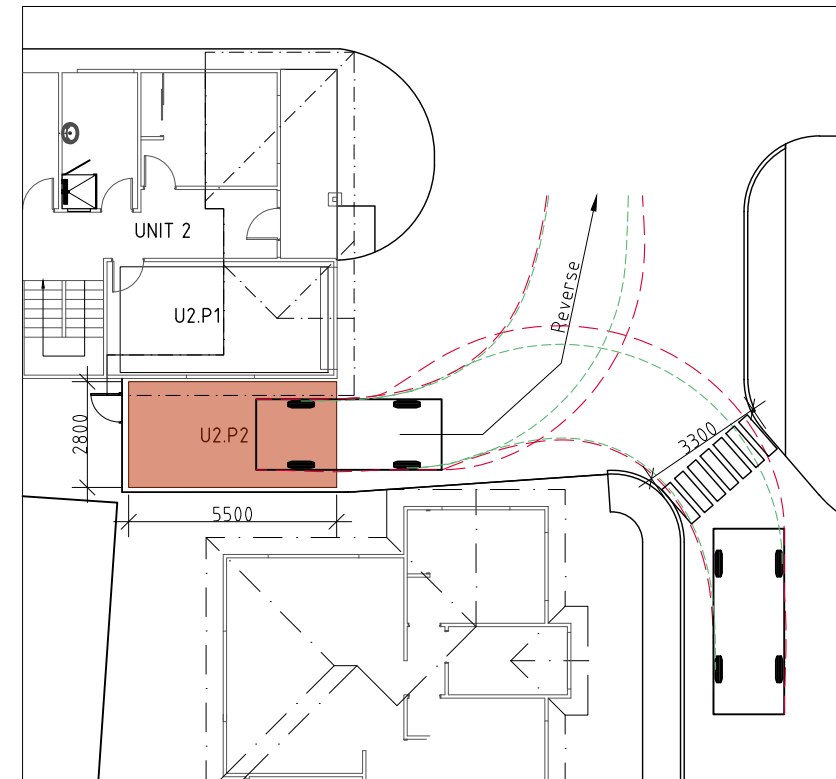
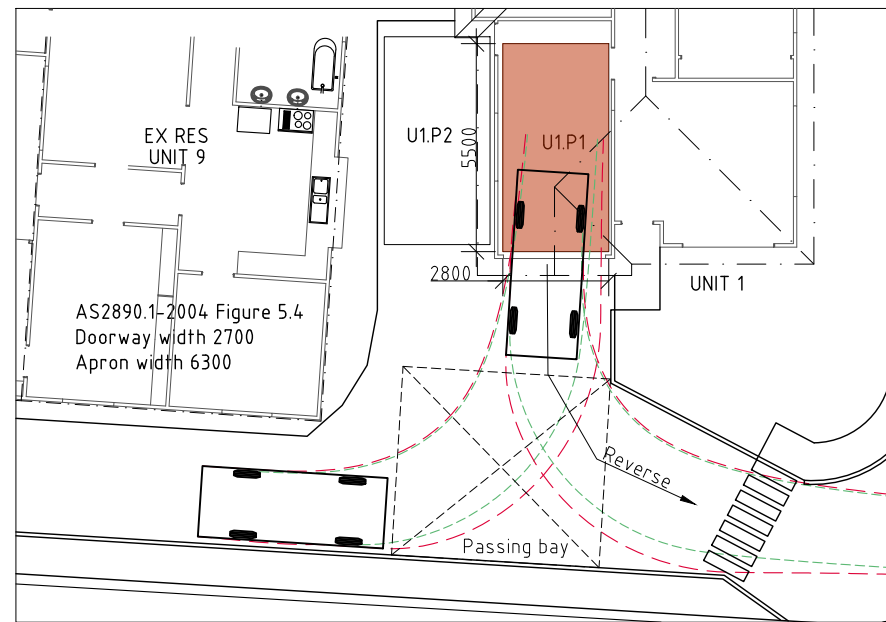
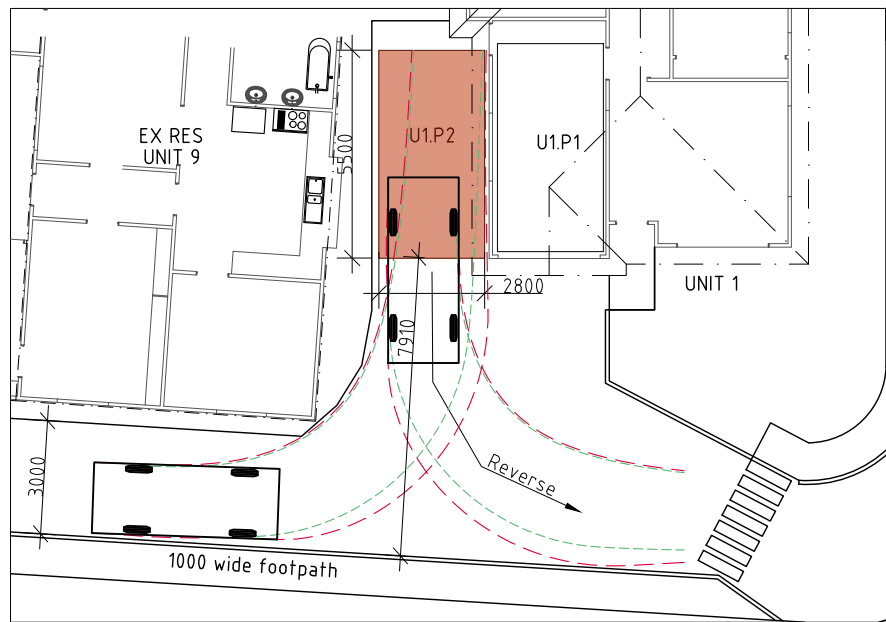
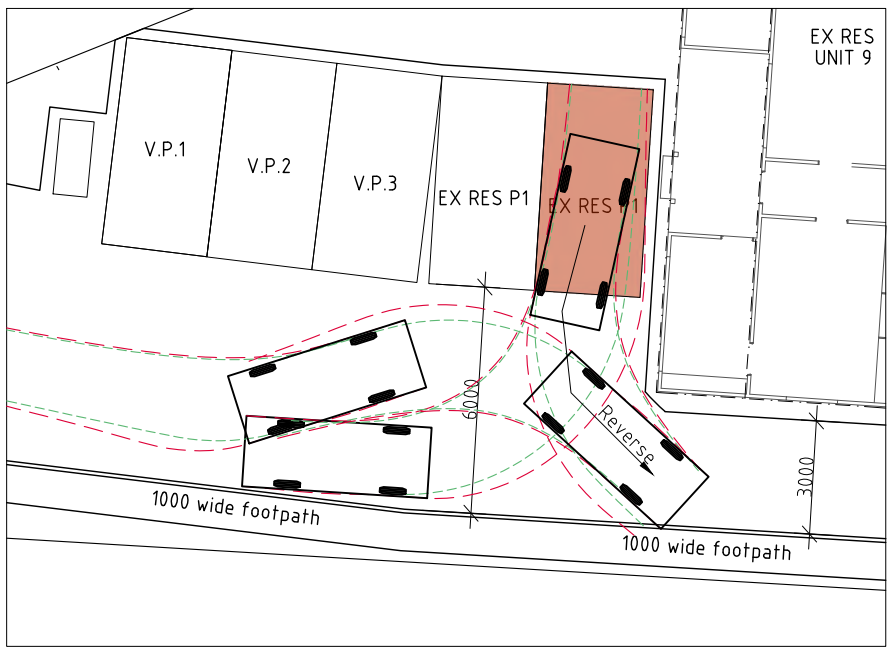
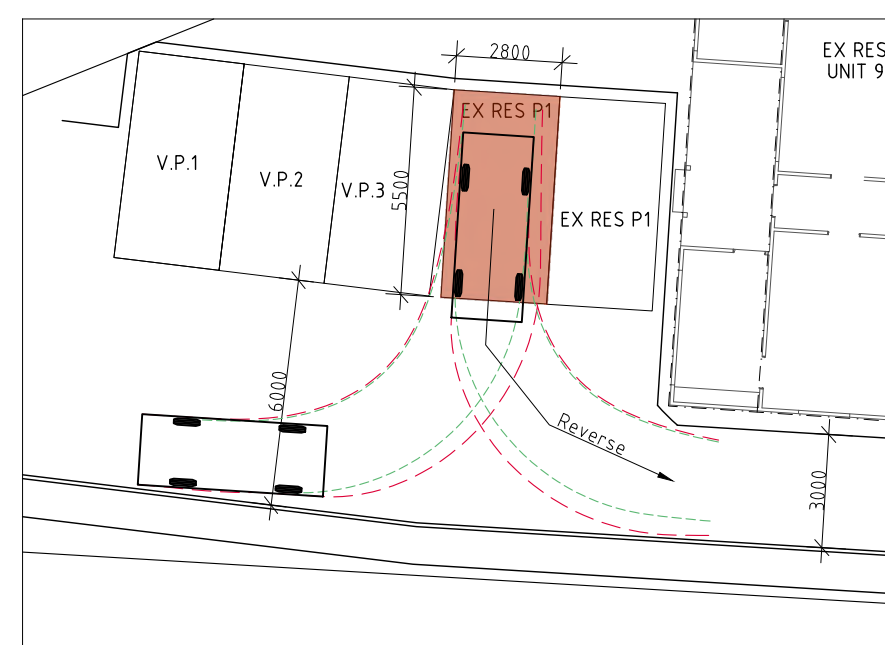
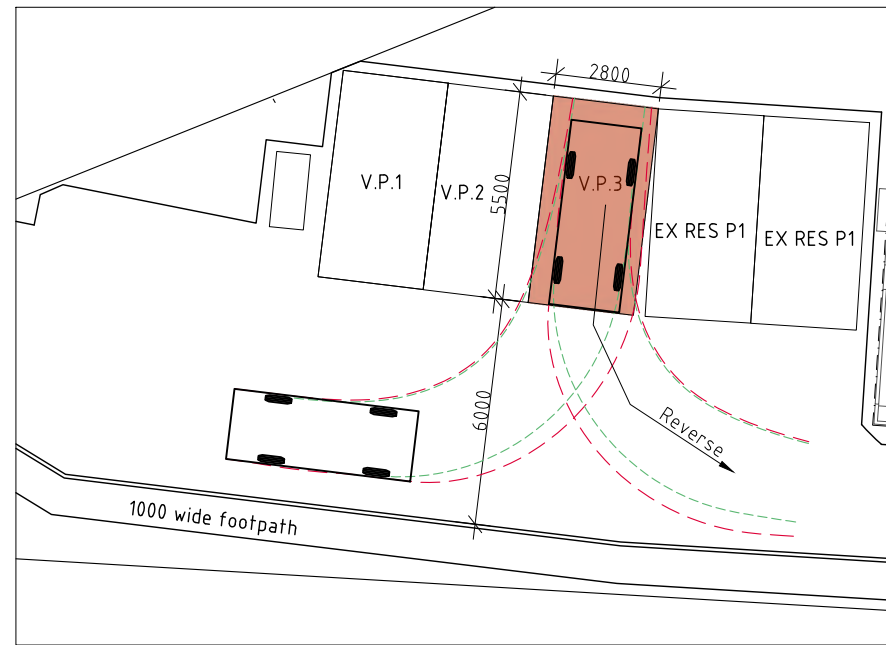
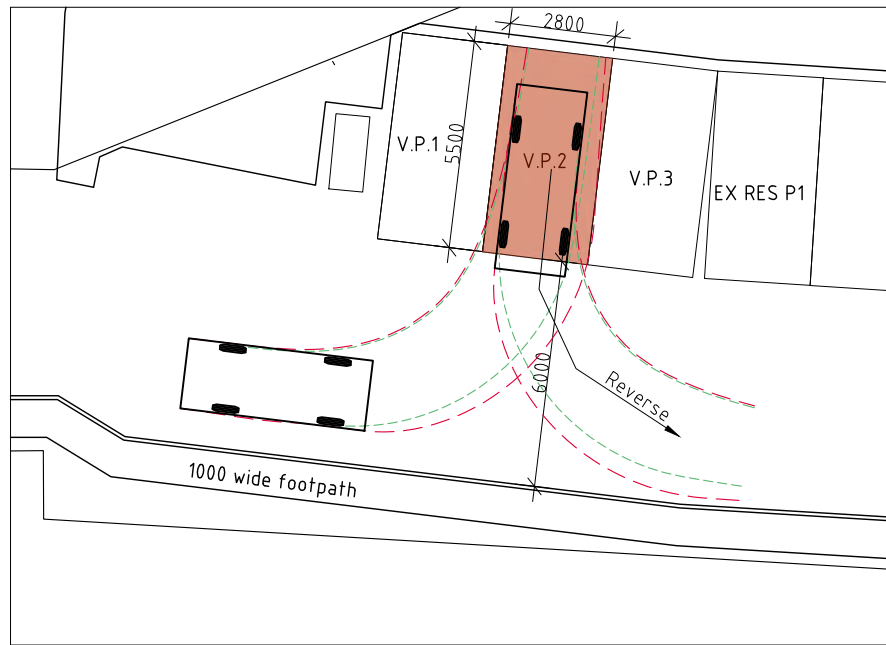
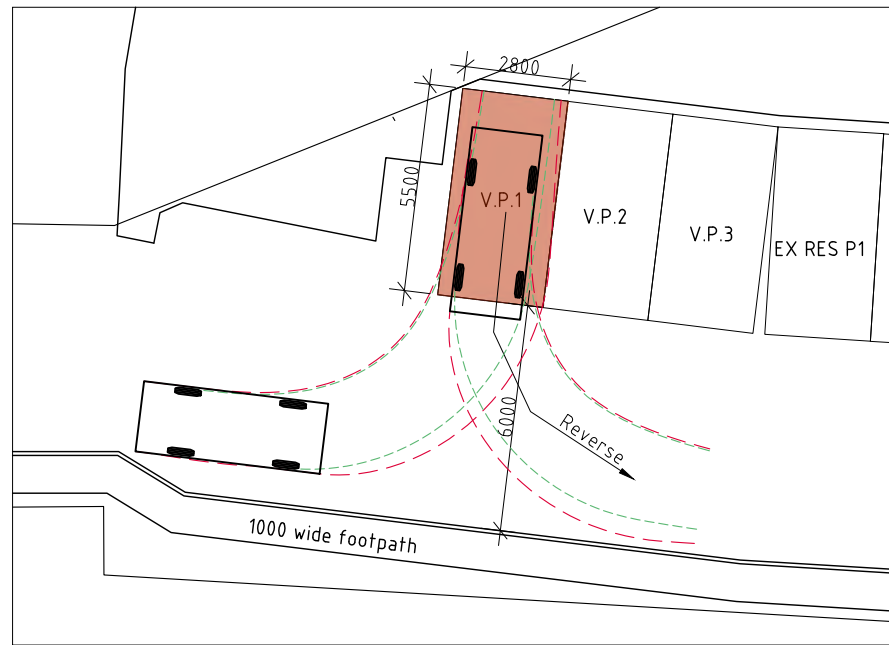
email: dillan7@bigpond.com

phone: 0400 671 582

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SLIM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD, CAMBRIDGE

VEHICLE TURNING PLANS

SCALE 1:200

AMENDED

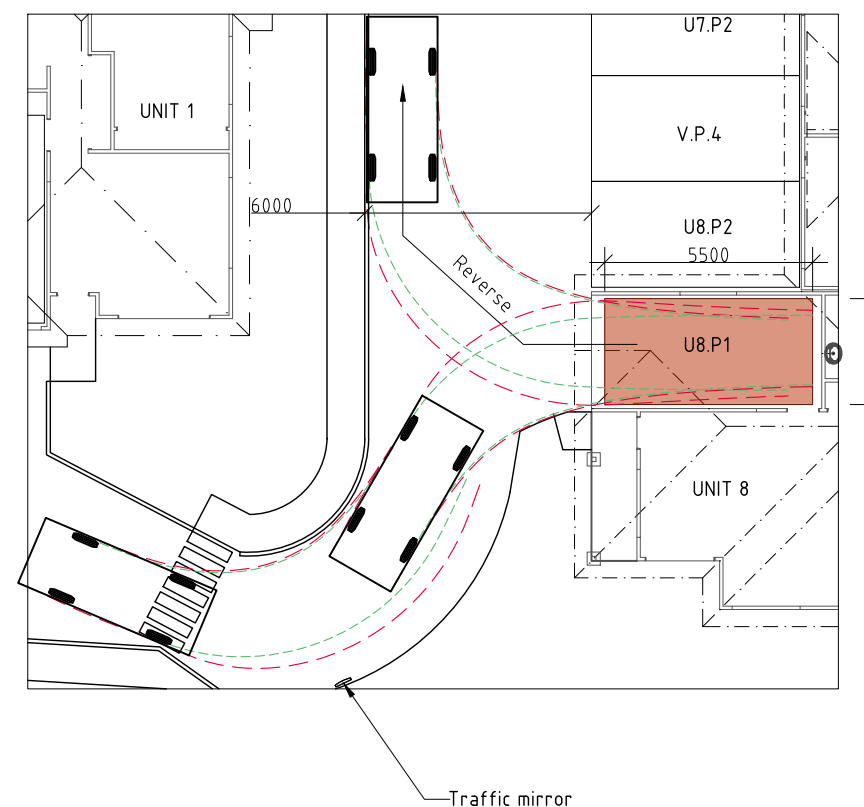
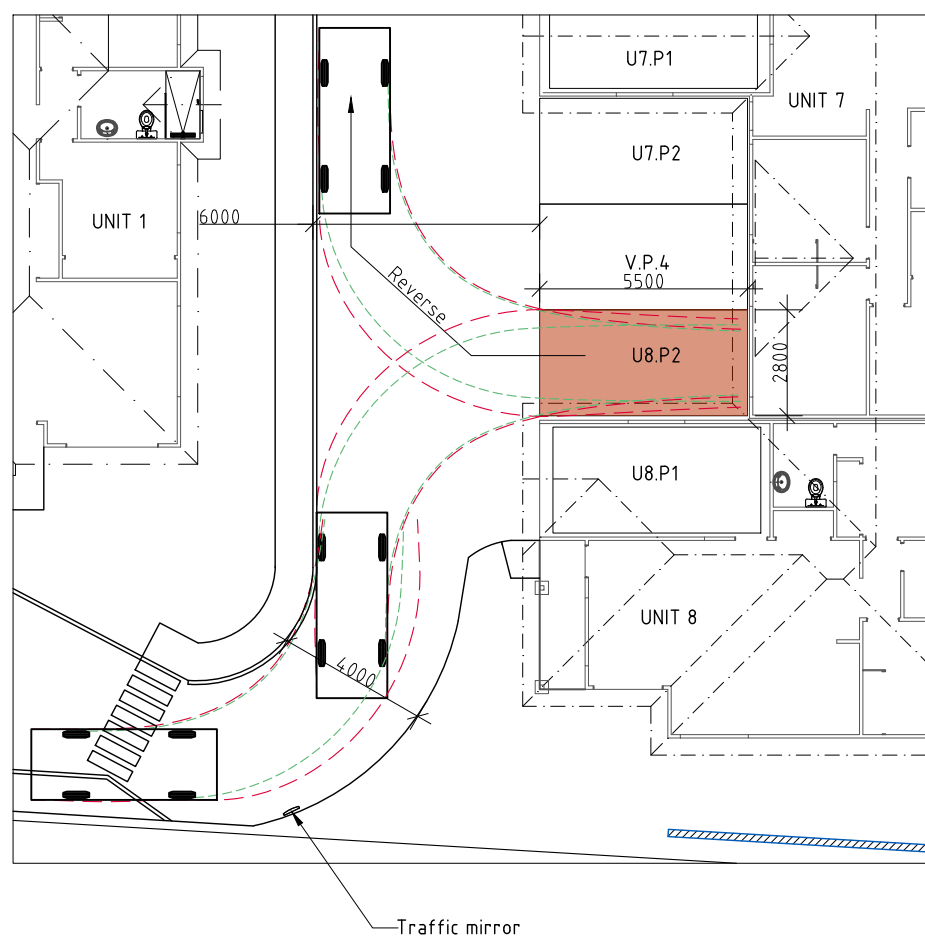
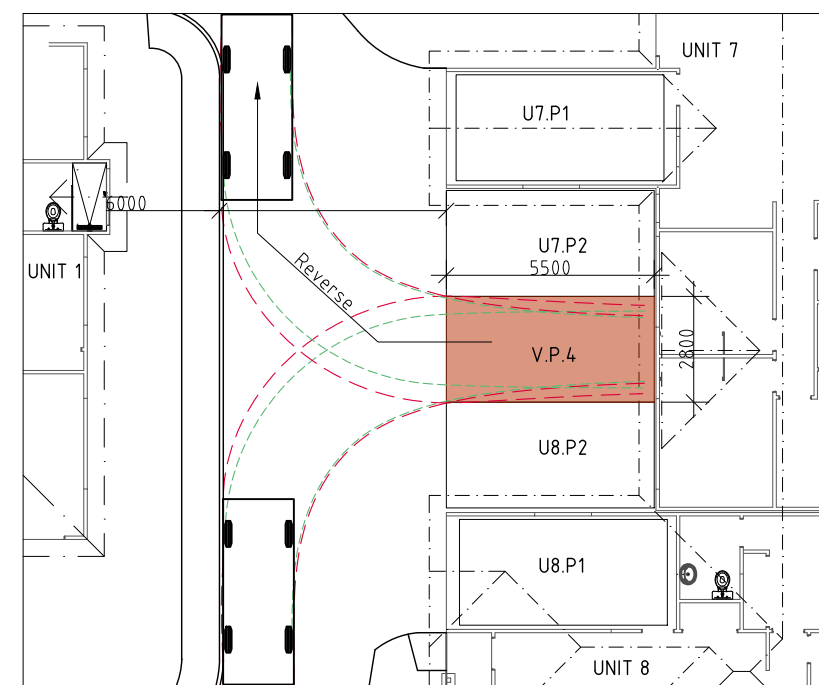
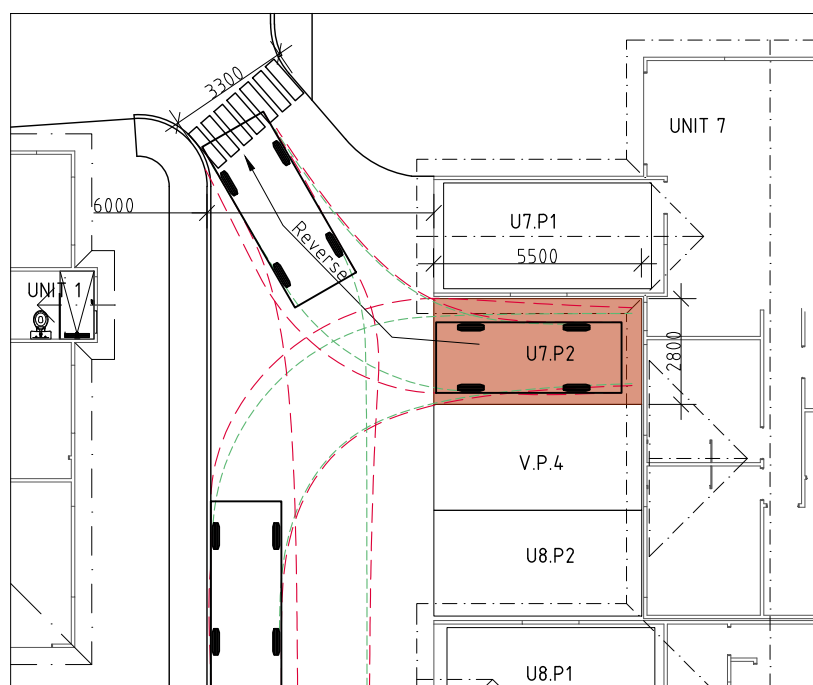
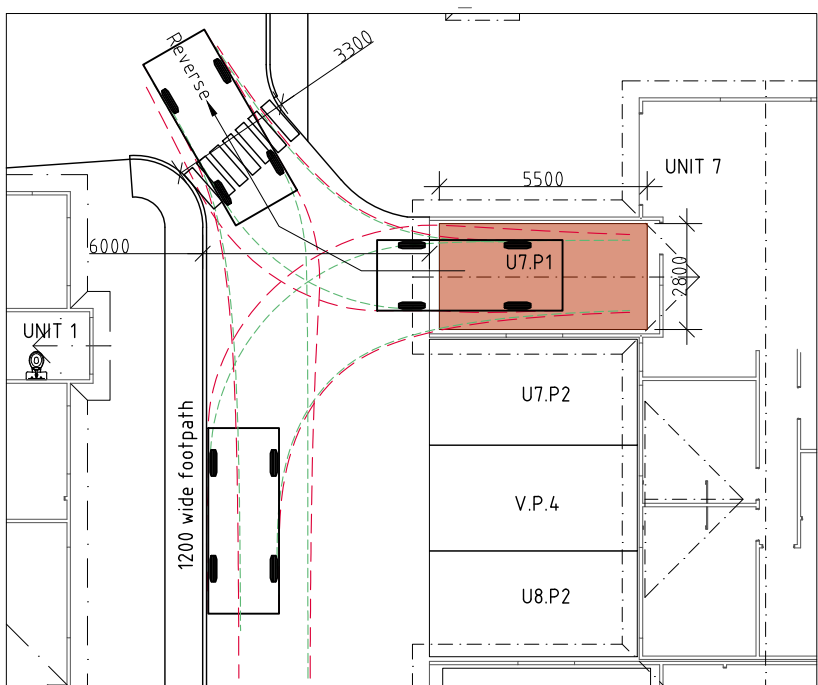
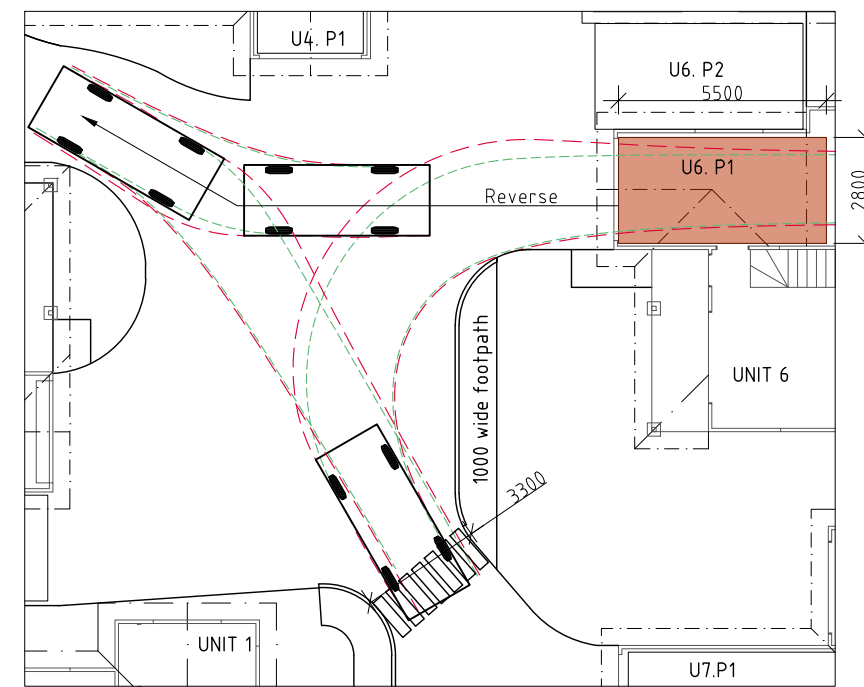
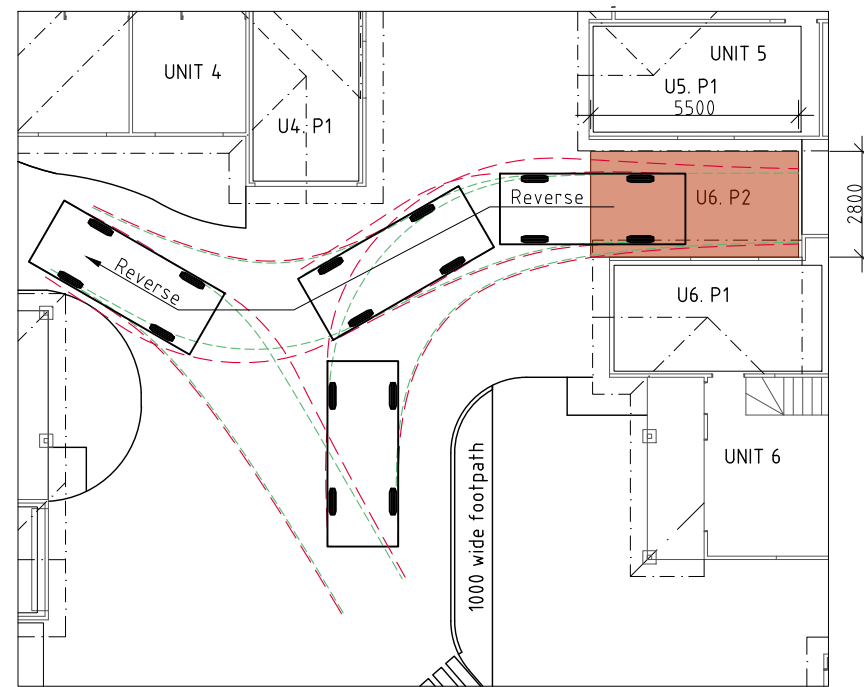
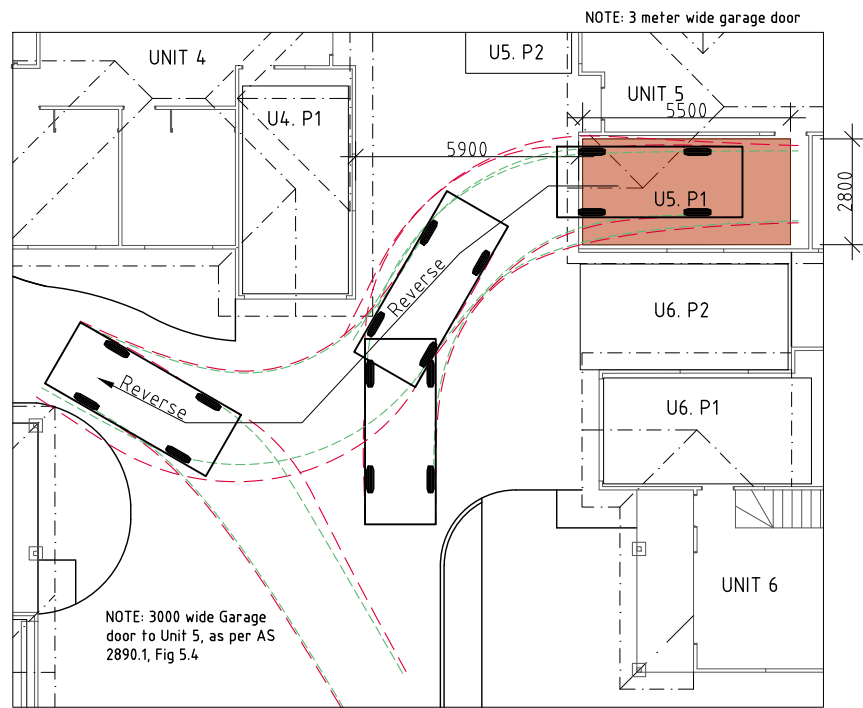
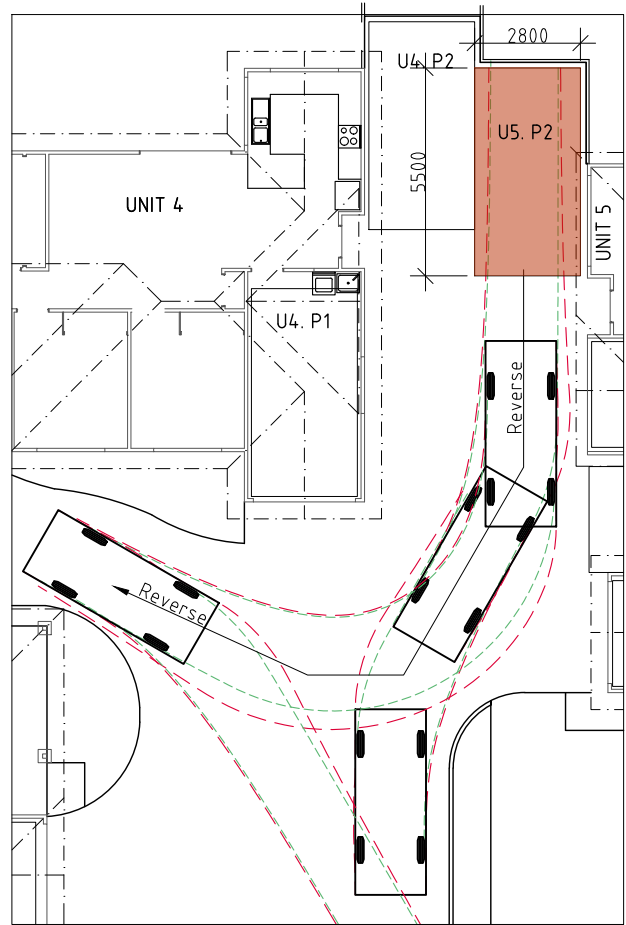
DATE
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05 OF 27

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SLIM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

VEHICLE TURNING PLANS 2

SCALE 1:200

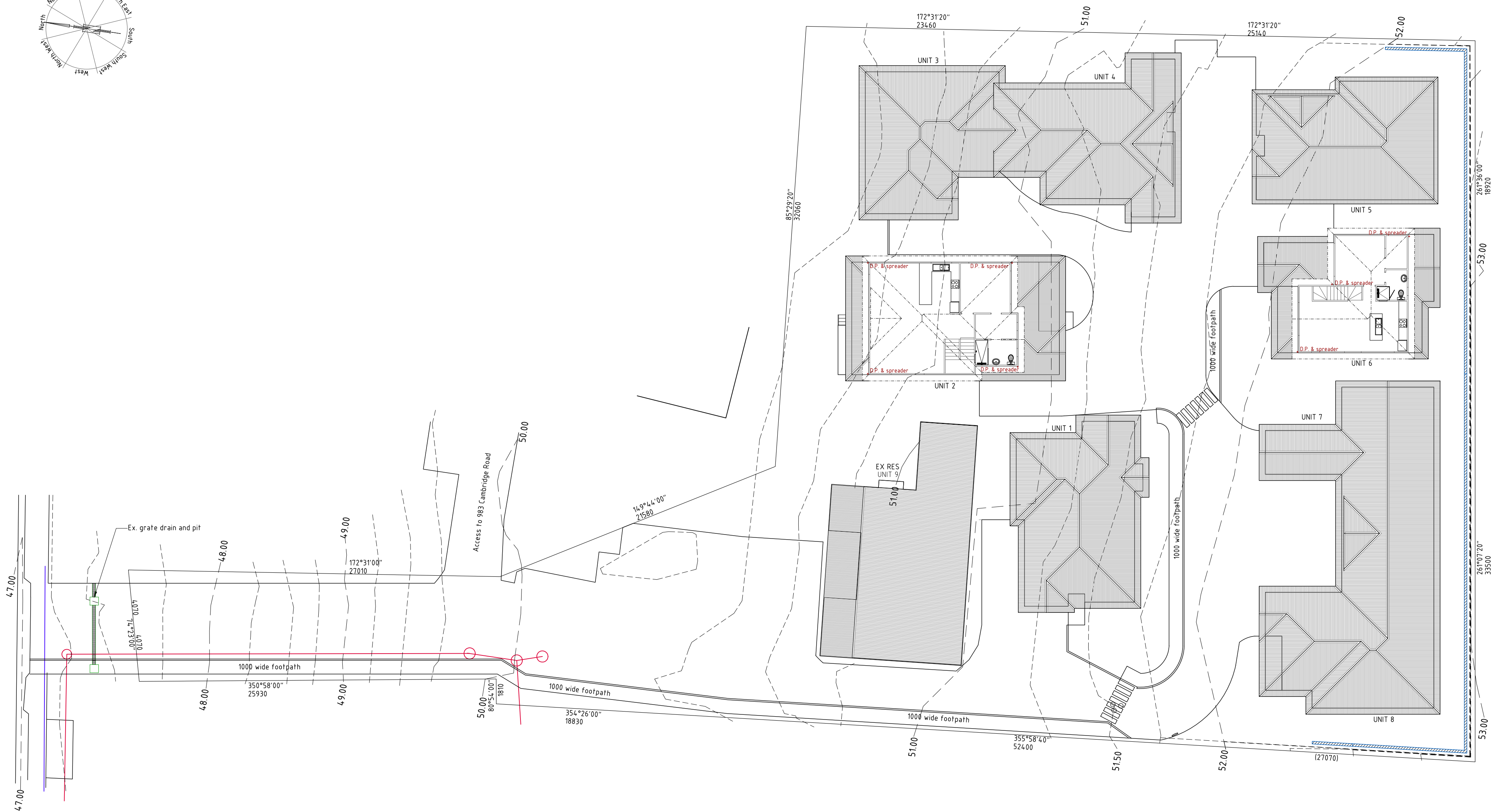
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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR SJM PROPERTY DEVELOPMENTS PTY LTD AT 981 CAMBRIDGE ROAD CAMBRIDGE	UPPER LEVEL STORMWATER PLAN		DATE 29/08/23	DRAWN BY email: dtbulla7@bigpond.com phone: ph 0400 671 582
	SCALE 1:200	AMENDED	DRAWING NO. 08 OF 27 A2	Accreditation No.CC620H  copyright 2022 6522

WET AREAS (To comply with NCC 3.8.1.2 and AS3740

VESSELS OR AREA WHERE THE FIXTURE IS INSTALLED	FLOORS AND HORIZONTAL SURFACES	WALLS	WALL JUNCTIONS AND JOINTS	WALL/FLOOR JUNCTIONS	PENETRATIONS
Shower areas (Ensuite & Bathroom)					
Without hob or step-down	Waterproof floor in shower area. Membrane 'M01' Ceramic floor tiles.	Waterproof walls in shower area to min. 150mm above the floor substrate. Membrane 'M01' Water resistant walls in shower area to min. 1800mm above FFL of the shower. Ceramic tiles	Waterproof wall junctions within shower area. Membrane 'M01'	Waterproof wall/floor junctions within shower area. Membrane 'M01'	Waterproof floor penetrations in shower area with Membrane 'M01' Waterproof tap & spout penetrations with 'Waterbar' tap penetration flange & silicone.
With preformed shower base	N/A	Water resistant walls in shower area to min. 1800mm above FFL of the shower. Ceramic tiles.	Waterproof wall junctions within shower area. Membrane 'M01'	Waterproof wall/floor junctions within shower area. Membrane 'M01'	
Area outside shower area (Ensuite & Bathroom)					
For concrete and compressed fibre-cement sheet flooring	Min. water resistant floor of the room. Membrane 'M01' to entire floor of room. Ceramic floor tiles.	N/A	N/A	Waterproof wall/floor junctions Membrane 'M02'	N/A
Areas adjacent to baths and spas (Ensuite & Bathroom)					
For concrete and compressed fibre-cement sheet flooring & inserted baths & spas	Min. water resistant floor of the room. Membrane 'M01' to entire floor of room. Waterproof shelf area with waterstop under the bath lip. Membrane 'M01' Ceramic tiles.	(a) Min. 150mm high ceramic tile splashback to bath perimeter of room. (b) Ceramic tile upstand from floor level to underside of bath lip.	Water resistant junctions within 150mm above bath with white silicone (3xwalls).	Water resistant ceramic tile upstand to extent of bath.	Waterproof tap and spout penetrations in horizontal surface with 'Waterbar' tap penetration flange & silicone.
Other areas					
Laundries and WC	Water resistant floor of the room. Ceramic floor tiles	N/A	N/A	Water resistant wall/floor junctions. Membrane 'M02' & ceramic tile skirting.	N/A
Walls adjoining sink, basin, or laundry tub	N/A	Water resistant to min.150mm above the vessel, for the extent of the vessel, where the vessel is within 75mm of a wall. Ceramic tiles.	Waterproof wall junctions where a vessel is fixed to a wall with silicone	N/A	Waterproof tap & spout penetrations if within splashback with 'Waterbar' tap penetration flange & silicone.
KEY: Membrane 'M01' = Dunlop (or similar) approved shower waterproofing kit complete with reinforcing mat, primer, neutral cure silicone & membrane to manufacturers recommendations. Membrane 'M02' = Dunlop (or similar) approved water based acrylic polyurethane membrane applied by either brush or roller in a consistent thickness to manufacturers recommendations.					

Shower screens
1800H Semi-frameless shower screens to comply with BCA Table 3.6.5. & AS1288. Minimum 4mm thick Grade A toughened safety glass, labelled to comply with industry standards.

WATERPROOFING OF UNENCLOSED SHOWERS

FLOOR: Waterproof entire floor
WALLS: Waterproof to not less than 150mm above the shower floor substrate or not less than 25mm above the maximum retained water level with the remainder being water resistant to a height of not less than 1800mm above the finished floor level.

WALL JUNCTIONS AND JOINTS: Waterproof internal and external corners and horizontal joints within a height of 1800mm above the floor level with not less than 40mm width either side of the junction

PENETRATIONS: Waterproof all penetrations

AREAS OUTSIDE THE SHOWER ON CONCRETE SLAB;

FLOORS:Water resistant to entire floor
WALL JUNCTIONS AND JOINTS; Waterproof all wall/floor junctions
Where a flashing is used the horizontal leg must be not less than 40mm

AREAS OUTSIDE THE SHOWER ON TIMBER FLOOR;

FLOORS: Waterproof entire floor
WALL JUNCTIONS AND JOINTS; Waterproof all wall/floor junctions
Where a flashing is used the horizontal leg must be not less than 40mm

WATERPROOFING

Enclosed shower with preformed shower base
Walls to be water resistant to a height of not less than 1800mm above finished floor level
Waterproof internal & external corners and horizontal joints within a height of 180mm above the floor level with not less than 40mm width either side of the junctions
Waterproof all penetrations

HYDRAULIC NOTES:

- All plumbing shall be in accordance with the Tasmanian plumbing regulations, AS3500 and to the local authority approval.
- The location of the existing services where shown are approximate only and shall be confirmed on site where possible. Determine location of existing power, Telstra, water and drainage services prior to commencing new work.
- Conceal all pipework in ceiling space, ducts, cavities, wall chases, cupboards etc unless otherwise approved.
- Refer to designers drawings and fixture and equipment technical specifications for pipework connections.
- Make good all disturbed surfaces to match existing
- Remove all excess soil and surplus materials from site.
- All plumbing to be installed by a licenced Plumber.

Surface drainage to conform with BCA Vol2 Part 3.1.2.2. NOTE: 50mm fall required over the first 1 meter from the building

Install inspection openings at major bends for stormwater and all low points of downpipes.
All plumbing & drainage to be in accordance with local Council requirements.
Provide surface drain to back of bulk excavation to drain levelled pad prior to commencing footing excavation.
Stormwater line (100mm UPVC)
Sewer line (100mm UPVC)

Services

The heated water system must be designed and installed with Part B2 of NCC Volume Three – Plumbing Code of Australia.

Thermal insulation for heated water piping must:

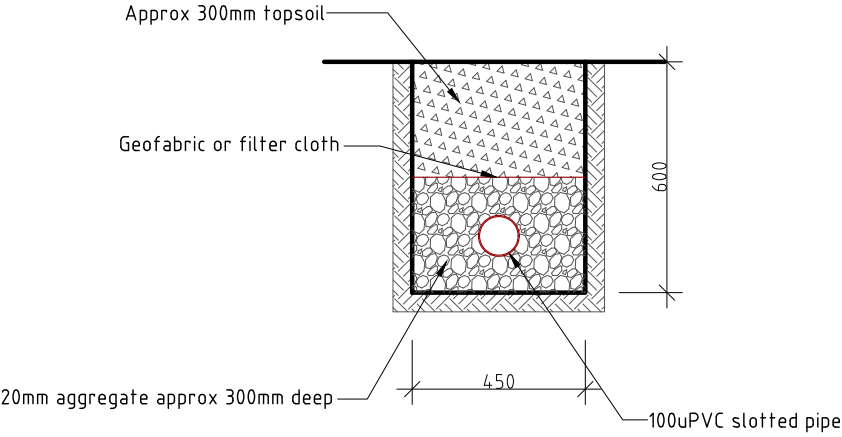
- be protected against the effects of weather and sunlight; and
- be able to withstand the temperatures within the piping; and
- use thermal insulation in accordance with AS/NZS 4859.1

Heated water piping that is not within a conditioned space must be thermally insulated as follows:

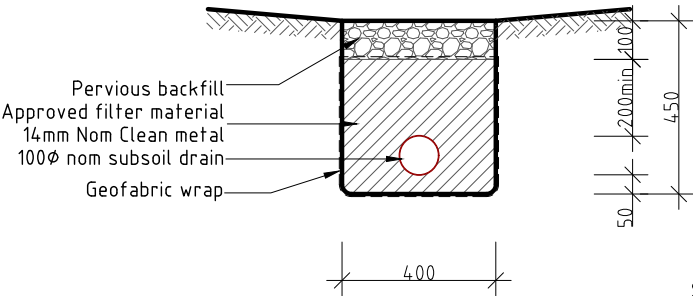
1. Internal piping

- All flow and return internal piping that is –
 - within an unventilated wall space
 - within an internal floor between storeys, or
 - between ceiling insulation and a ceilingMust have a minimum R-Value of 0.2 (ie 9mm of closed cell polymer insulation)
- Piping located within a ventilated wall space, an enclosed building subfloor or a roof space
 - All flow and return piping
 - Cold water supply piping and Relief valve pipingwithin 500mm of the connection to central water heating system
Must have a minimum R-Value of 0.45 (ie 19mm of closed cell polymer insulation)
 - Piping located outside the building or in an unenclosed building sub-floor or roof space
 - All flow and return piping
 - Cold water supply piping and Relief valve pipingwithin 500mm of the connection to central water heating system
Must have a minimum R-Value of 0.6 (ie 25mm of closed cell polymer insulation)

Piping within an insulated timber framed wall, such as that passing through a wall stud, is considered to comply with the above insulation requirements



ABSORPTION TRENCH (TYPICAL)



TYPICAL AGG DRAIN DETAIL

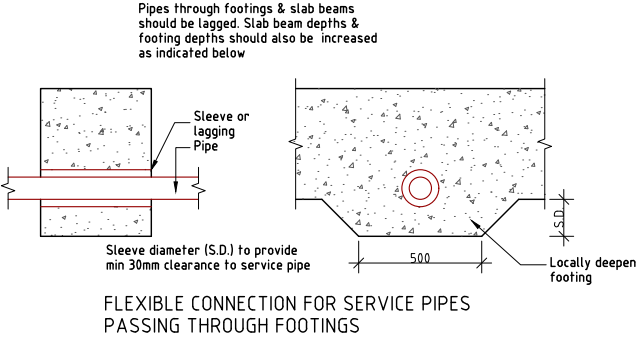
NOTE: All materials and construction to comply with AS3500.3.2003

TRENCH WIDTHS	
Pipe diameter	Min trench width
Less than 50mm	250
75-100mm	450
150-300mm	600
>300mm	Ø plus 300mm

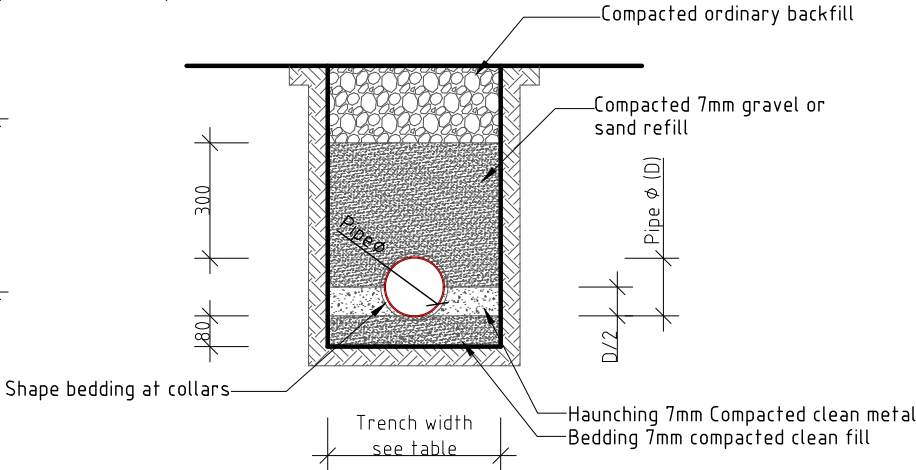
Hot & Cold water nominal diameters	
Branch off takes	Min DN20
Max off take length 6m	DN18
Max off take length 3m	DN15
Max off take length 1m	DN10

Insulation Schedule		
Heated water pipes		
Type	Size Range	Insulation
Circulating Line	32-40	25mm Rockwool with foil wrap
Branch Line Offtake	20-25 18	19mm Bradflex 13mm Bradflex
Cold water pipes exposed		
Type	Size Range	Insulation
All	>20	13mm Bradflex
Other cold watere pipes		
Type	Size Range	Insulation
All	All	Not required

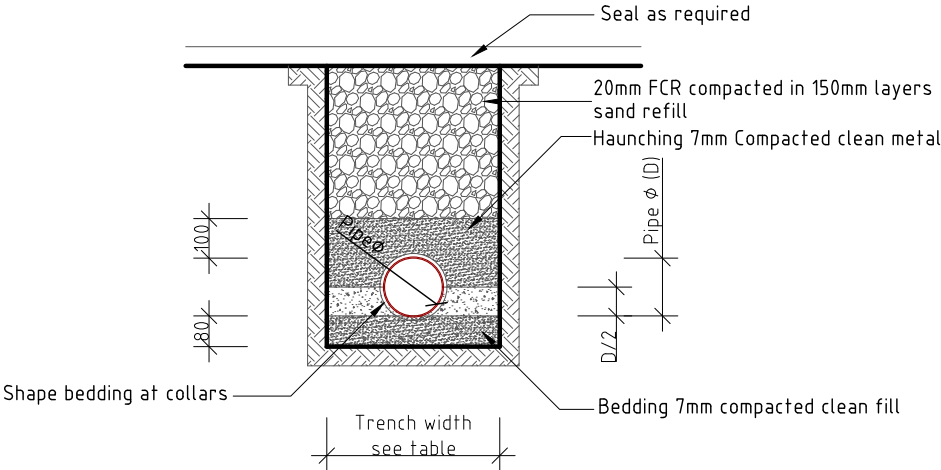
NOTE: Water pipes associated directly with plant equipment shall be insulated in accordance with the manufacturers instructions for a typical installation



FLEXIBLE CONNECTION FOR SERVICE PIPES PASSING THROUGH FOOTINGS

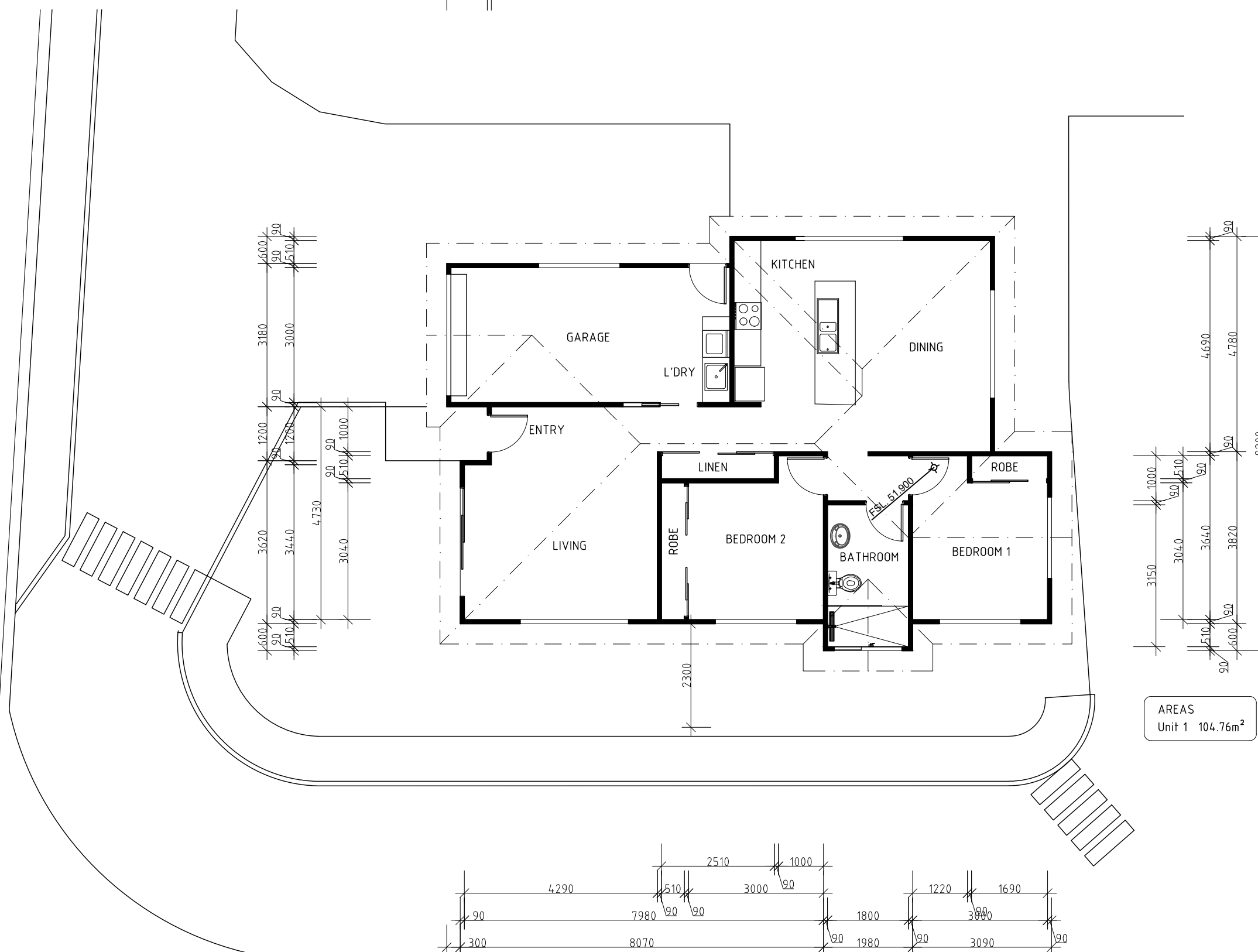
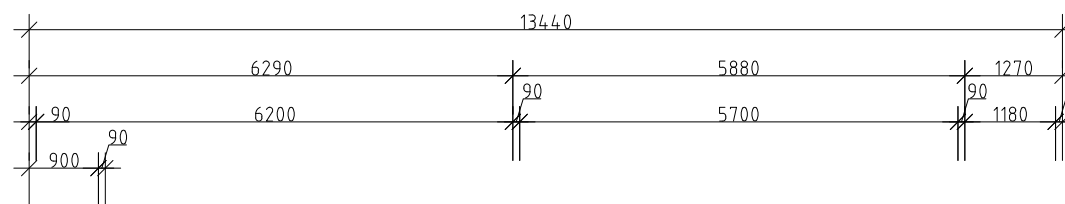


TYPICAL PIPE TRENCH DETAIL NON-TRAFFICABLE AREAS



TYPICAL PIPE TRENCH DETAIL TRAFFICABLE AREAS

IMPORTANT NOTICE FOR ATTENTION OF OWNER.
The owner's attention is drawn to the fact that foundations and associated drainage in all sites requires continuing maintenance to assist footing performance. Advice for foundation maintenance is contained in the CSIRO Building Technology File 18 and it is the owner's responsibility to maintain the site in accordance with this document.



AREAS
Unit 1 104.76m²

UNIT 1

DEVELOPMENT DRAWINGS ONLY
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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT I PLAN

SCALE 1:100

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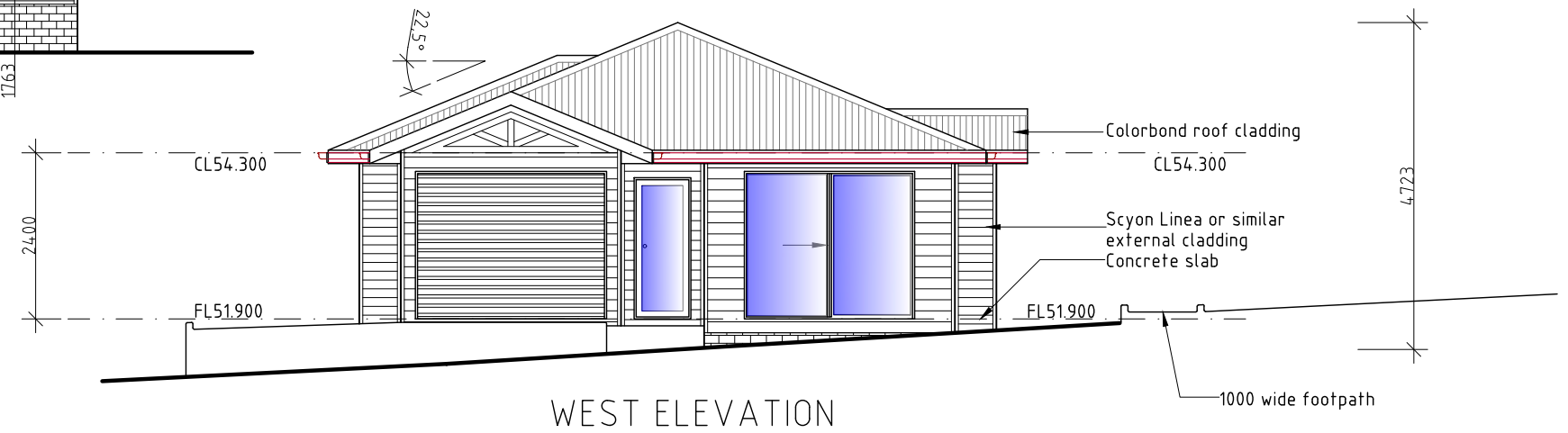
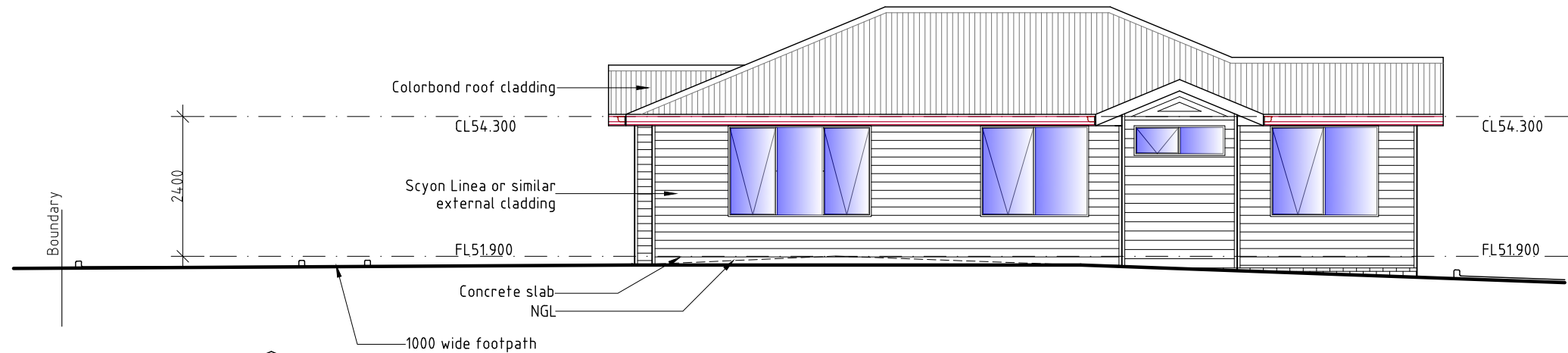
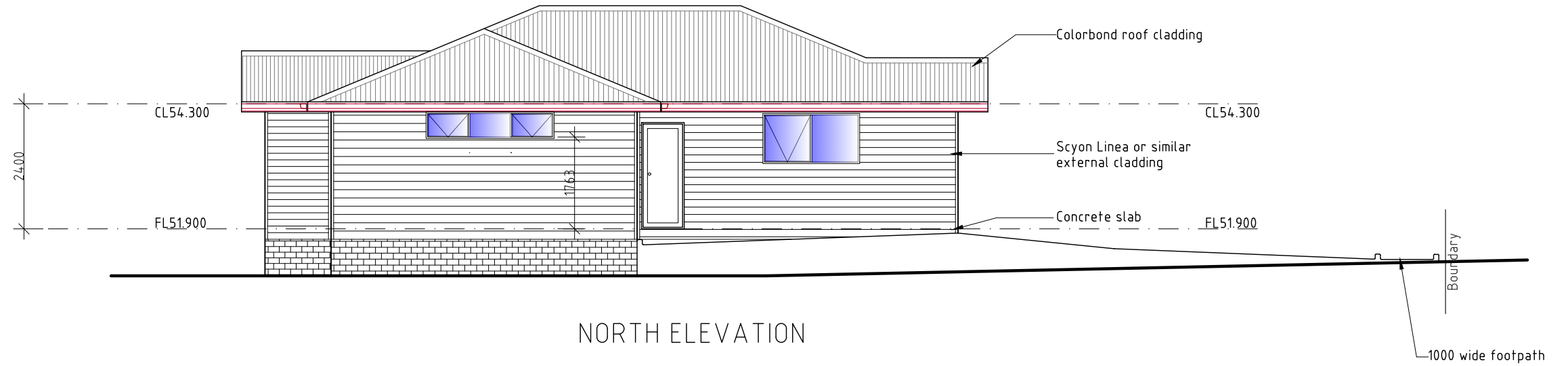
DATE
29/08/23

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DRAWN BY G Tilley
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Agenda Attachments - 981 Cambridge Road, Cambridge Page 11 of 35



UNIT 1

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 1 ELEVATIONS

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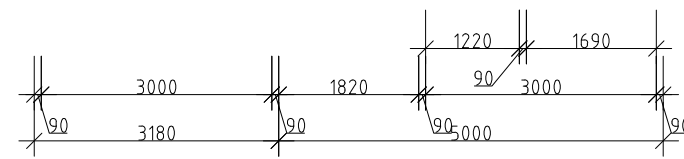
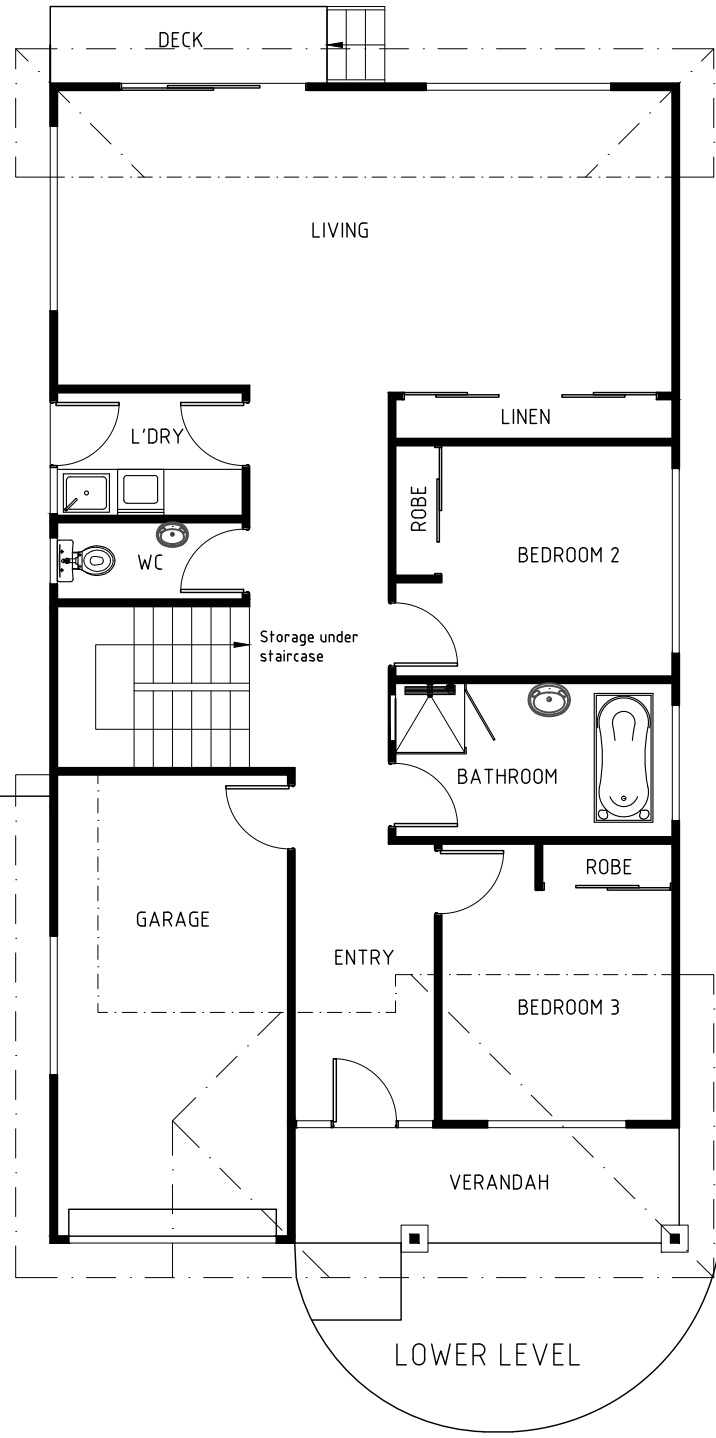
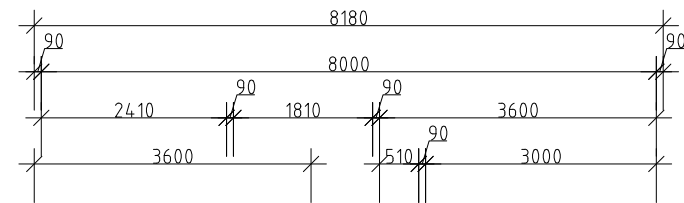
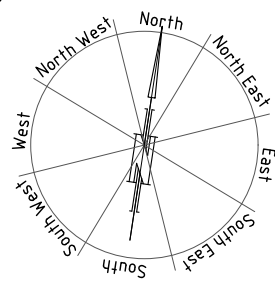
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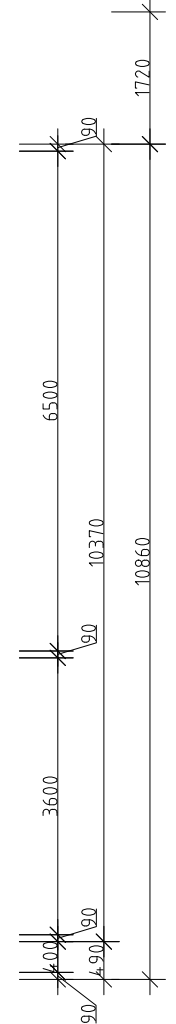
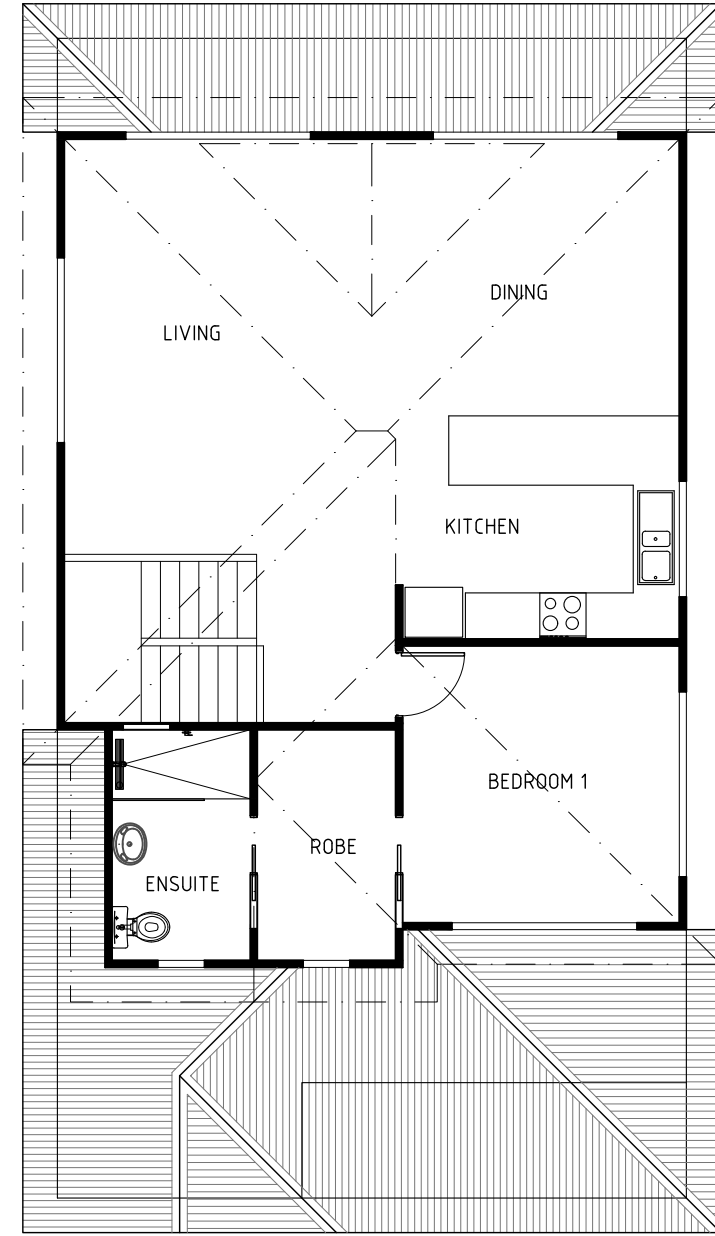
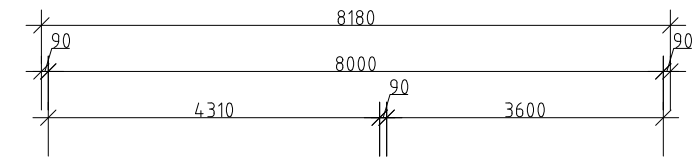
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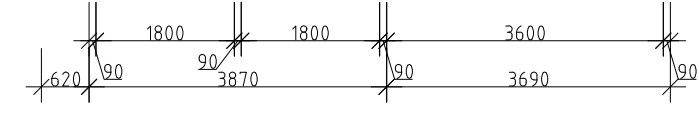
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AREAS Unit 2	
Lower level	115.04m ²
Upper level	85.11m ²
Total	200.15m ²



UPPER LEVEL



UNIT 2

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 2 PLAN

DATE
29/08/23

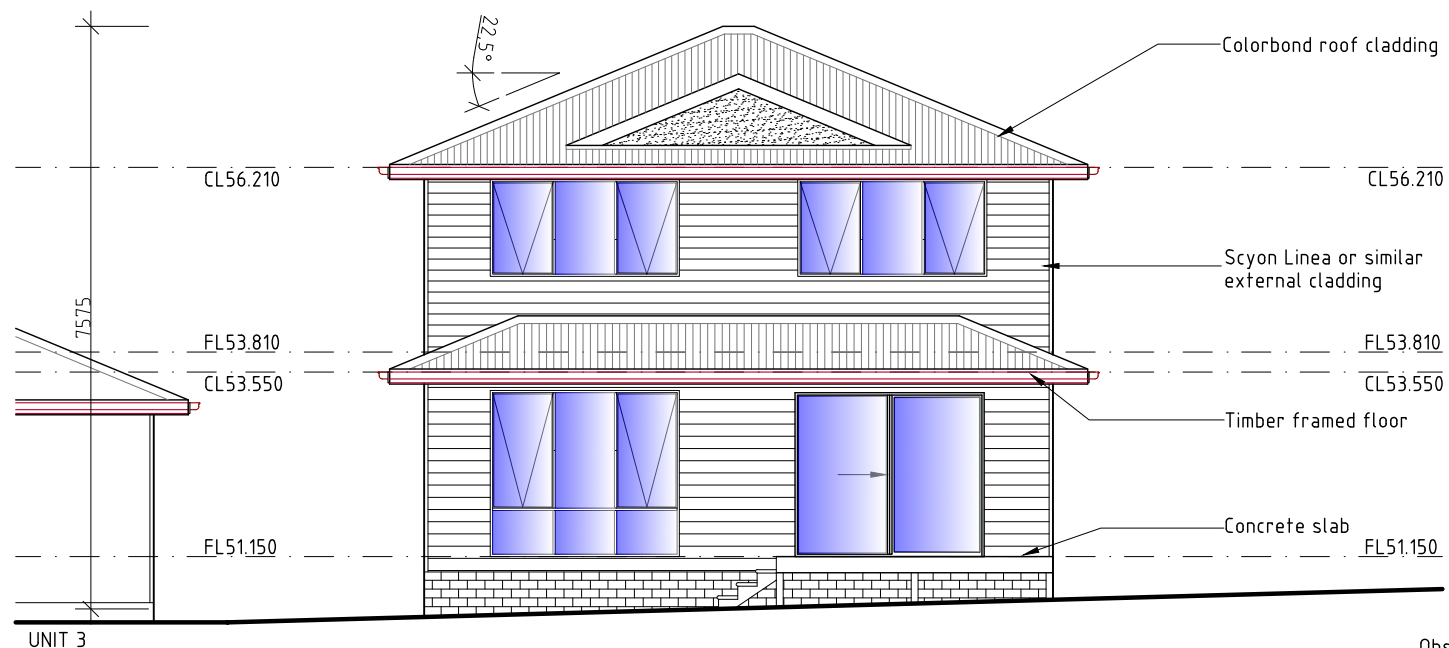
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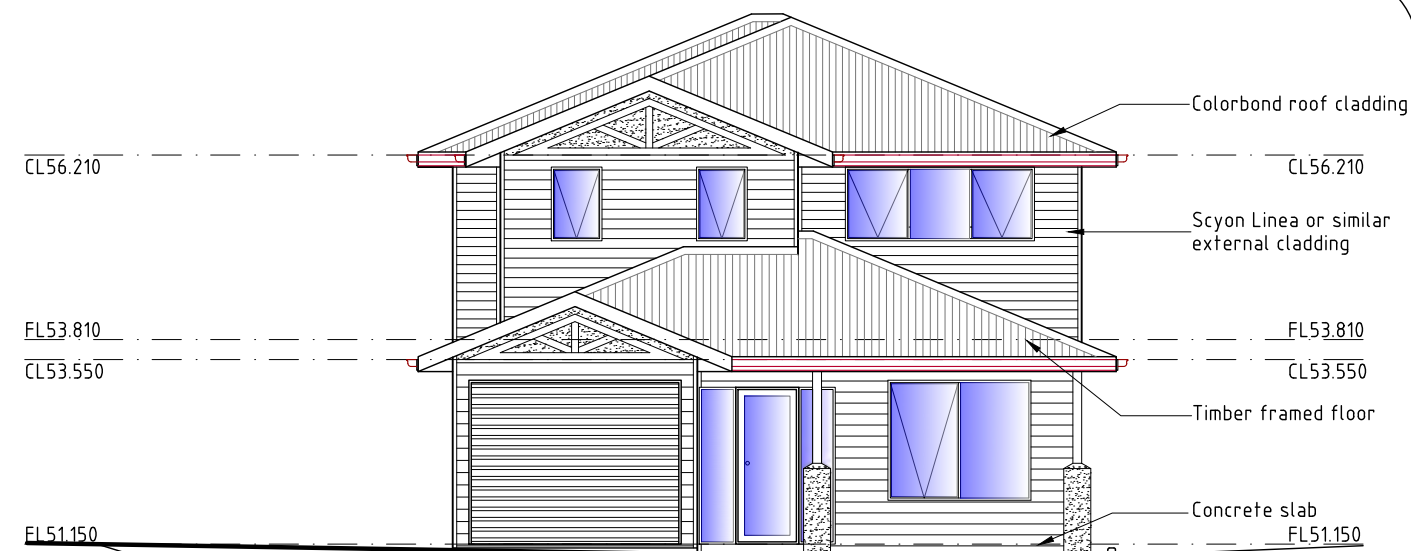
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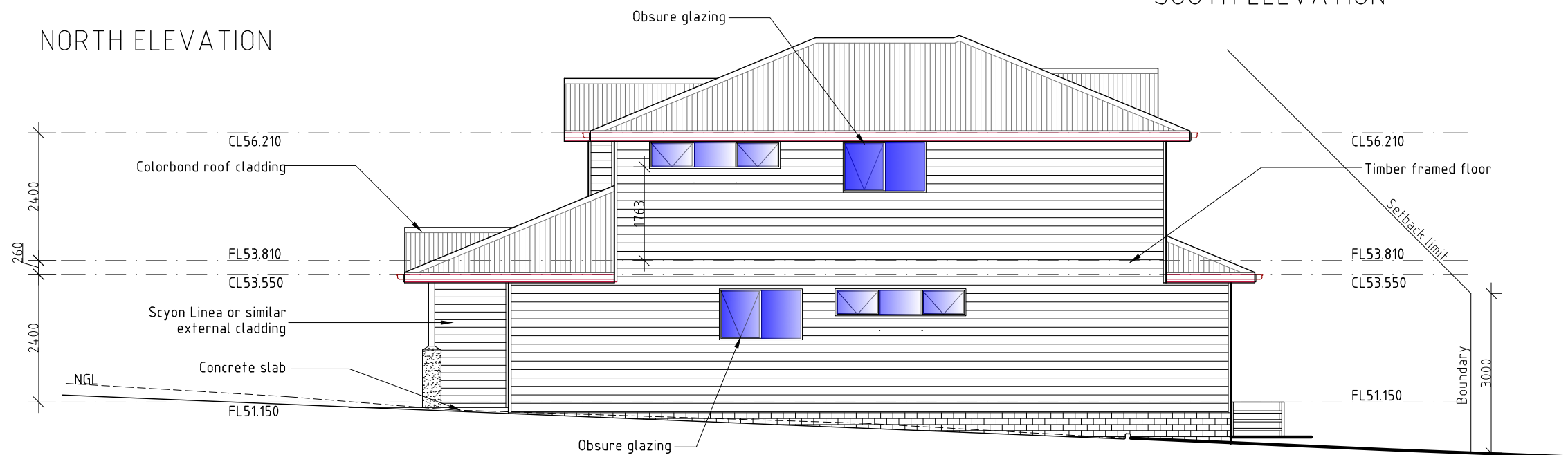
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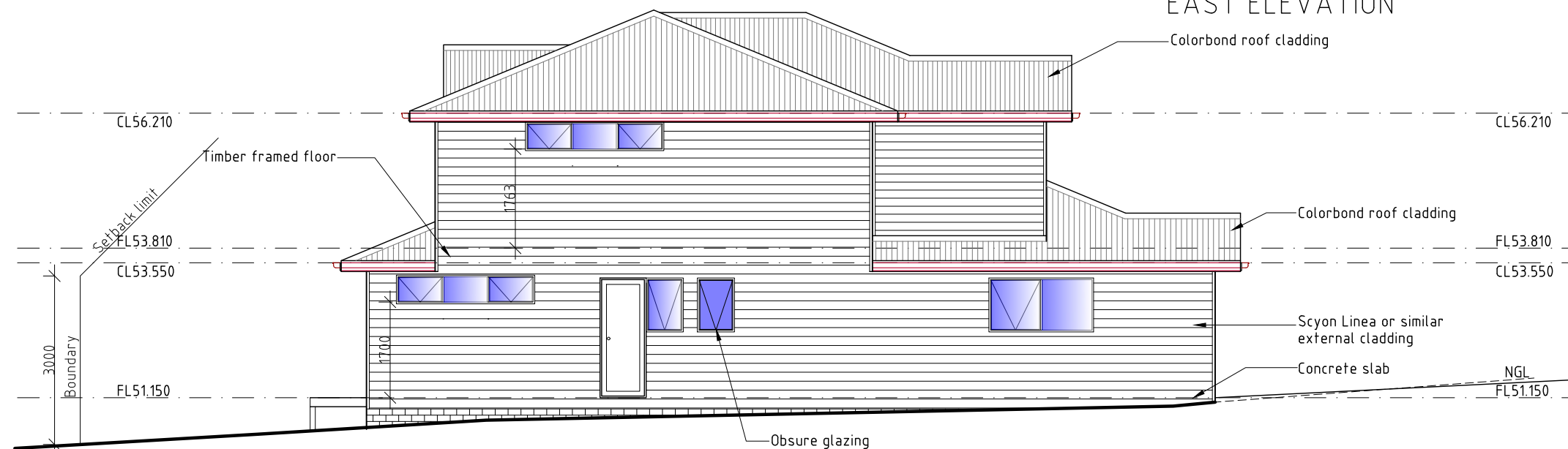
NORTH ELEVATION



SOUTH ELEVATION



EAST ELEVATION



WEST ELEVATION

UNIT 2

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 2 ELEVATIONS

SCALE 1:100

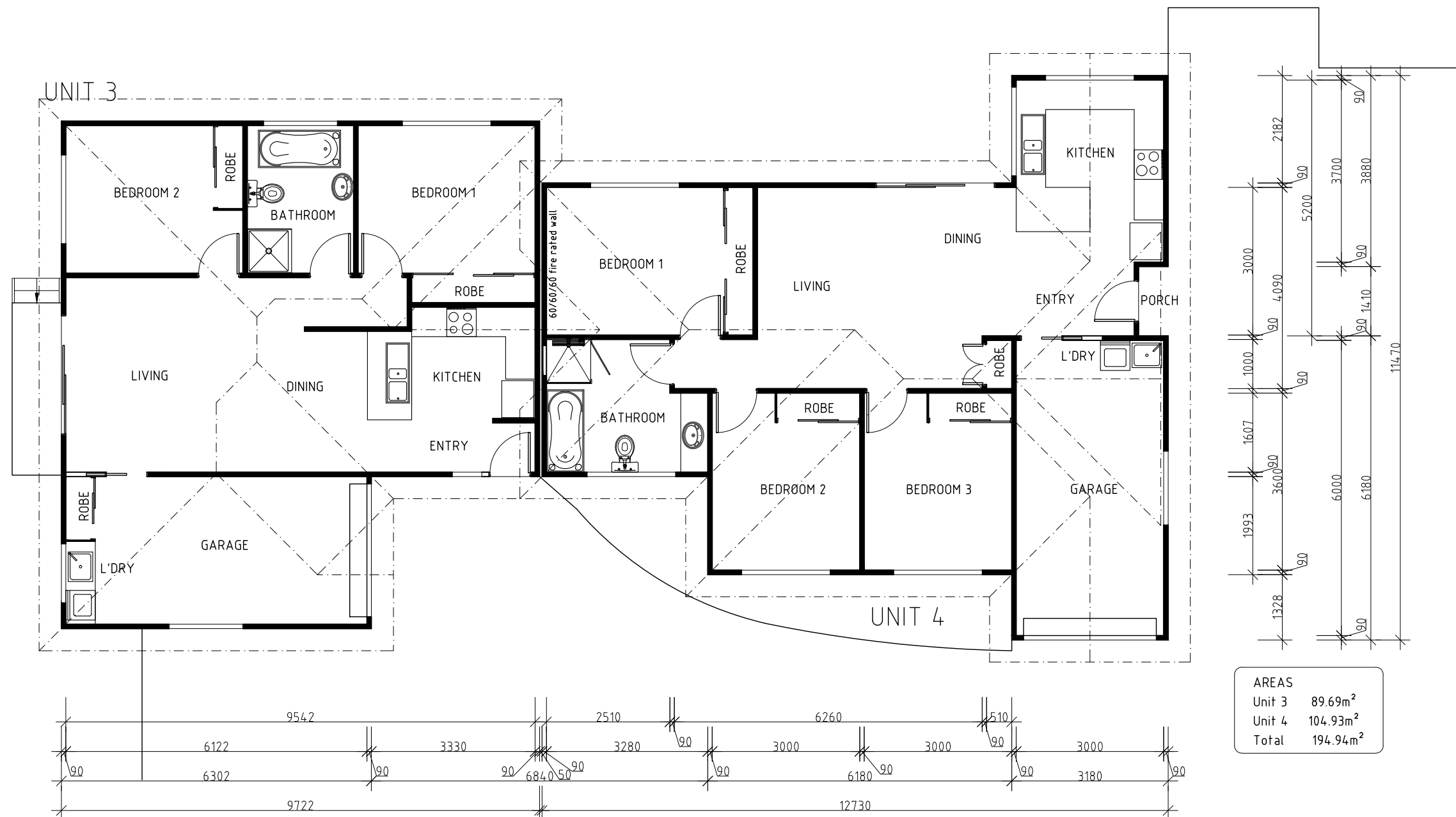
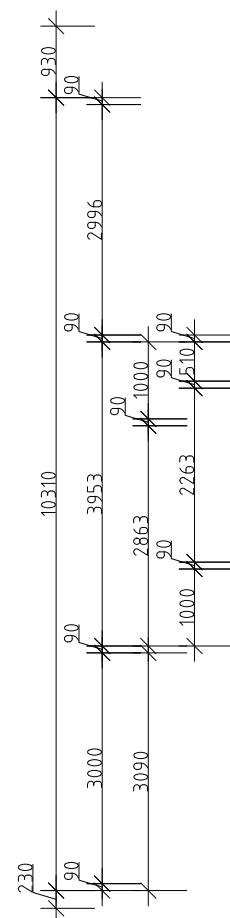
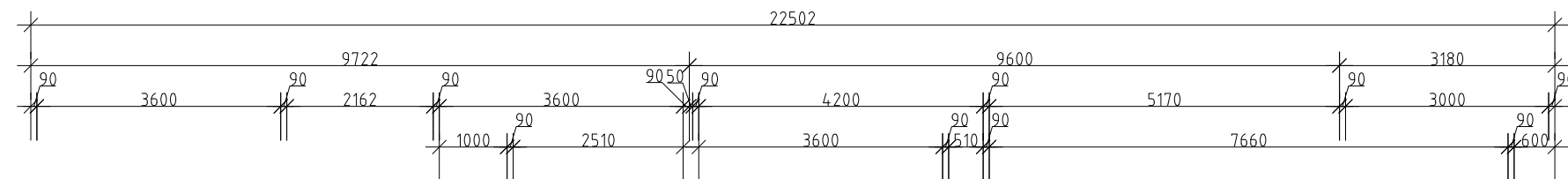
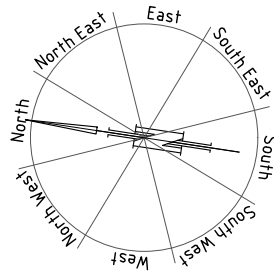
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AREAS	
Unit 3	89.69m ²
Unit 4	104.93m ²
Total	194.94m ²

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UNIT 3 & 4

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 3 & 4 PLAN

DATE
29/08/23

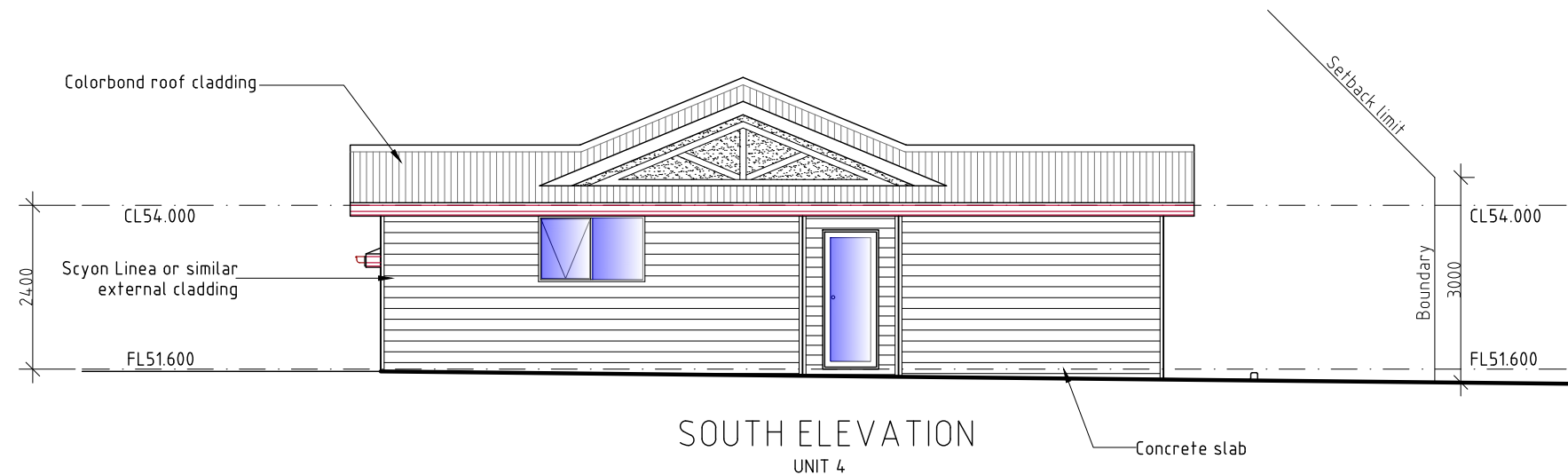
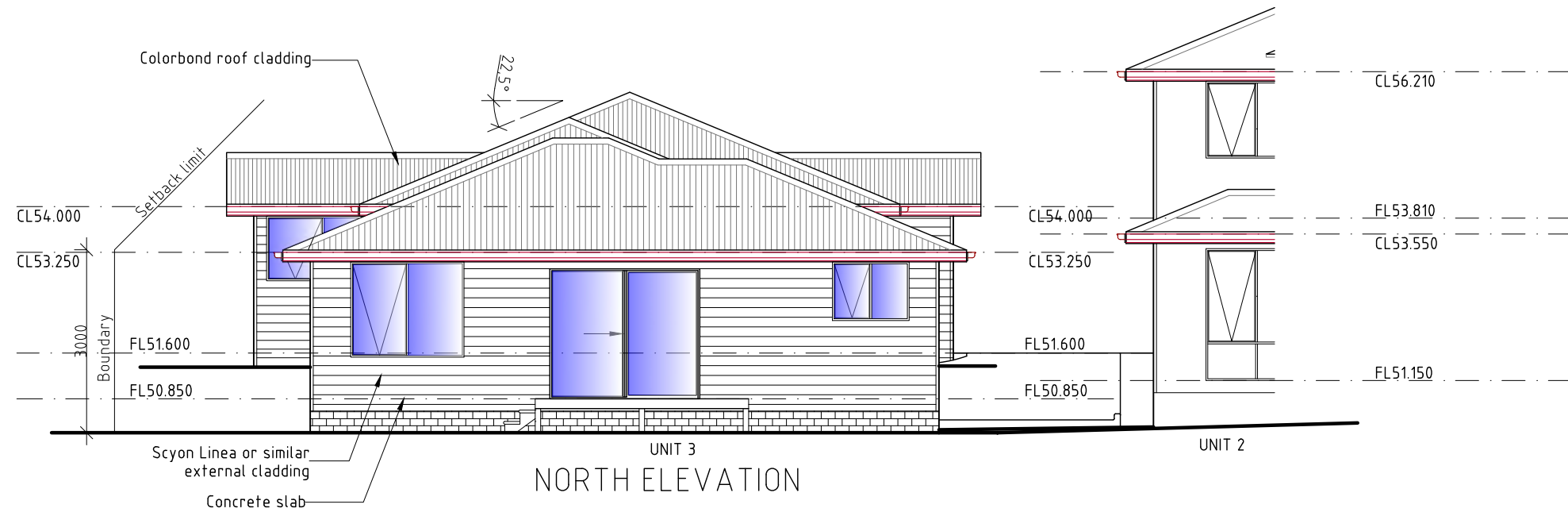
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UNIT 3 & 4

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 3 & 4 ELEVATIONS

DATE
29/08/23

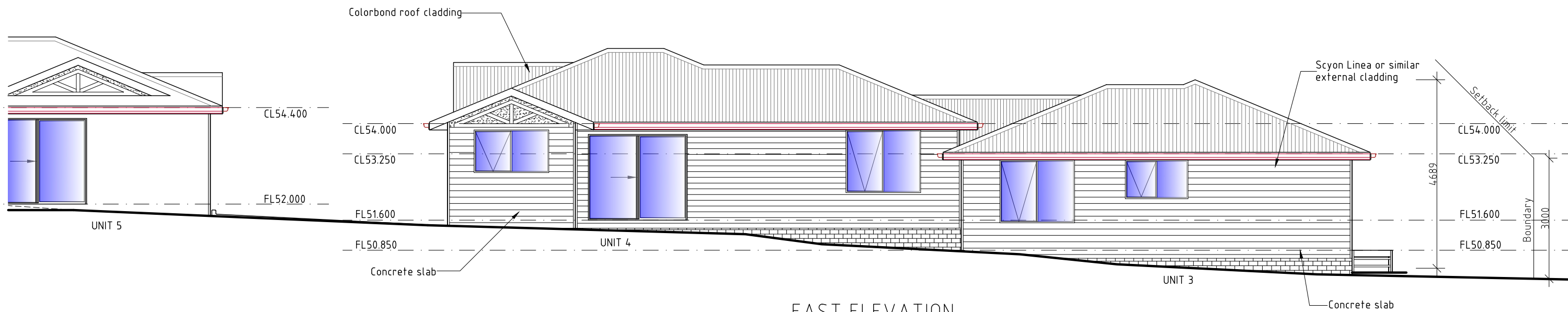
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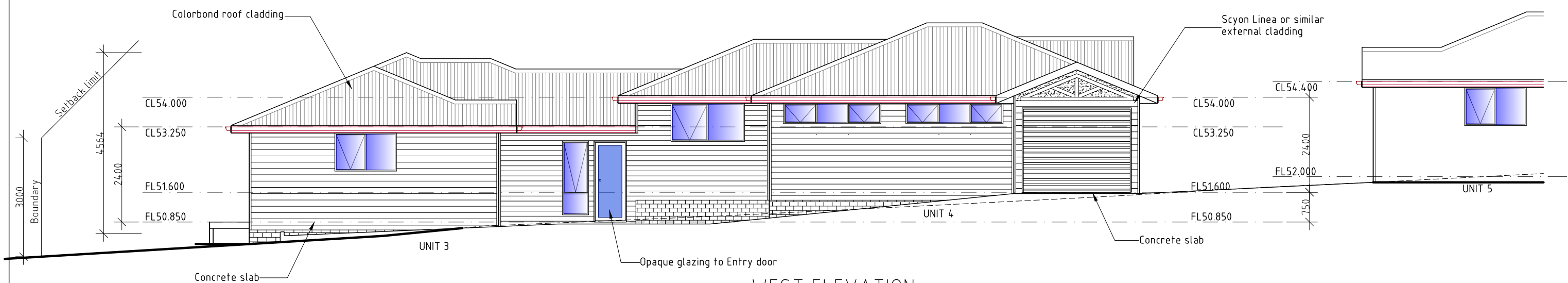
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6/8/2022



EAST ELEVATION



WEST ELEVATION

UNIT 3 & 4

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 3 & 4 ELEVATIONS

SCALE 1:100

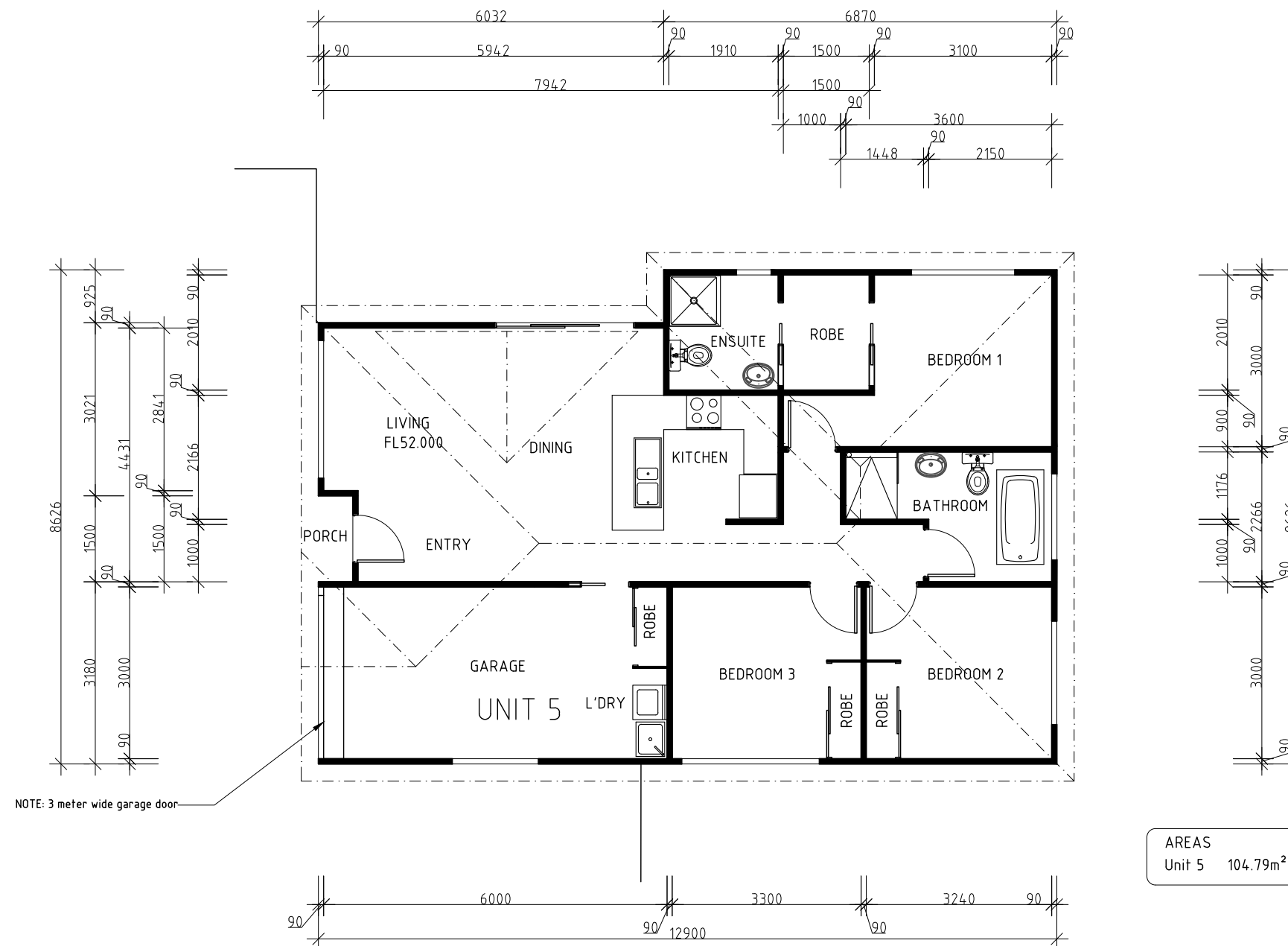
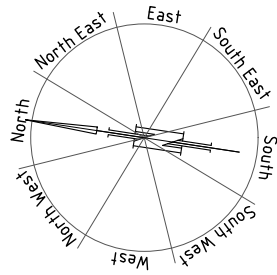
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29/08/23

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UNIT 5

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 5 PLAN

DATE
29/08/23

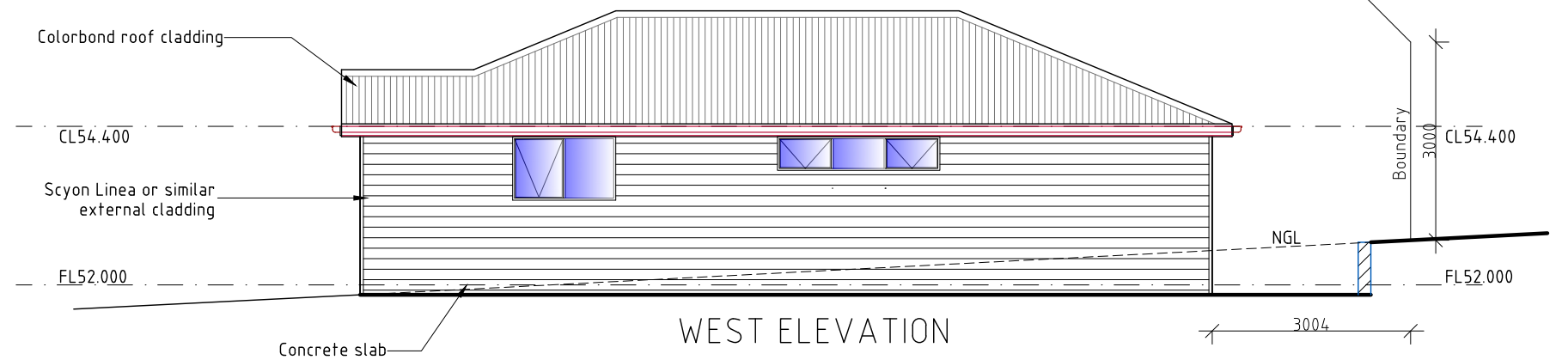
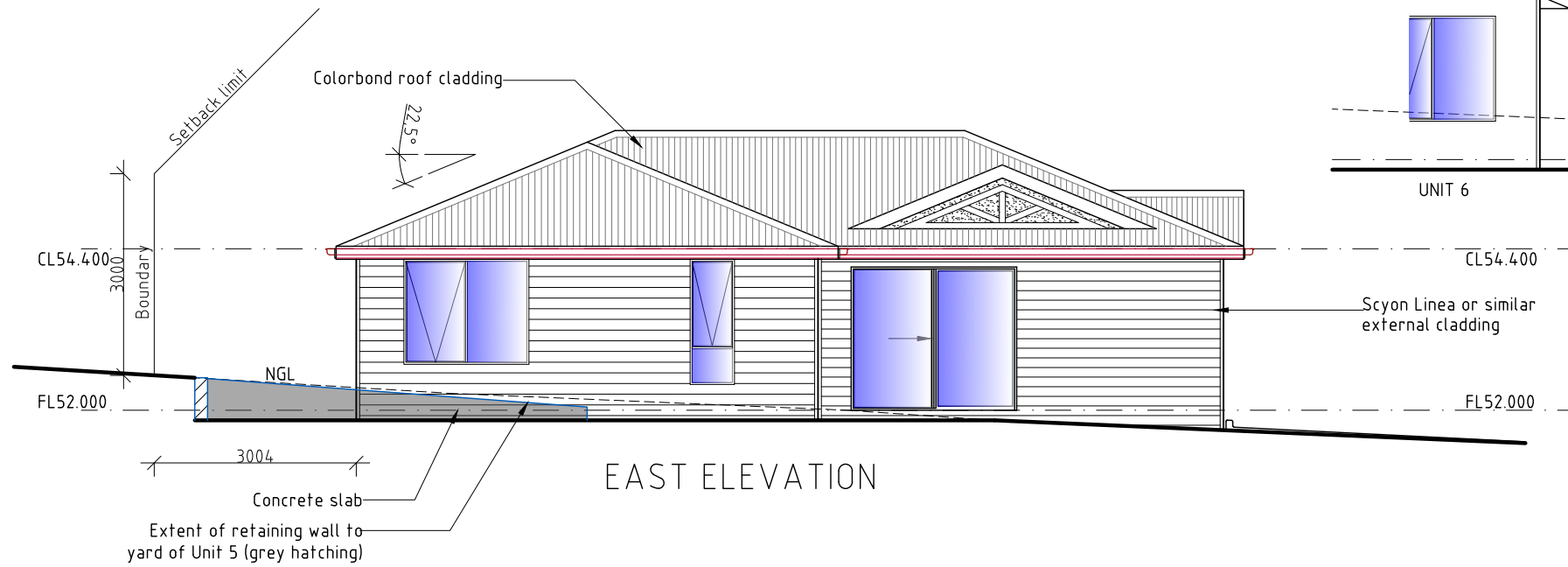
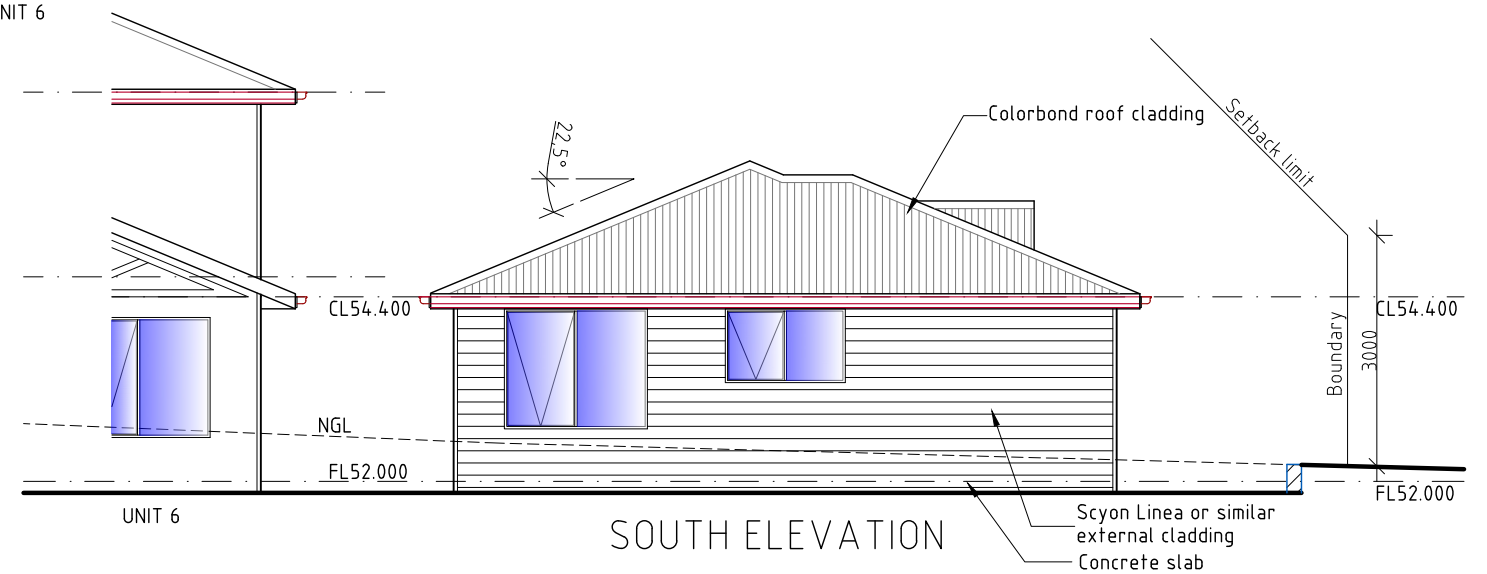
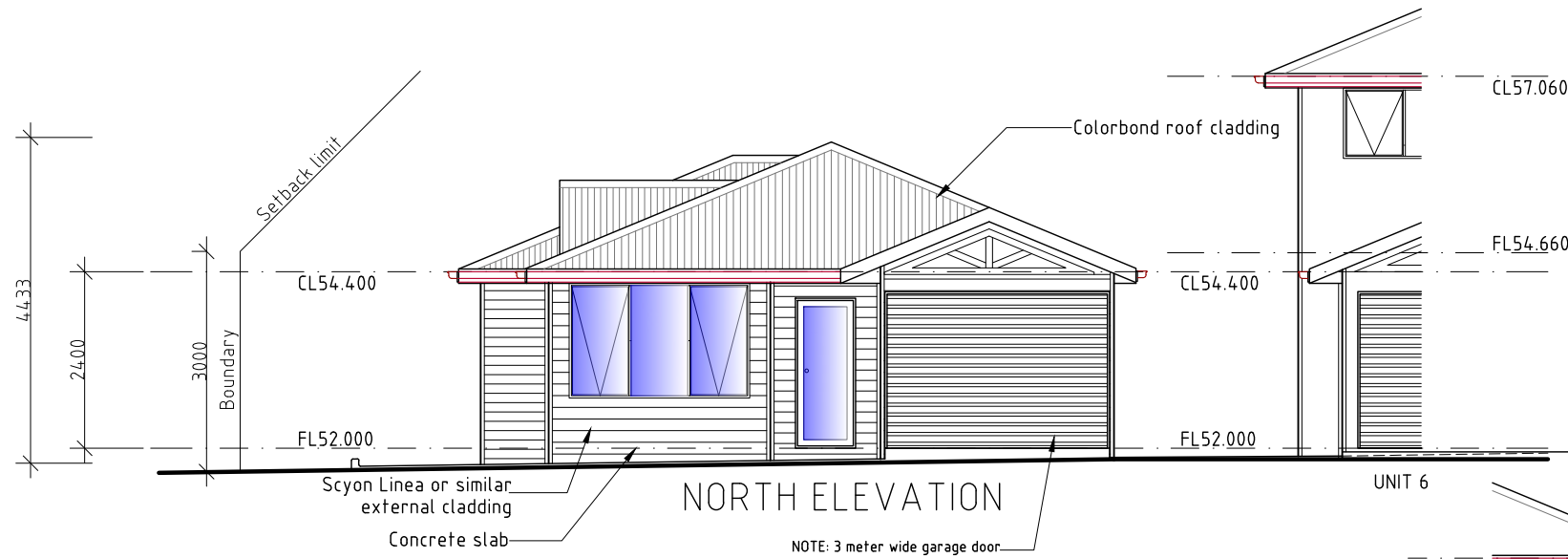
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UNIT 5

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 5 ELEVATIONS

SCALE 1:100

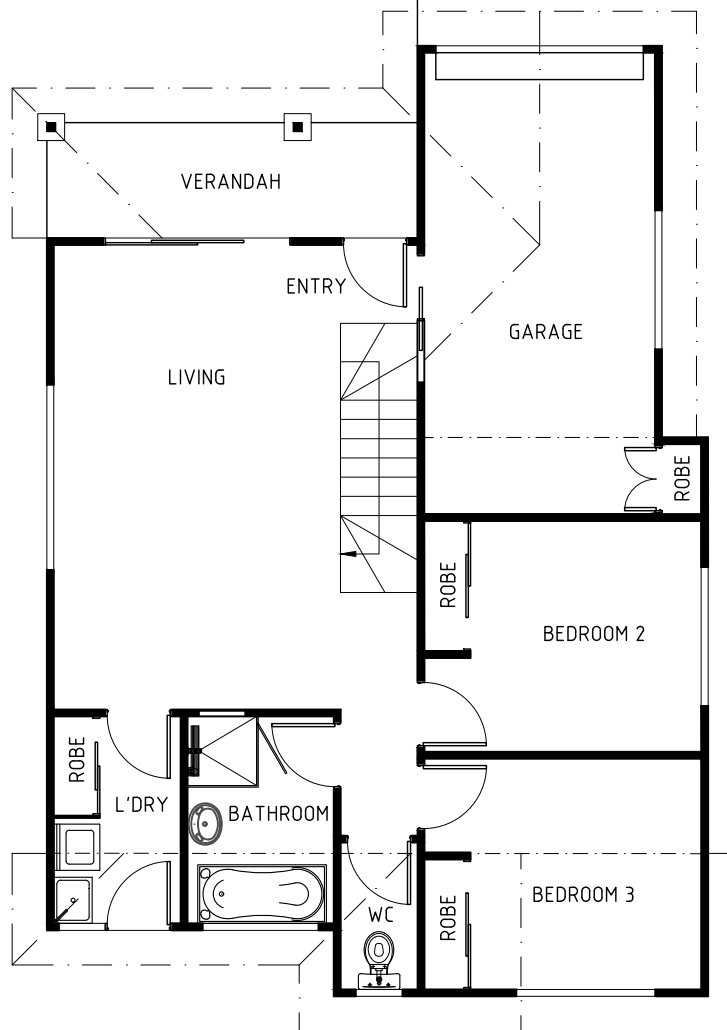
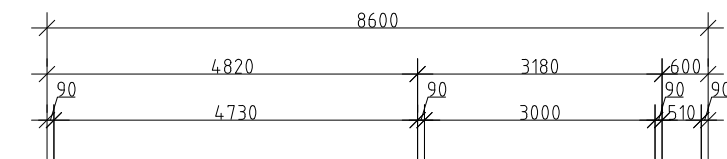
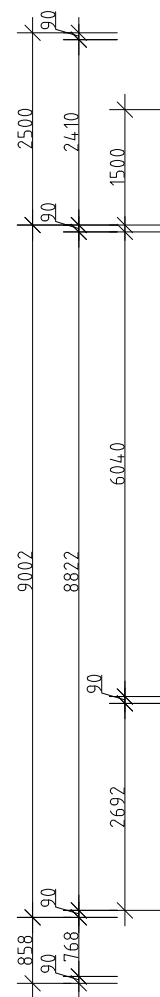
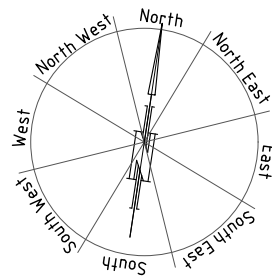
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29/08/2025

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29/08/23

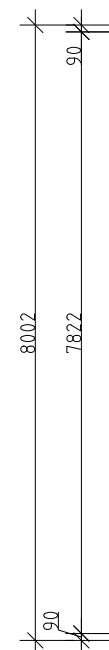
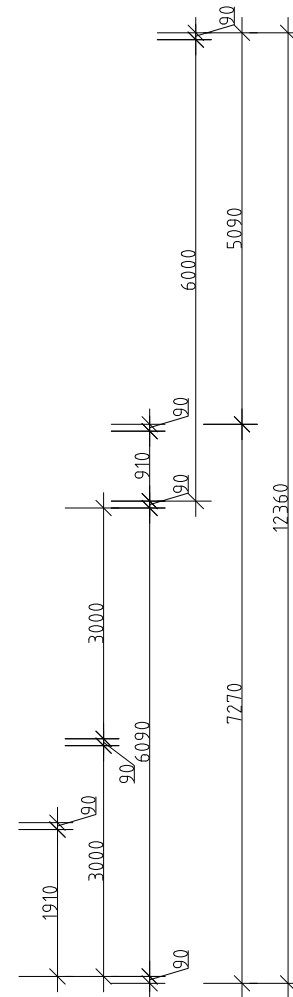
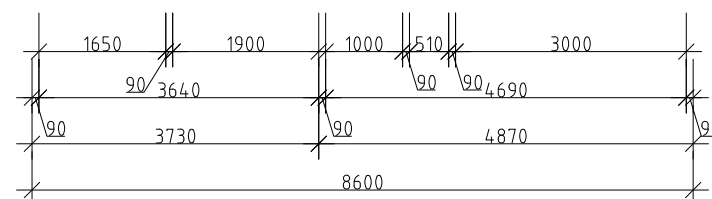
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160603

DRAWN BY G Tilley
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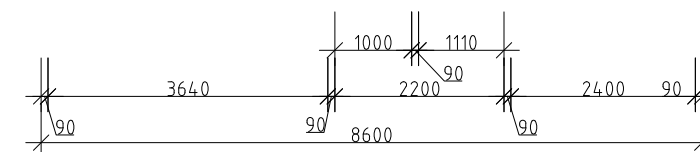
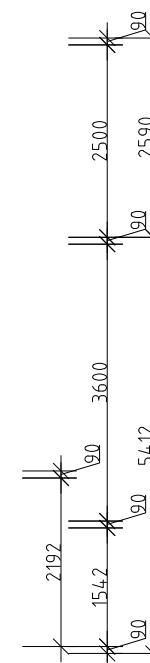
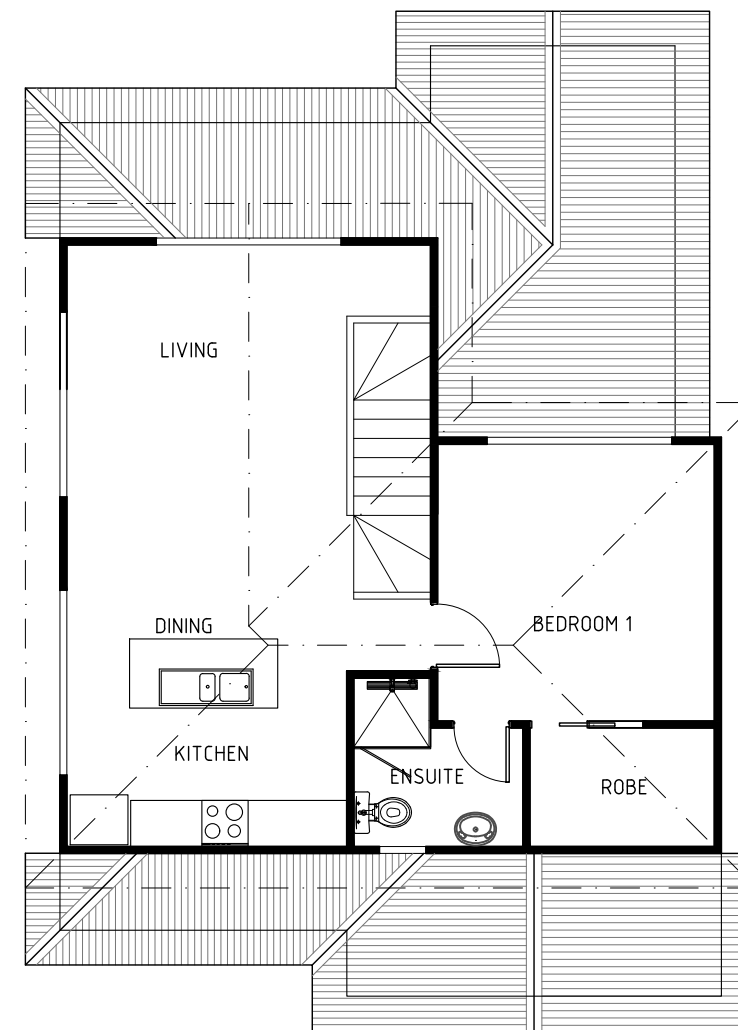
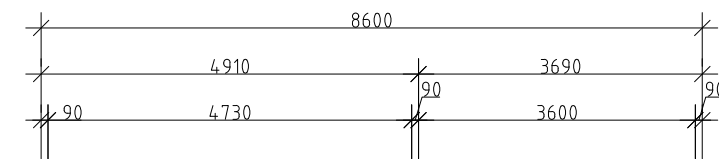
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LOWER LEVEL



AREAS Unit 6
Lower level 87.99m ²
Upper level 59.26m ²
Total 147.25m ²



UPPER LEVEL

UNIT 6

DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 6 PLAN

SCALE 1:100

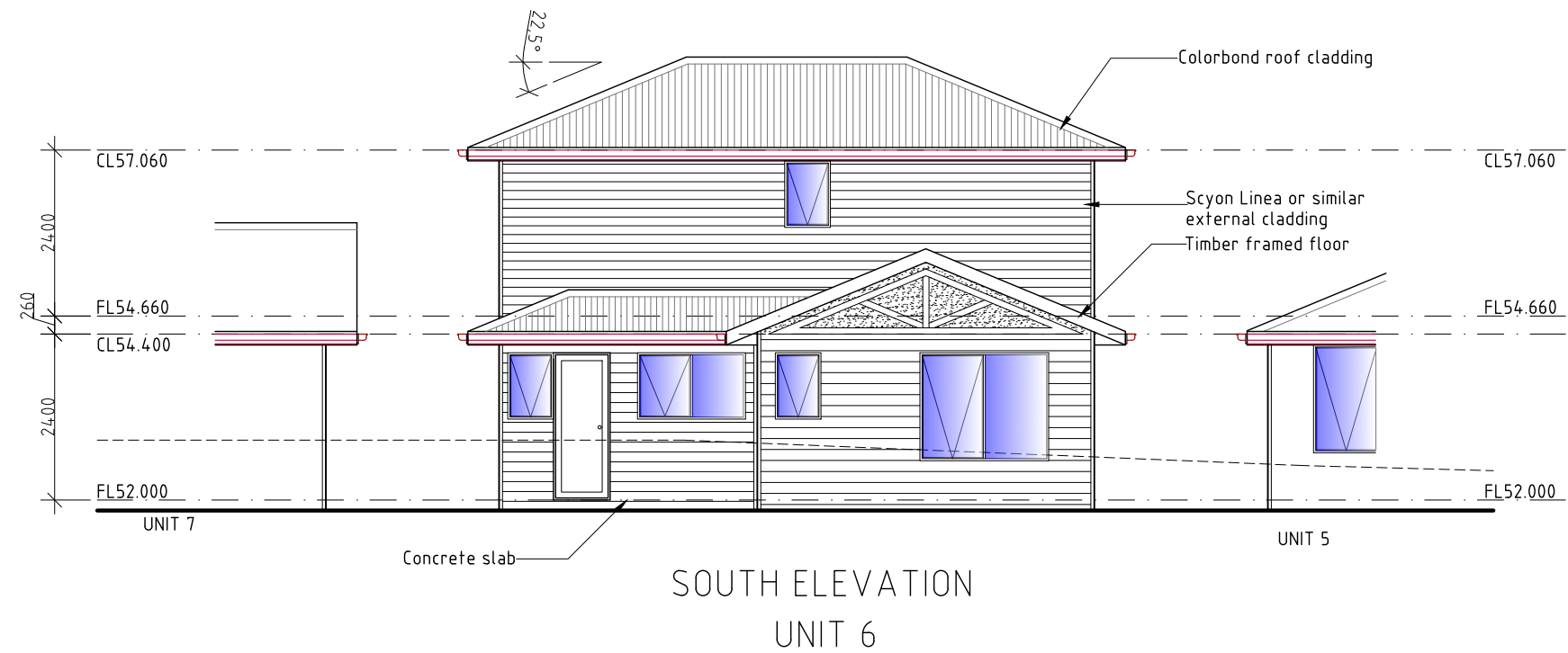
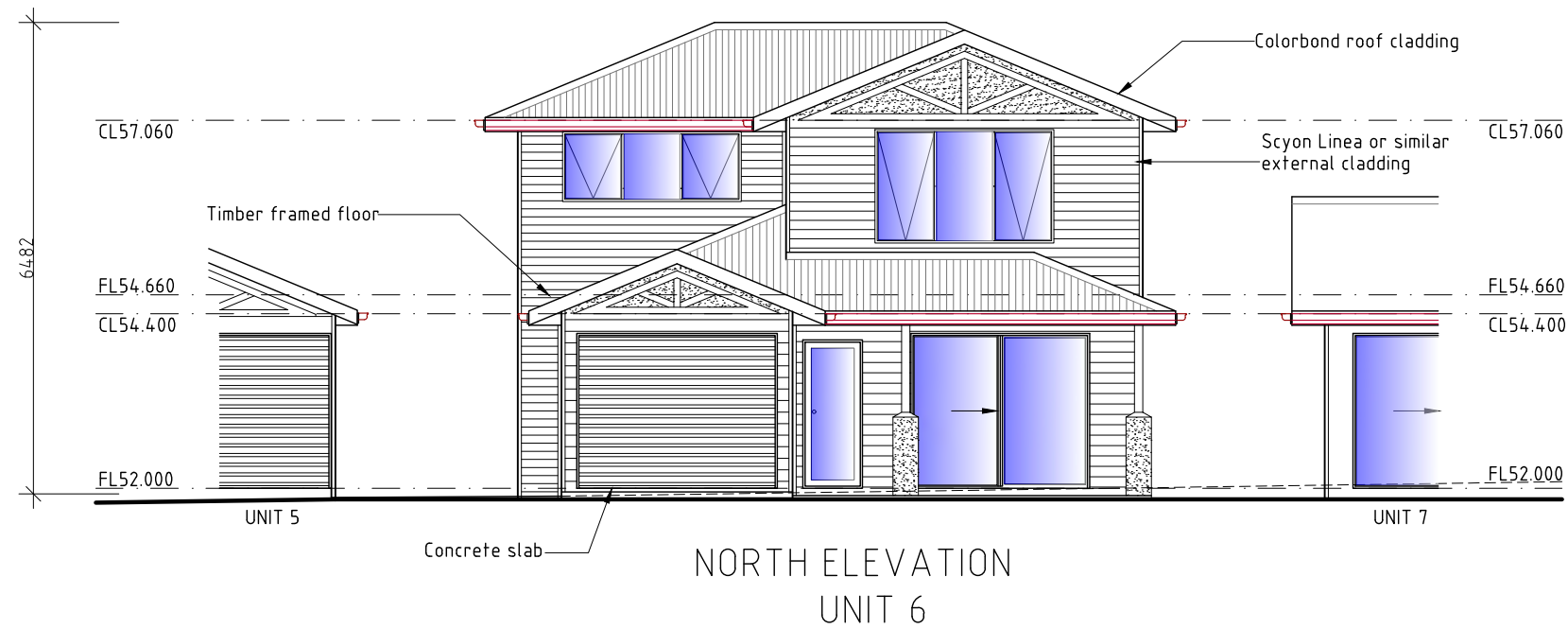
AMENDED

DATE
29/08/23

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Accreditation No. CC620H



DEVELOPMENT DRAWINGS ONLY
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UNIT 6

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 6 ELEVATIONS

SCALE 1:100

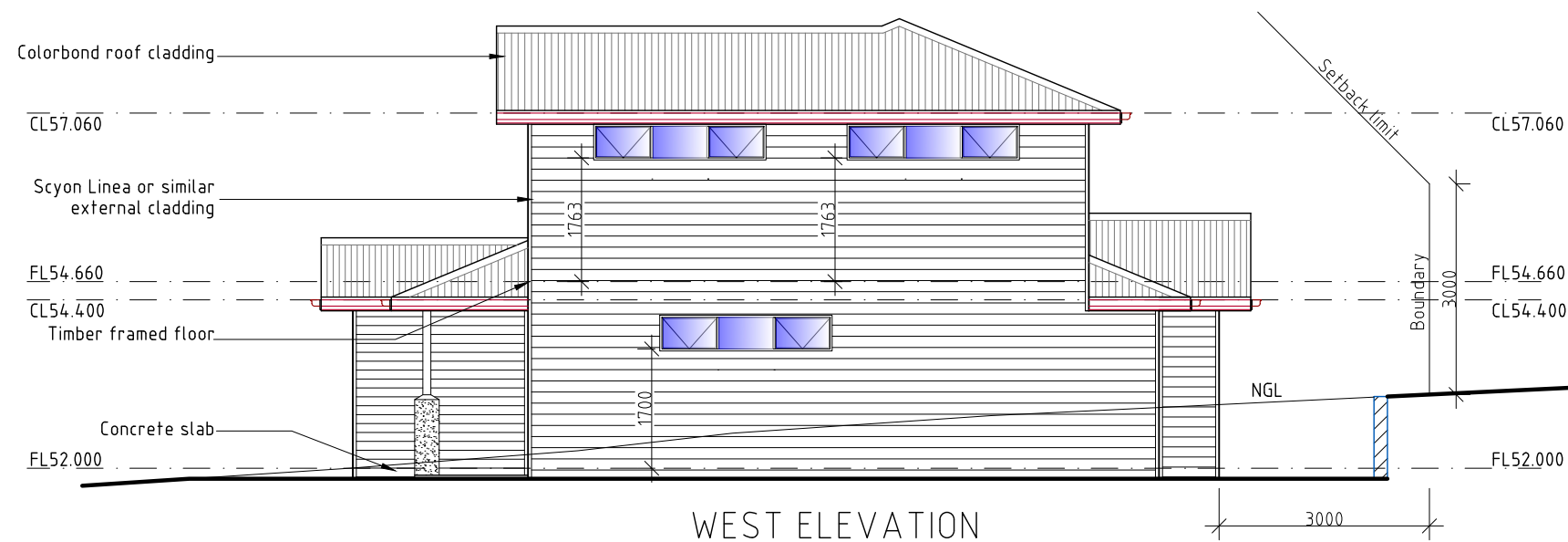
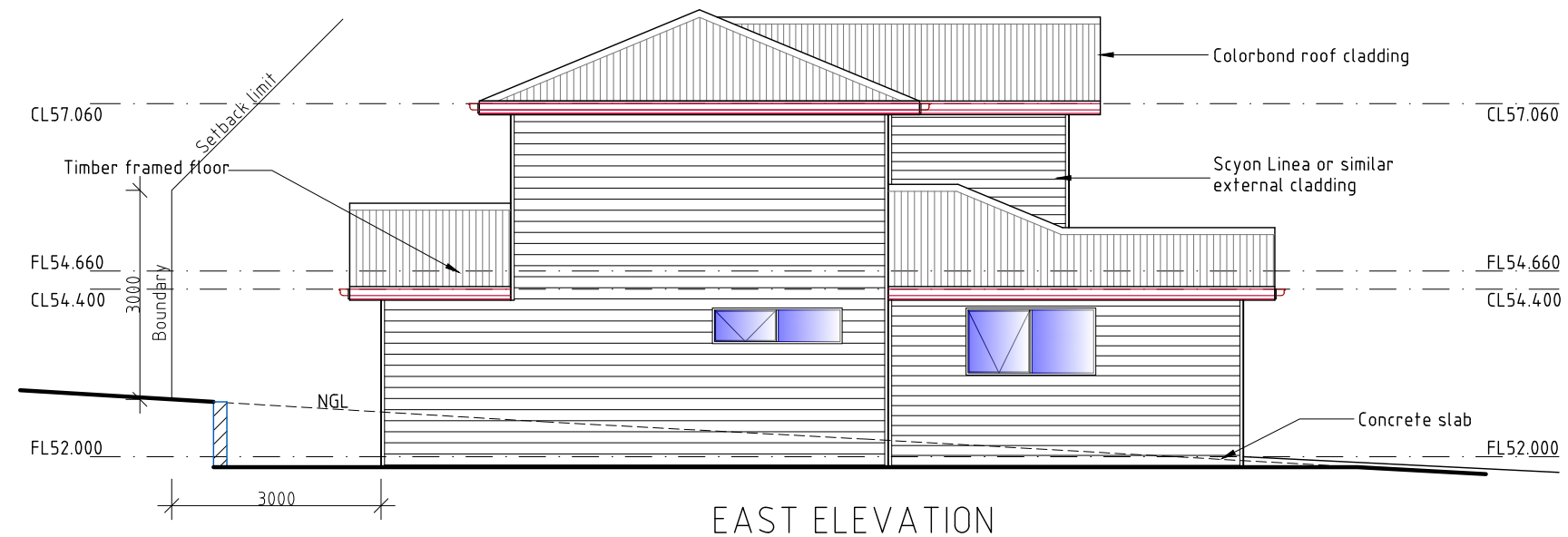
AMENDED

DATE
29/08/23

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20-06-23

DRAWN BY G Tilley
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phone ph 0400 671 582

Accreditation No. CC620H



UNIT 6

DEVELOPMENT DRAWINGS ONLY
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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 6 ELEVATIONS

DATE
29/08/23

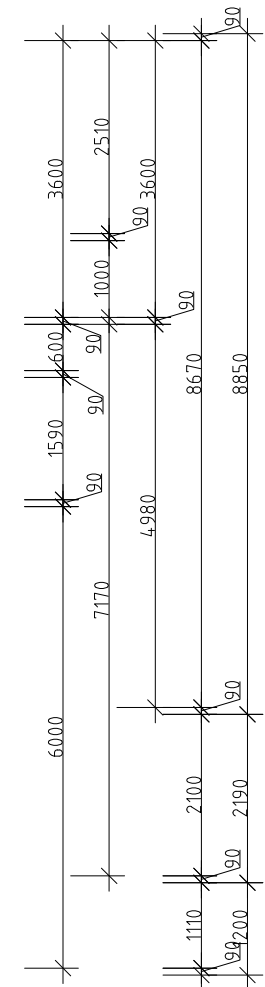
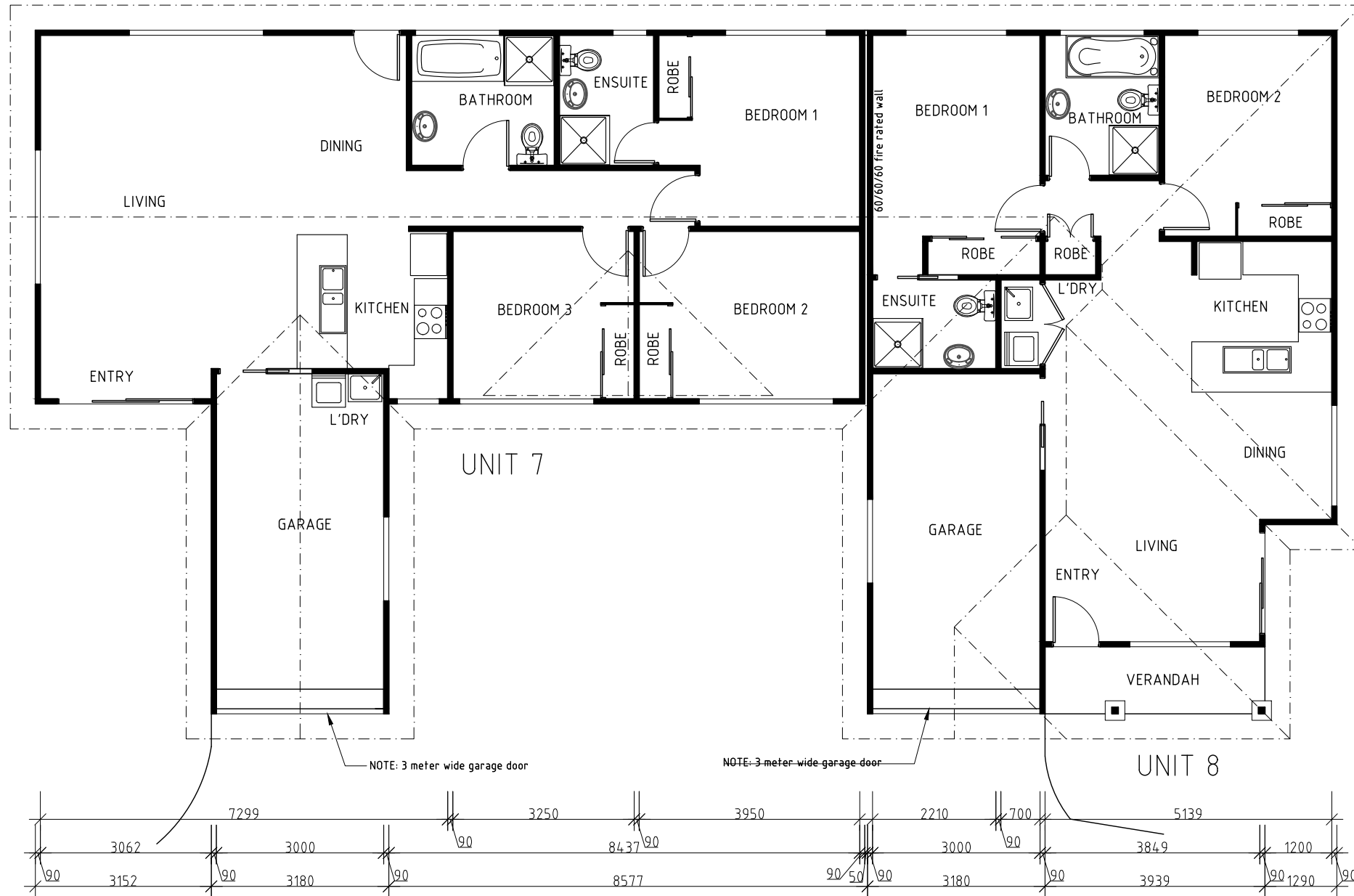
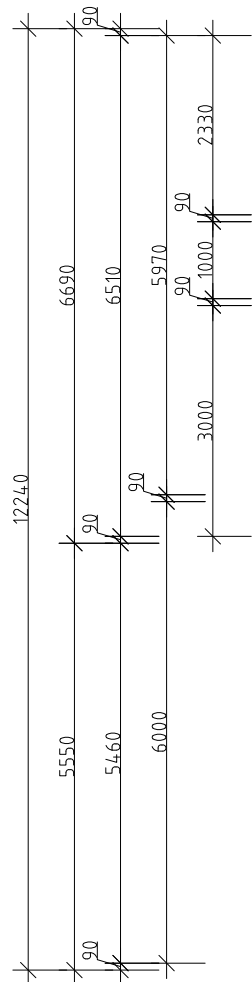
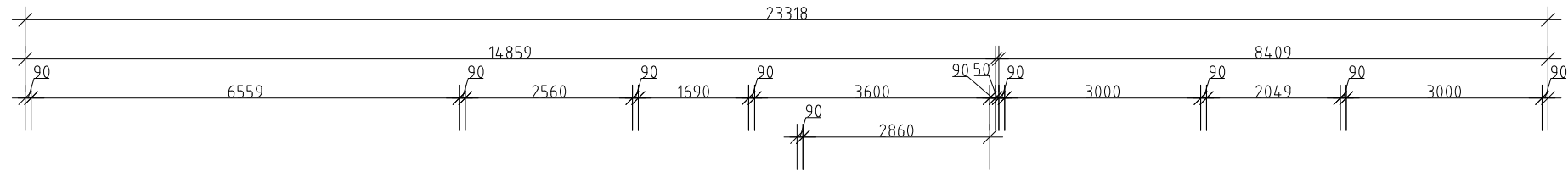
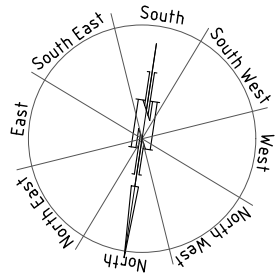
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email: gtilley7@biapond.com
phone ph 0400 671 582

SCALE 1:100

AMENDED

DRAWING NO.
210623

Accreditation No. CC620H
6/2022



AREAS	
Unit 7	117.06m ²
Unit 8	93.83m ²
Total	211.22m ²
Unit 8 Verandah	4.72m ²

DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION

UNIT 7 & 8

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 7 & 8 PLAN

SCALE 1:100

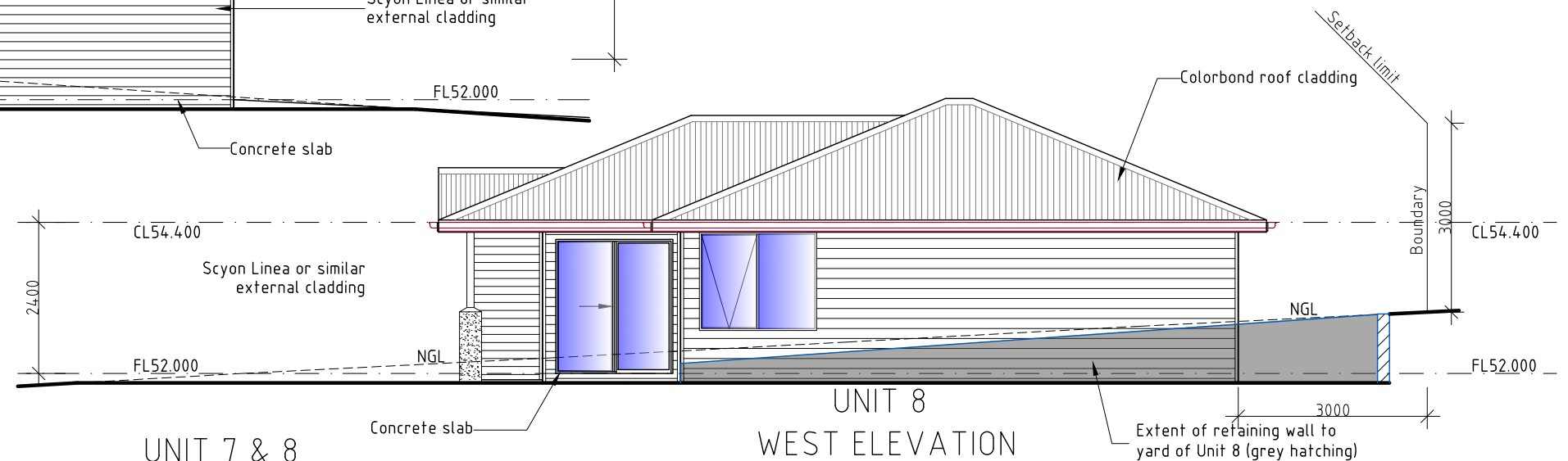
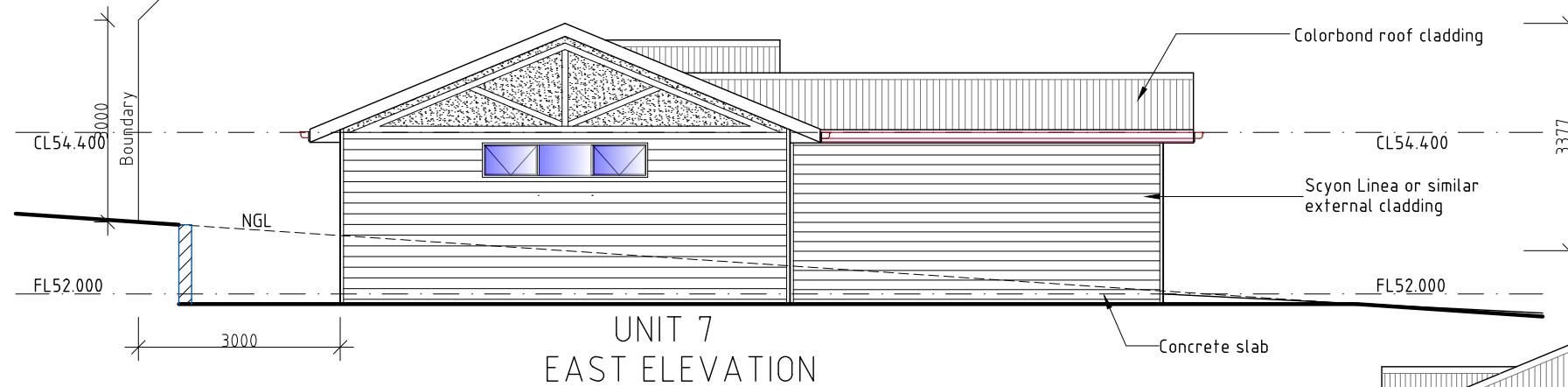
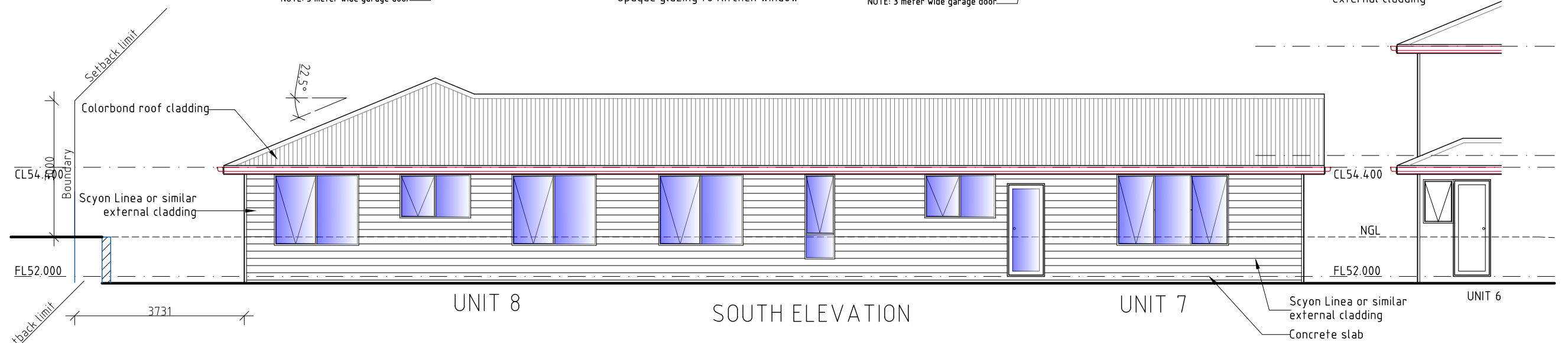
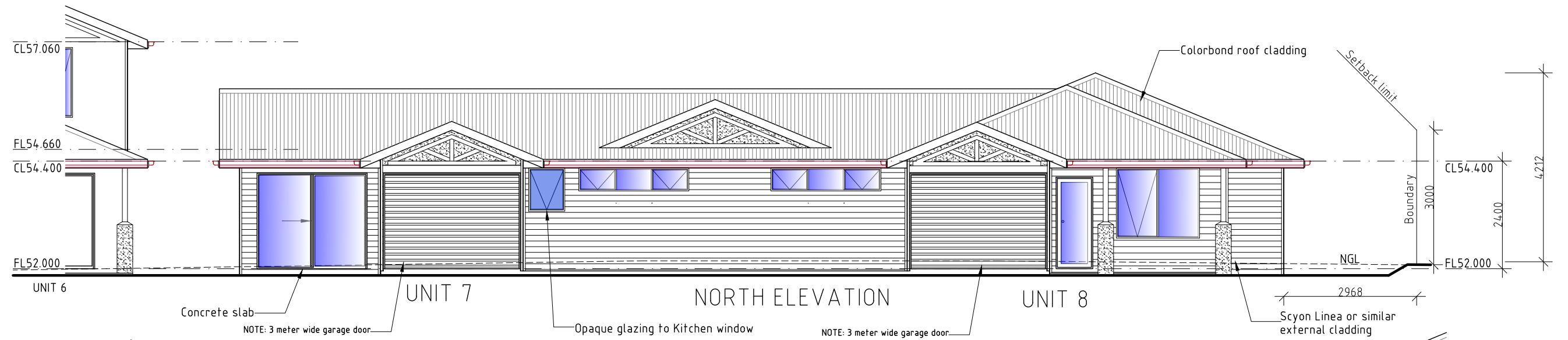
AMENDED

DATE
29/08/23

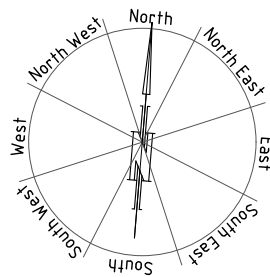
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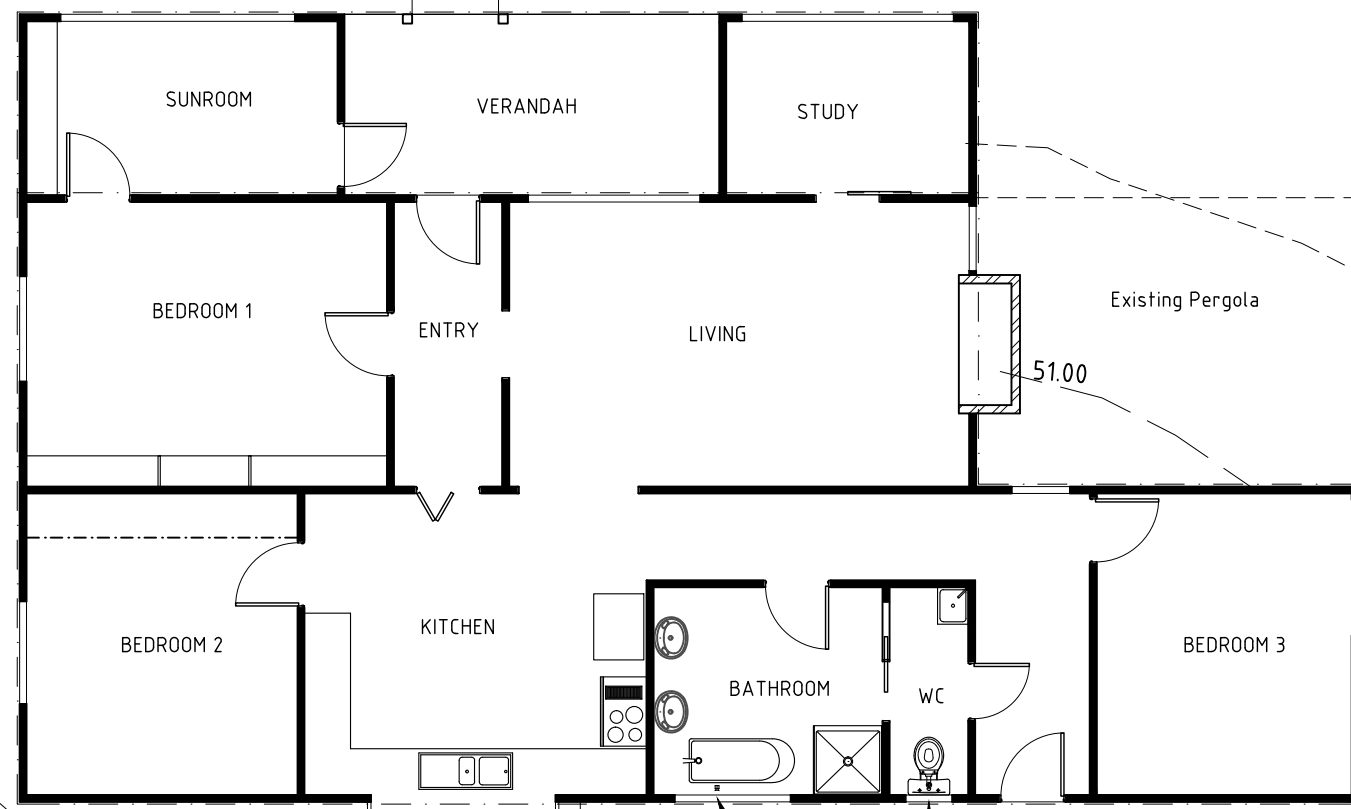


**DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION**



51.00

355°58'40"
524.00



Existing Pergola

51.00

UNIT 2

Existing Shed
(To be removed)

UNIT 1

Existing Shed
(To be removed)

Frosted glazing

Existing Carport
(To be removed)

UNIT 9 EXISTING

DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 9 PLAN

SCALE 1:100

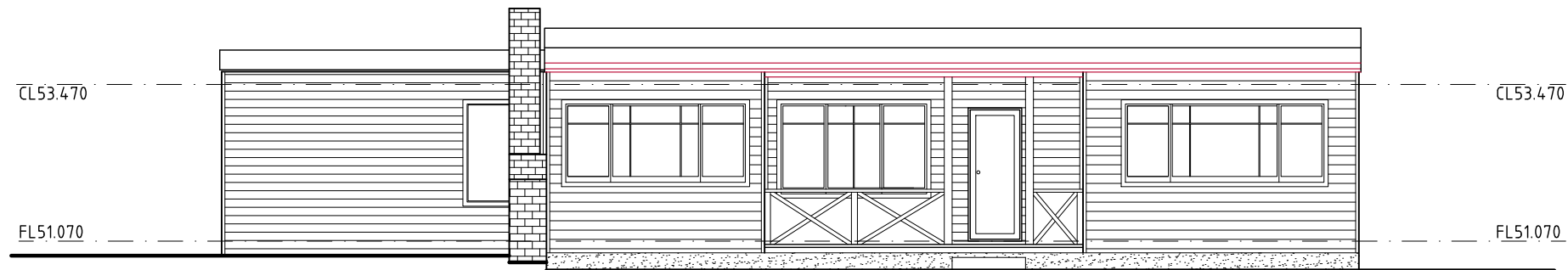
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DATE
29/08/23

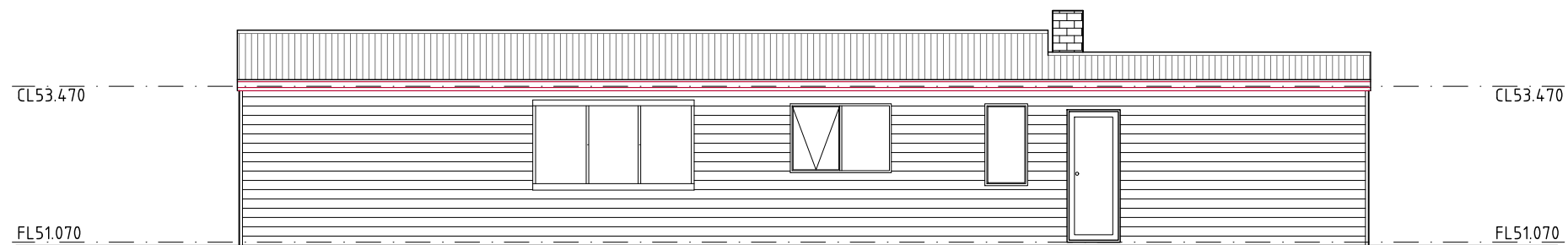
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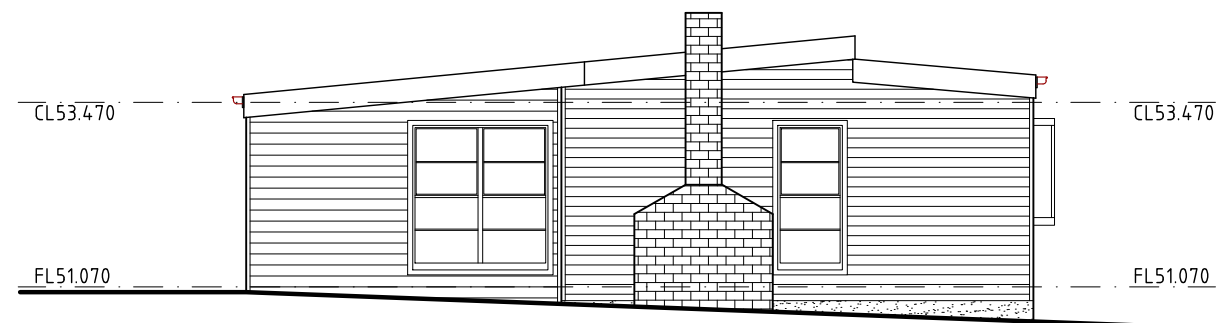
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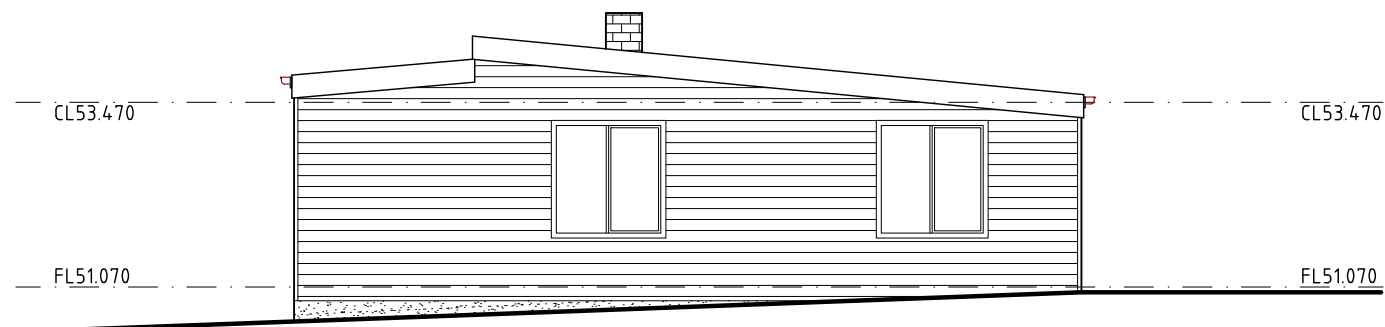
NORTH ELEVATION



SOUTH ELEVATION



EAST ELEVATION



WEST ELEVATION

UNIT 9 EXISTING

DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 9 ELEVATIONS

DATE
29/08/23

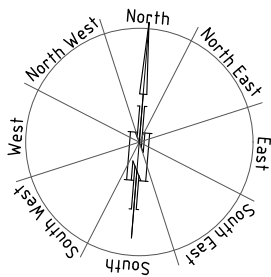
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phone ph 0400 671 582

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AMENDED

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20-06-23

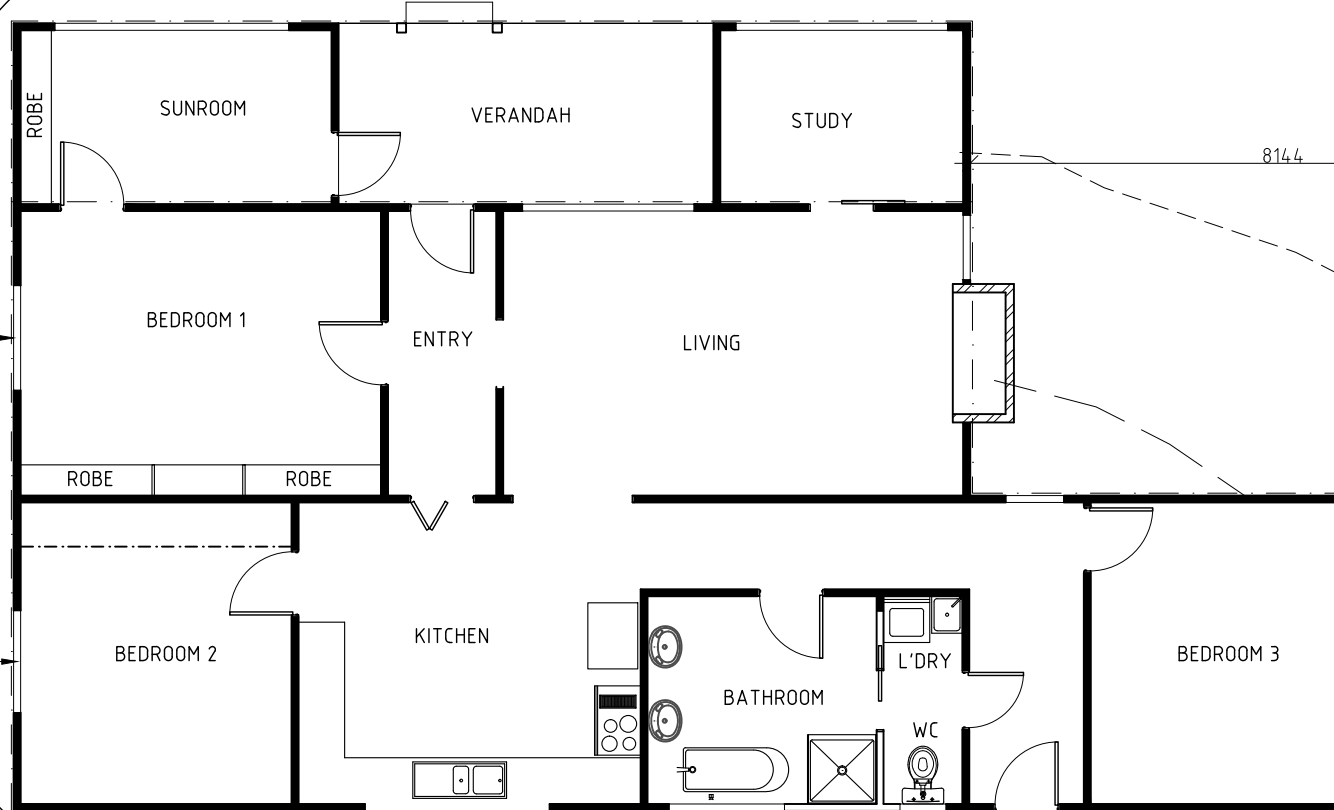
Accreditation No. CC620H
20-06-23



Opaque glazing to min 1700 above floor

Opaque glazing to min 1700 above floor

1000 wide footpath



814.4

1700 high timber slat screen with max 25% transparency

UNIT 2

UNIT 1

UNIT 9 PROPOSED

DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 9 PLAN PROPOSED

DATE
29/08/23

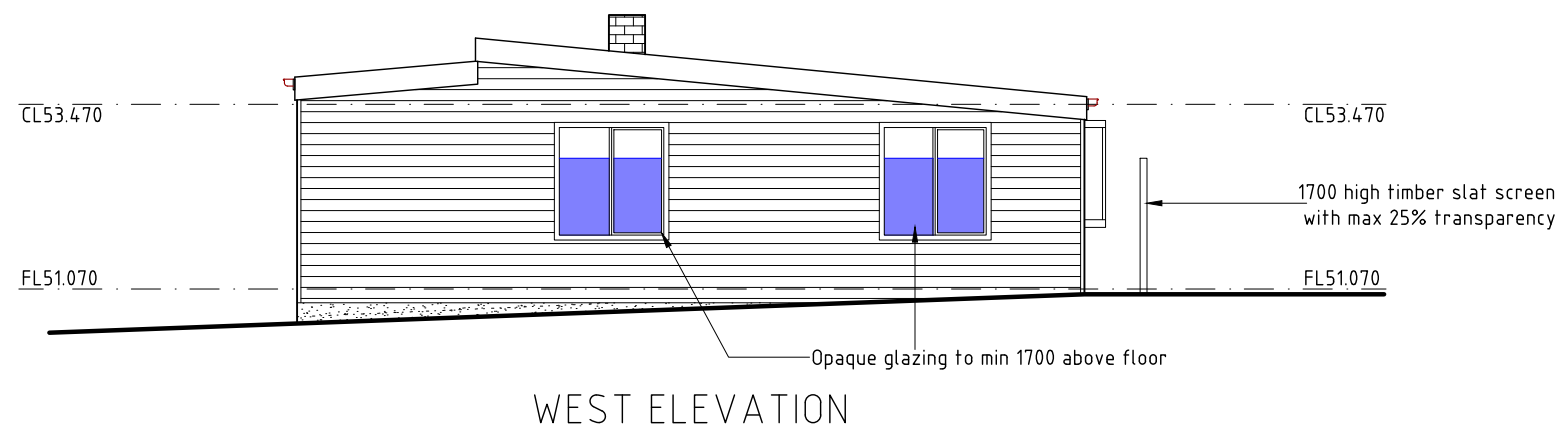
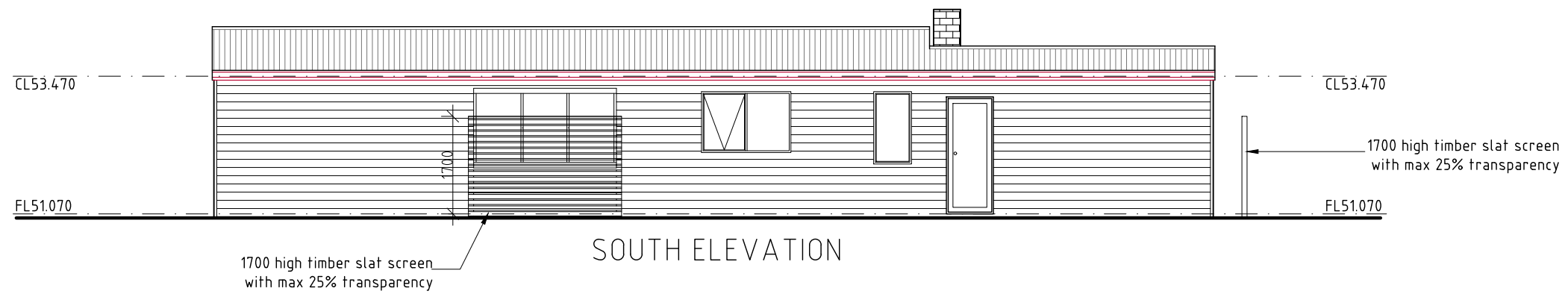
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SCALE 1:100

AMENDED

DRAWING NO.
24-08-23

Accreditation No.CC620H



UNIT 9 PROPOSED

DEVELOPMENT DRAWINGS ONLY
NOT FOR CONSTRUCTION

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR
SJM PROPERTY DEVELOPMENTS PTY LTD AT
981 CAMBRIDGE ROAD CAMBRIDGE

UNIT 9 ELEVATIONS PROPOSED

DATE
29/08/23

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phone ph 0400 671 582

SCALE 1:100

AMENDED

DRAWING NO.
24-08-23

Accreditation No. CC620H

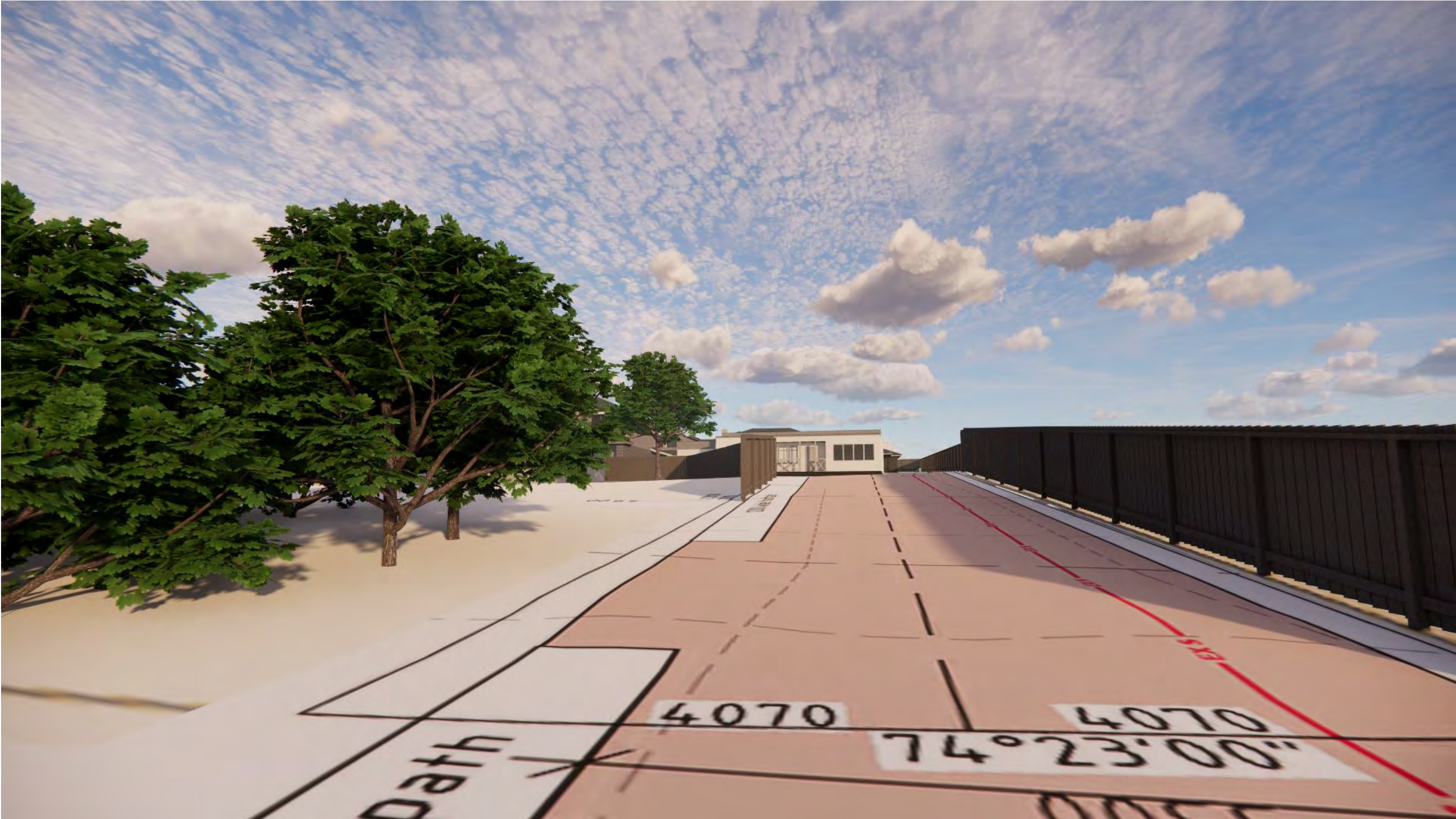
View from Habitable Zone of 1/47 Camrise Drive looking towards proposed unit 5 - 981 Cambride Rd



View from Habitable Zone of 45 Camrise Drive looking towards proposed unit 7 and 8 - 981 Cambride Rd



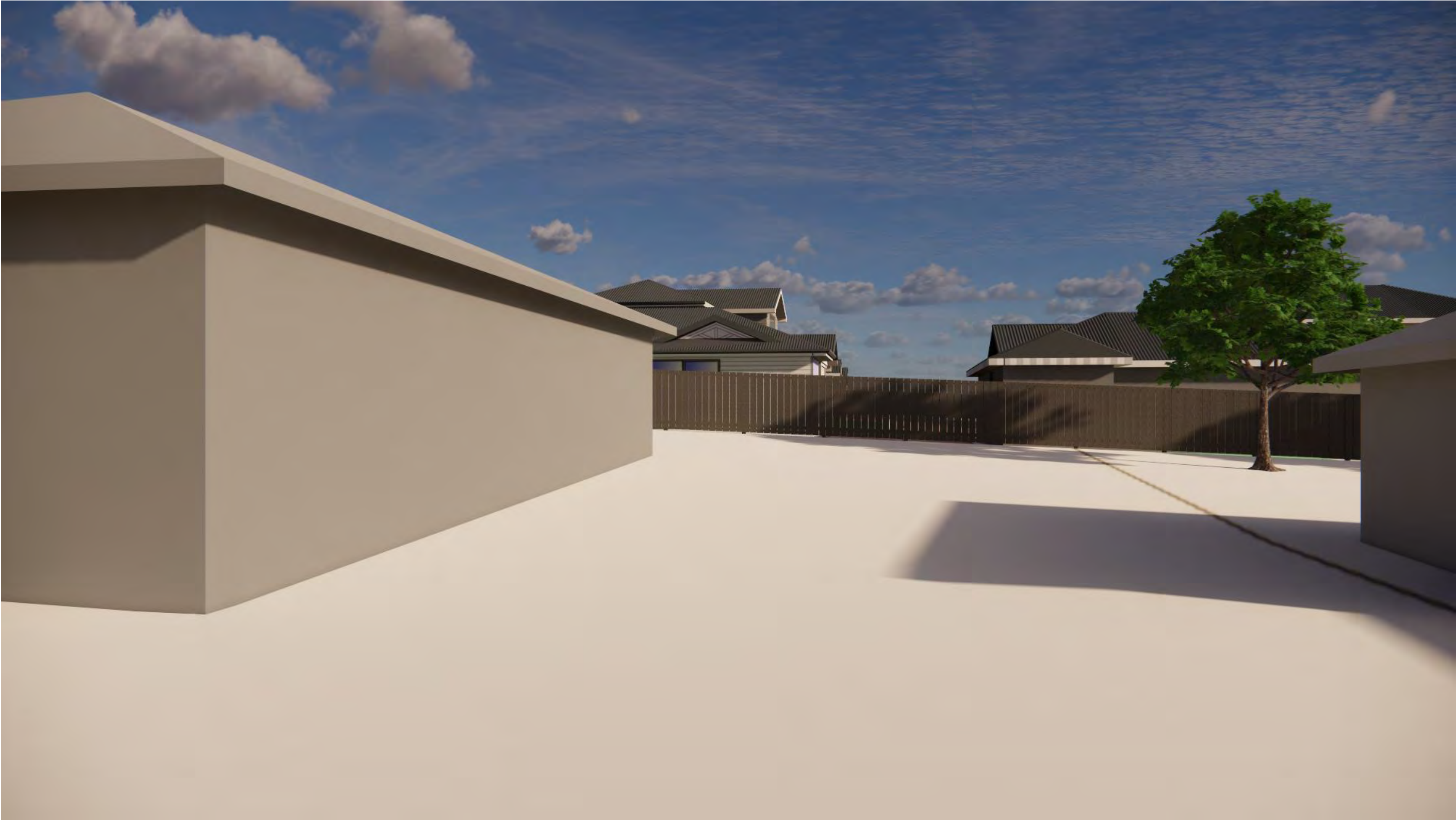
View from footpath of Cambridge Rd - Looking up driveway towards Unit 9 (Existing structure)



View from footpath of Cambridge Rd - Looking up driveway towards 993 Cambridge Road and proposed Unit2 -981 Cambridge Rd



View from garden of 49 Camrise Drive towrds proposed Unit3-4 and Unit 5 -981 Cambridge Rd



Attachment 3

Site Photos: 981 Cambridge Road, Cambridge



Photo 1: The site when viewed from Cambridge Road.



Photo 2: The neighbouring dwelling at 983 Cambridge Road when viewed from the reciprocal right of way.



Photo 3: The existing dwelling when viewed from the proposed internal driveway.



Photo 4: The view towards the rear boundary from within the lot proper.

7.2 DEVELOPMENT APPLICATION PDPLANPMTD-2023/037246 – 20 LOWELLY ROAD, LINDISFARNE - PARTIAL CHANGE OF USE TO FOOD SERVICES (WHOLESALE BAKERY)**EXECUTIVE SUMMARY****PURPOSE**

The purpose of this report is to consider the application made for a Partial Change of Use to Wholesale Bakery (Food Services) at 20 Lowelly Road, Lindisfarne.

RELATION TO PLANNING PROVISIONS

The land is zoned General Residential and subject to the Car Parking and Sustainable Transport Code, the Road and Railway Assets Code and the Safeguarding of Airports Code under the Tasmanian Planning Scheme - Clarence (the Scheme). In accordance with the Scheme the proposal is a Discretionary development.

LEGISLATIVE REQUIREMENTS

The report on this item details the basis and reasons for the recommendation. Any alternative decision by Council will require a full statement of reasons in order to maintain the integrity of the Planning approval process and to comply with the requirements of the Judicial Review Act and the Local Government (Meeting Procedures) Regulations 2015.

Council is required to exercise a discretion within the statutory 42-day period, which was extended via agreement with the applicant and now expires on 10 October 2023.

CONSULTATION

The proposal was advertised in accordance with statutory requirements and three representations were received raising the following issues:

- Traffic;
- Parking on Beach Road.

RECOMMENDATION:

A. That the Development Application for Partial Change of Use to Food Services (Wholesale Bakery) at 20 Lowelly Road, Lindisfarne (Cl Ref PDPLANPMTD-2023/037246) be approved subject to the following conditions and advice.

1. GEN AP1 – ENDORSED PLANS.
2. GEN AM5 – TRADING HOURS After “Monday – Friday” add “8.00am – 6.00pm.” After “Public Holidays” add “8.00am – 6.00pm.”
3. No signage is approved as part of the application.
4. The development must meet all required Conditions of Approval specified by TasWater notice dated 20 July 2023 (TWDA 2023/00911-CCC).

ADVICE

- a. This Permit will lapse after two years from the date on which it is granted unless the development/use has been substantially commenced. Upon request, under Section 53(5A) of the *Land Use Planning and Approvals Act 1993*, Council may grant an extension of time for a further two years. A further two years may be granted upon request under Section 53(5B) of the *Land Use Planning and Approvals Act 1993*. Any such requests must be made in writing and within six months of the day on which the permit has lapsed.
- b. This is a town planning permit only. Please be aware that a building permit and/or a plumbing certificate of likely compliance or plumbing permit may be required before the development can proceed. It is recommended that you contact Council's Building Department on (03) 6217 9580 to discuss the requirement for any additional permits or certification.
- c. Non-compliance with this permit is an offence under Section 63 of the *Land Use Planning and Approvals Act 1993* and may result in enforcement action under Division 4A of the *Land Use Planning and Approvals Act 1993*, which provides for substantial fines and daily penalties.
- d. A Plumbing permit will be required before any works are to commence.

All plumbing works must comply with the Tasmanian Plumbing Code and Australian Standard 3500.
- e. The site must be registered as a business in accordance with the *Food Act 2003*, prior to the commencement of the use. An application for a food business registration must be made at least two weeks prior to the intention to commence operating.

Please be aware that Council officers may require modifications to be made to the food preparation area in order to comply with the food safety standards.

- B. That the details and conclusions included in the Associated Report be recorded as the reasons for Council's decision in respect of this matter.

ASSOCIATED REPORT**1. BACKGROUND**

There is an existing guest house on-site. The guest house is listed on the Tasmanian Heritage Register.

2. STATUTORY IMPLICATIONS

- 2.1.** The land is zoned General Residential under the Scheme.
- 2.2.** The proposal is discretionary because it does not meet the Acceptable Solutions under the Scheme.
- 2.3.** The relevant parts of the Planning Scheme are:
- Section 5.6 – Compliance with Applicable Standards;
 - Section 6.10 – Determining Applications;
 - Clause 8 – General Residential Zone;
 - Clause C2.0 – Parking and Sustainable Transport Code;
 - Clause C3.0 – Road and Railway Assets Code; and
 - Clause C.16 – Safeguarding of Airports Code.
- 2.4.** Council’s assessment of this proposal should also consider the issues raised in any representations received, the outcomes of the State Policies and the objectives of Schedule 1 of the *Land Use Planning and Approvals Act, 1993* (LUPAA).

3. PROPOSAL IN DETAIL

3.1. The Site

The site is a large square corner block of 3531m². The site can be accessed from both Lowelly Road and Beach Road.

The site is fully serviced by TasWater potable water and reticulated sewerage, and council stormwater infrastructure.

The site is within an established residential area, with a small area of local business adjoining the property to the north-east. The site is proximate to the River Derwent, and a large public park around 200m to the south-west.

3.2. The Proposal

The proposal is for a partial change of use to a wholesale bakery. The applicant proposes to convert a utility room within the existing Guest House to a kitchen area to bake burger buns for supply to local cafes.

The bakery will operate between the hours of 8am and 6pm. Baked goods will be delivered to cafes in the owner's personal car within operating hours. On-site there are four car parking spaces which may be utilised by the bakery use; however, the bakery will not have any on-site customers.

4. PLANNING ASSESSMENT

4.1. Compliance with Applicable Standards [Section 5.6]

“5.6.1 A use or development must comply with each applicable standard in the State Planning Provisions and the Local Provisions Schedules.”

4.2. Determining Applications [Section 6.10]

“6.10.1 In determining an application for any permit for use or development the planning authority must, in addition to the matters required by section 51(2) of the Act, take into consideration:

- (a) all applicable standards and requirements in this planning scheme; and*
- (b) any representations received pursuant to and in conformity with section 57(5) of the Act, but in the case of the exercise of discretion, only insofar as each such matter is relevant to the particular discretion being exercised.”*

References to these principles are contained in the discussion below.

4.3. General Provisions

The Scheme contains a range of General Provisions relating to specific circumstances not controlled through the application of Zone, Code or Specific Area Plan provisions.

There are no General Provisions relevant to the assessment of this proposal.

Compliance with Zone and Codes

The proposal does not require an assessment against the Safeguarding of Airports Code as the proposed development height is below the Obstacle Surface Layer (OSL) height of 147m, and it is located outside the airport noise attenuation area.

The proposal meets the Scheme’s relevant Acceptable Solutions of the General Residential Zone, the Car Parking and Sustainable Transport Code and the Road and Railways Assets Code, with the exception of the following.

General Residential Zone

- **Clause 8.3.1 Discretionary uses P4** – the proposed use is discretionary and there is no acceptable solution.

The application must be assessed against Performance Criteria P4 of 8.3.1 *Discretionary uses* as follows.

Clause	Performance Criteria	Assessment
8.3.1 P4	<i>“A use listed as Discretionary must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to:</i>	The proposed use for a wholesale bakery is not considered to cause an unreasonable loss of amenity to adjacent sensitive uses.
	<i>(a) the intensity and scale of the use;</i>	The wholesale bakery is to occupy a small 25m ² area within the existing large building. The applicant is proposing to make hamburger buns to be sold to a few local cafes. There will not be a shop front associated with the bakery, meaning there will be no customers entering the site to purchase baked goods. The business proposes to operate within the hours of 8am and 6pm, which are considered acceptable under the use standards of the General Residential Zone.
	<i>(b) the emissions generated by the use;</i>	There is unlikely to be any emissions generated by the modest use.

	(c) <i>the type and intensity of traffic generated by the use;</i>	No large or heavy vehicles will be used for the operation of the use. Baked goods will be delivered to surrounding cafes by the owner in their personal vehicle during operating hours. The owner is expecting to deliver goods once daily. Any increase in traffic is considered to be reasonable within the context of the area.
	(d) <i>the impact on the character of the area; and</i>	Given the use is of a modest scale and intensity and occurs mainly within the confines of an existing building, the impact on the character of the area is not considered to be unreasonable.
	(e) <i>the need for the use in that location.”</i>	The surrounding area is mainly residential with a small number of local businesses. The use is considered to be compatible within the residential context and is expected to contribute to the boutique vibe of the local business.

5. REPRESENTATION ISSUES

The proposal was advertised in accordance with statutory requirements and three representations were received. The following issues were raised by the representors.

5.1. Traffic

Representors are concerned with increase vehicle movements associated with the use and the adverse impacts this may have on the surrounding road networks.

- **Comment**

The proposal is considered to meet the acceptable solution CA1.4 of C3.5.1 *Traffic generation at a vehicle crossing, level crossing or new junction*. No customers will be accessing the site and the business owner is proposing to deliver a small amount of baked goods to local cafes between the hours of 8am and 6pm. The traffic generated from the use is not expected to generate any adverse impacts on the road network.

It appears that many of the representations relate to the existing road safety and traffic situation along Beach Street from the intersection at East Derwent Highway. While many of the concerns are valid, they do not appear to directly relate to the proposal.

5.2. Parking on Beach Street

Representors have raised concerns that the proposal may increase on street parking demand.

- **Comment**

The proposed wholesale bakery has four on-site car parking spaces; however, the parking spaces are not likely to be needed given the owner of the business is proposing to deliver their baked goods off-site within their personal car. The bakery is not a shop front, and the site will not be attracting customers. As such, no additional demand on street parking is expected.

6. EXTERNAL REFERRALS

The proposal was referred to TasWater, who have provided a number of conditions to be included on the planning permit if granted.

The application was referred to the Tasmanian Heritage Council, who provided a certificate of exemption.

7. STATE POLICIES AND ACT OBJECTIVES

7.1. The proposal is consistent with the outcomes of the State Policies, including those of the State Coastal Policy.

7.2. The proposal is consistent with the objectives of Schedule 1 of LUPAA.

8. COUNCIL STRATEGIC PLAN/POLICY IMPLICATIONS

There are no inconsistencies with Council's adopted Strategic Plan or any other relevant Council Policy.

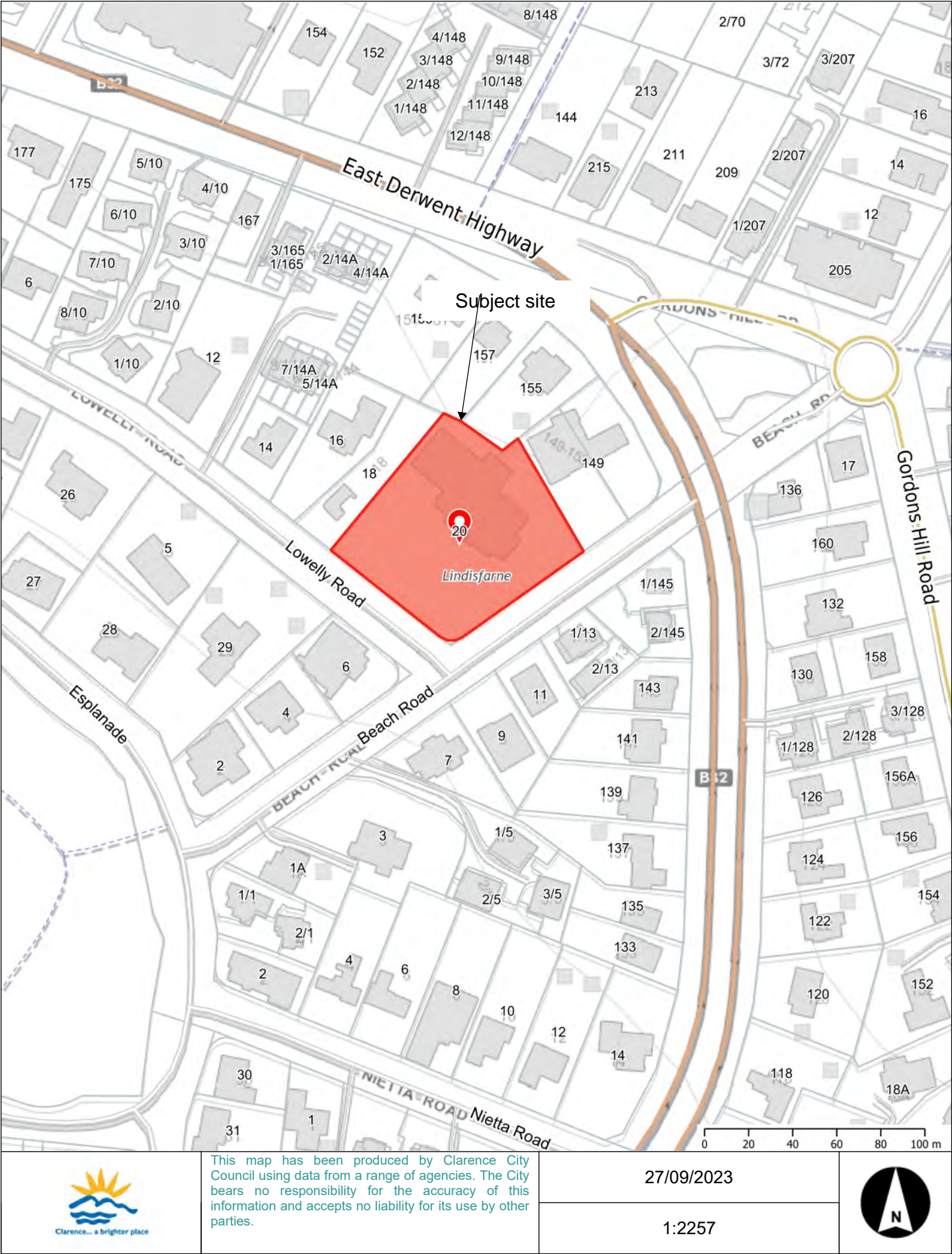
9. CONCLUSION

The proposal is considered to meet the relevant standards of the scheme and is therefore recommended for approval, subject to conditions.

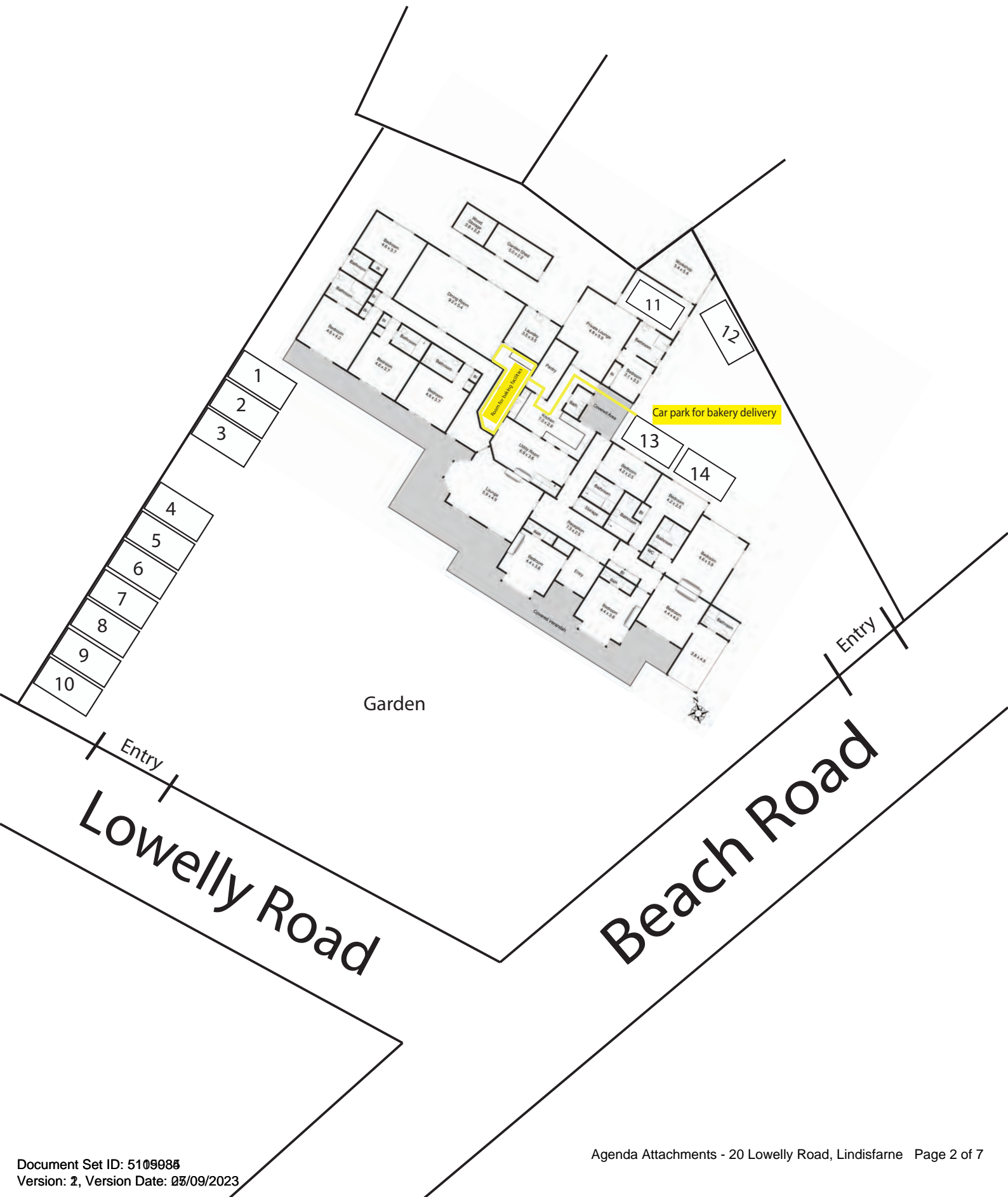
Attachments: 1. Location Plan (1)
2. Proposal Plan (4)
3. Heritage Exemption (1)
4. Site Photo (1)

Robyn Olsen
ACTING HEAD OF CITY PLANNING

Attachment 1



Attachment 2



Orana House
20 Lowelly Road, Lindisfarne
TAS 7015



Total Approx. Floor Area : 610 sqm
Total Approx. Outbuilding Area : 59 sqm

All measurements are internal and approximate.
This plan is a sketch for illustration, not valuation.
Produced by Open2view.com



Utility Room 3.5 X 7.1
Kitchen 7.0 X 2.8

Currently we use this utility room as a guest laundry room. There is a washing machine and a dryer. It is our intention to move the washing machine and dryer to our main laundry room (Red coloured zone). We will then provide laundry service for our guests instead of self service laundry.



TAS 7015





Our plan is to install the oven, dough divider, dough proofing cabinet, bakery trolley here. We also have a large pantry which can accommodate the additional storage of flour, sugar and other ingredients (Blue coloured zone)



Tasmanian Heritage Council
GPO Box 618 Hobart Tasmania 7000
134 Macquarie St, Hobart Tasmania 7000
Tel: 1300 850 332
enquiries@heritage.tas.gov.au
www.heritage.tas.gov.au

PLANNING REF: n/a
EXEMPTION NO: 5439
REGISTERED PLACE NO 1054
FILE NO: 06-98-99THC
APPLICANT: Pauline Heng
DATE: 8-Jun-2023

CERTIFICATE OF EXEMPTION

(Historic Cultural Heritage Act 1995)

The Place: Orana

20 LOWELLY RD LINDISFARNE 7015 TAS

Thank you for your application for a Certificate of Exemption for works to the above place. Your application has been approved by the Heritage Council under section 42(3) (a) of the *Historic Cultural Heritage Act 1995* for the following works:

Works: Change Utility Room to a small bakery for wholesale bread. Works include replacing current oven with a bigger oven, placing a proofing rack and a 50 kg flour mixer in the room. No internal structural changes.

Documents: 1. Application form
2. Plans

Comments: The proposed works are consistent with that which the Tasmanian Heritage Council's Works Guidelines describes as being eligible for a certificate of exemption.

A copy of this certificate will be forwarded to the local planning authority for their information. Please note, this certificate of exemption is an approval under the *Historic Cultural Heritage Act 1995* only. This certificate is not an approval under any other Act. Further approvals such as planning, building or plumbing may be required. For information regarding these or any other approval, contact your local Council.

Attachment 4



Photo 1: North-eastern elevation of 20 Lowelly Road



Photo 2: North-western elevation of 20 Lowelly Road, taken from Beach Road

7.3 DEVELOPMENT APPLICATION PDPLANPMTD-2023/036864 – 249 CARELLA STREET, HOWRAH - ADDITIONS AND ALTERATIONS (SINGLE DWELLING)**EXECUTIVE SUMMARY****PURPOSE**

The purpose of this report is to consider the application made for Additions and Alterations (Single Dwelling) at 249 Carella Street, Howrah.

RELATION TO PLANNING PROVISIONS

The land is zoned General Residential and subject to the Car Parking and Sustainable Transport Code, the Flood-Prone Areas Hazard Code and the Safeguarding of Airports Code under the Tasmanian Planning Scheme - Clarence (the Scheme). In accordance with the Scheme the proposal is a Discretionary development.

LEGISLATIVE REQUIREMENTS

The report on this item details the basis and reasons for the recommendation. Any alternative decision by Council will require a full statement of reasons in order to maintain the integrity of the Planning approval process and to comply with the requirements of the Judicial Review Act and the Local Government (Meeting Procedures) Regulations 2015.

Council is required to exercise a discretion within the statutory 42-day period, which was extended with the agreement of the applicant, and now expires on 10 October 2023.

CONSULTATION

The proposal was advertised in accordance with statutory requirements and seven representations were received raising the following issues:

- Overshadowing;
- Privacy;
- Construction disruptions;
- Devaluation of neighbouring property; and
- Provision of onsite amenity.

RECOMMENDATION:

A. That the Development Application for Additions and Alterations (Single Dwelling) at 249 Carella Street, Howrah (CI Ref PDPLANPMTD-2023/036864) be approved subject to the following conditions and advice.

1. GEN AP1 – ENDORSED PLANS.
2. The additions and alterations are approved as part of the single dwelling and must not be used for visitor accommodation or a secondary residence, unless otherwise approved by the Planning Authority.

ADVICE

- a. This Permit will lapse after two years from the date on which it is granted unless the development/use has been substantially commenced. Upon request, under Section 53(5A) of the Land Use Planning and Approvals Act 1993, Council may grant an extension of time for a further two years. A further two years may be granted upon request under Section 53(5B) of the Land Use Planning and Approvals Act 1993. Any such requests must be made in writing and within six months of the day on which the permit has lapsed.
 - b. This is a town planning permit only. Please be aware that a building permit and/or a plumbing certificate of likely compliance or plumbing permit may be required before the development can proceed. It is recommended that you contact Council's Building Department on (03) 6217 9580 to discuss the requirement for any additional permits or certification.
 - c. Non-compliance with this permit is an offence under Section 63 of the Land Use Planning and Approvals Act 1993 and may result in enforcement action under Division 4A of the Land Use Planning and Approvals Act 1993, which provides for substantial fines and daily penalties.
 - d. A Building Surveyor is required to be engaged, to create and certify an Application for Building Approval.
- B. That the details and conclusions included in the Associated Report be recorded as the reasons for Council's decision in respect of this matter.

ASSOCIATED REPORT**1. BACKGROUND**

No relevant background.

2. STATUTORY IMPLICATIONS

2.1. The land is zoned General Residential under the Scheme.

2.2. The proposal is discretionary because it does not meet the Acceptable Solutions under the Scheme.

2.3. The relevant parts of the Planning Scheme are:

- Section 5.6 – Compliance with Applicable Standards;
- Section 6.10 – Determining Applications;
- Clause 8 – General Residential Zone;
- Clause C2.0 – Parking and Sustainable Transport Code;
- Clause C12.0 – Flood-Prone Areas Hazard Code; and
- Clause C.16 – Safeguarding of Airports Code.

2.4. Council's assessment of this proposal also considers the issues raised in any representations received, the outcomes of the State Policies and the objectives of Schedule 1 of the *Land Use Planning and Approvals Act, 1993* (LUPAA).

3. PROPOSAL IN DETAIL

3.1. The Site

The site is a 658m² corner block with a primary frontage to Carella Street and a secondary frontage to Somerset Street. The site is oriented from north-east to south-west. The site has an existing crossover, providing access to the site from Somerset Street. The site has a moderate declining slope from the Carella Street frontage to the rear of the property, with a gradient of 1 in 7.

The site is fully serviced by TasWater potable water and reticulated sewerage, and council stormwater infrastructure.

The site is within an established residential area, with the adjoining land to the south developed with a single dwelling (251 Carella Street), and a single dwelling to the rear (86 Elinga Street). The site is proximate to the River Derwent, around 200m to the west.

3.2. The Proposal

The proposal is for additions and alterations at 249 Carella Street. The additions include a proposed deck to the west of the existing dwelling and a dwelling extension to the east, inclusive of a home office, kitchenette, bedroom and ensuite. The proposed additions have a maximum height of 7.6m.

The deck addition will have a 3m setback from the southern side boundary and a 7.19m setback from the rear boundary. No changes to the parking arrangement on-site are proposed. The proposed site coverage is 33%.

4. PLANNING ASSESSMENT

4.1. Compliance with Applicable Standards [Section 5.6]

“5.6.1 A use or development must comply with each applicable standard in the State Planning Provisions and the Local Provisions Schedules.”

4.2. Determining Applications [Section 6.10]

“6.10.1 In determining an application for any permit for use or development the planning authority must, in addition to the matters required by section 51(2) of the Act, take into consideration:

- (a) all applicable standards and requirements in this planning scheme; and*
- (b) any representations received pursuant to and in conformity with section 57(5) of the Act, but in the case of the exercise of discretion, only insofar as each such matter is relevant to the particular discretion being exercised.”*

References to these principles are contained in the discussion below.

4.3. General Provisions

The Scheme contains a range of General Provisions relating to specific circumstances not controlled through the application of Zone, Code or Specific Area Plan provisions.

There are no General Provisions relevant to the assessment of this proposal.

4.4. Compliance with Zone and Codes

The proposal does not require an assessment against the Safeguarding of Airports Code as the proposed development height is below the Obstacle Surface Layer (OSL) height of 147m, and it is located outside the airport noise attenuation area.

The proposal meets the Scheme’s relevant Acceptable Solutions of the General Residential Zone, the Car Parking and Sustainable Transport Code and the Flood-Prone Areas Hazard Code, with the exception of the following.

General Residential Zone

- **Clause 8.4.2 Setbacks and building envelope – P3** – the proposal cannot meet part (a)(i) and (a)(ii) of the acceptable solution given there are protrusions outside of the building envelope.

The application must be assessed against Performance Criteria P3 of 8.4.2 *Setbacks and building envelopes for all dwellings* as follows:

Clause	Performance Criteria	Assessment
8.4.2 P3	<i>“The siting and scale of a dwelling must:</i>	
	<i>(a) Not cause an unreasonable loss of amenity to adjoining properties, having regard to:</i>	The proposal is assessed as not causing an unreasonable loss of amenity to the adjoining properties through siting and scale. Shadow diagrams provided have been reviewed in assessing the Performance Criteria.
	<i>(i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;</i>	The analysis of the shadow diagrams (refer shadow diagram drawings provided as part of plan set) indicate that the adjoining dwelling to the south accessed from Carella Street and those to the west, accessed from Somerset Street and Elinga Street are most impacted by overshadowing on 21 June between 9am and 3pm.

		<p>The shadow diagrams show the impact of overshadowing to the western adjoining dwellings is not sustained throughout the day on 21 June. In effect, the dwellings located to the west of the subject site are only impacted by overshadowing on 21 June before 10am and therefore, it is expected that any windows to habitable rooms will receive adequate sunlight between the hours of 9am and 3pm.</p> <p>Regarding the dwelling to the south, there is only one window, along the northern elevation, to a habitable room, other than a bedroom, which is impacted by overshadowing. This is the kitchen window, which faces onto the deck. The applicant has provided existing and proposed shadow diagrams, which demonstrate that the window will not be any more impacted by overshadowing, other than that which is already caused by the existing dwelling.</p>
	<p>(ii) overshadowing the private open space of a dwelling on an adjoining property;</p>	<p>The adjoining property to the south is most affected by overshadowing; however, the shadow diagrams revealed that on 21 June at least 50% of the private open space at 251 Carella Street will receive a minimum of three hours of sunlight. Additionally, the large upper floor deck area of 251 Carella Street provides as additional private open space area, which is likely to be less impacted by overshadowing, due to its elevation from ground level.</p> <p>The private open space of the properties to the west will be afforded with full access to sunlight from midday onwards on 21 June.</p>

		Additional overshadowing, other than that which is already existing to the private open space of dwellings on adjoining properties, caused by the proposal, is not considered unreasonable.
	<i>(iii) overshadowing of an adjoining vacant property; and</i>	Not applicable. There are no adjoining vacant properties.
	<i>(iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property.</i>	<p>When viewed from the adjoining property to the south, the visual impacts will appear minimal. Firstly, the proposed additions are developed in a way that compliments the slope of the land. The maximum height of the dwellings is 7.7m and the new addition offers visual respite by being constructed with materials varying from the existing dwelling.</p> <p>When viewed from the adjoining properties to the west, visual bulk is minimised by variations in height. Further to this, the western elevations provide material variation and well-articulated window fenestration.</p> <p>It is considered that the proposed design presents a reasonable outcome to minimise any visual impacts when viewed from adjoining properties.</p>
	<i>(b) Provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and</i>	Howrah is an established residential area with an historic pattern of single dwelling development. The proposed side and rear boundary setbacks of the proposed additions and alterations are assessed as providing separation to adjoining properties, which is consistent with that established in the area.

		This can be further demonstrated by the proposed pattern of separation being similarly compatible with the surrounding and established properties.
	(c) <i>Not cause an unreasonable reduction in sunlight to an existing solar energy installation on:</i>	Solar installations are evident at 88 Elinga Street.
	(i) <i>an adjoining property; or</i>	88 Elinga Street appears to have solar panels on their north-east facing roof; however, the existing and proposed overshadowing diagrams on 21 June, demonstrate there will be no increased overshadowing impact. The solar panels are within shadow at 9am; however, will have full access to sunlight from 10am onwards on 21 June.
	(ii) <i>another dwelling on the same site.”</i>	Not applicable. In summary, the application complies with the standard through the performance criteria.

- **C12.6.1 Buildings and works within a flood-prone hazard area – P1.1** – There is no acceptable solution.

The application must be assessed against Performance Criteria P1.1 of *C12.6.1 Buildings and works within a flood-prone hazard area* as follows.

Clause	Performance Criteria	Assessment
P8.4.6 P2	<i>“Buildings and works within a flood-prone hazard area must achieve and maintain a tolerable risk from a flood, having regard to:</i>	The proposed dwelling addition to the east of the existing dwelling is within a flood-prone hazard area; however, is assessed as being able to achieve and maintain a tolerable risk from a flood.

	<i>(a) the type, form, scale and intended duration of the development;</i>	The proposed addition is for a double floor extension to the existing dwelling, as well as a deck area. As a result, the site coverage will increase by approximately 55m ² with the total site coverage proposed at around 33% of the site.
	<i>(b) whether any increase in the level of risk from flood requires any specific hazard reduction or protection measures;</i>	No hazard reduction or protection measures are required.
	<i>(c) any advice from a State authority, regulated entity or a council; and</i>	The application was referred to Council's engineers, who have formed the view that the risk level is not great enough to request a Flood Report. Council's engineers provided advice, suggesting that due to the site being a corner block, a small amount of flood stream and overland flow is redirected from the road into a small depression on the subject site. As such, any flooding impacts are likely to be negligible and the dwelling addition will not contribute to an intolerable flood risk.
	<i>(d) the advice contained in a flood hazard report."</i>	Not applicable. A flood report was not required for the assessment of the standard. The proposal is assessed as meeting the performance criteria.

5. REPRESENTATION ISSUES

The proposal was advertised in accordance with statutory requirements and seven representations were received. The following issues were raised by the representors.

5.1. Overshadowing

Representors are concerned that the proposal will cause unreasonable overshadowing into the neighbouring properties.

Additionally, representors have raised concerns regarding overshadowing solar installations on neighbouring properties at 88 Elinga Street and proposed solar installations at 251 Carella Street.

- **Comment**

The overshadowing has been assessed within the report and the application is assessed as complying with *8.4.2 Setbacks and building envelopes for all dwellings*. All existing windows to habitable rooms, excluding bedrooms, on adjoining properties are assessed as being able to receive adequate access to sunlight throughout the day on 21 June. Similarly, at least 50% of the private open space on all adjoining properties is considered to receive a minimum of three hours of sunlight between 9am and 3pm.

Overshadowing of solar installations at 88 Elinga Street has been addressed within the reports and the application is assessed as complying with performance criteria P3 of *8.4.2 Setbacks and building envelopes for all dwellings*. The planning scheme only allows for the assessment of overshadowing to existing solar installations. The fact that there is proposed solar installations to be installed at 251 Carella Street is inconsequential to the current assessment.

5.2. Privacy

Representors are concerned with the privacy implications to the southern and western neighbouring properties, regarding the deck addition.

- **Comment**

The deck, which has a finished floor level of more than 1m from existing ground level, is setback from the southern side boundary by 3m and by 7.192m from the rear western boundary. As such, and as stipulated in clause 8.4.6 *Privacy for all dwellings*, a privacy screen is not required. The proposal is considered to meet the acceptable solution regarding privacy.

5.3. Construction Disruption

Representors are concerned with disruptions, including noise, associated with the construction of the additions and alterations.

- **Comment**

This is not a planning matter and has no determining weight regarding the planning decision. Despite this, construction parameters will be set as part of the building approval.

5.4. Devaluation of Neighbouring Property

Representors believe the proposal will decrease the value of surrounding properties.

- **Comment**

This is not a planning matter and has no determining weight regarding the planning decision.

5.5. The Provision of On-site Amenity

One of the representors queried the provision of on-site parking, landscaping, drainage, and private open space.

- **Comment**

The proposal is assessed as meeting the private open space and car parking requirements of the Tasmanian Planning Scheme – Clarence. The General Residential Zone has no landscaping provisions and therefore this is not a planning matter and bears no weight regarding the decision of the application. Drainage will be assessed at the plumbing approval stage.

6. EXTERNAL REFERRALS

No external referrals were required or undertaken as part of this application.

7. STATE POLICIES AND ACT OBJECTIVES

7.1. The proposal is consistent with the outcomes of the State Policies, including those of the State Coastal Policy.

7.2. The proposal is consistent with the objectives of Schedule 1 of LUPAA.

8. COUNCIL STRATEGIC PLAN/POLICY IMPLICATIONS

There are no inconsistencies with Council's adopted Strategic Plan or any other relevant Council Policy.

9. CONCLUSION

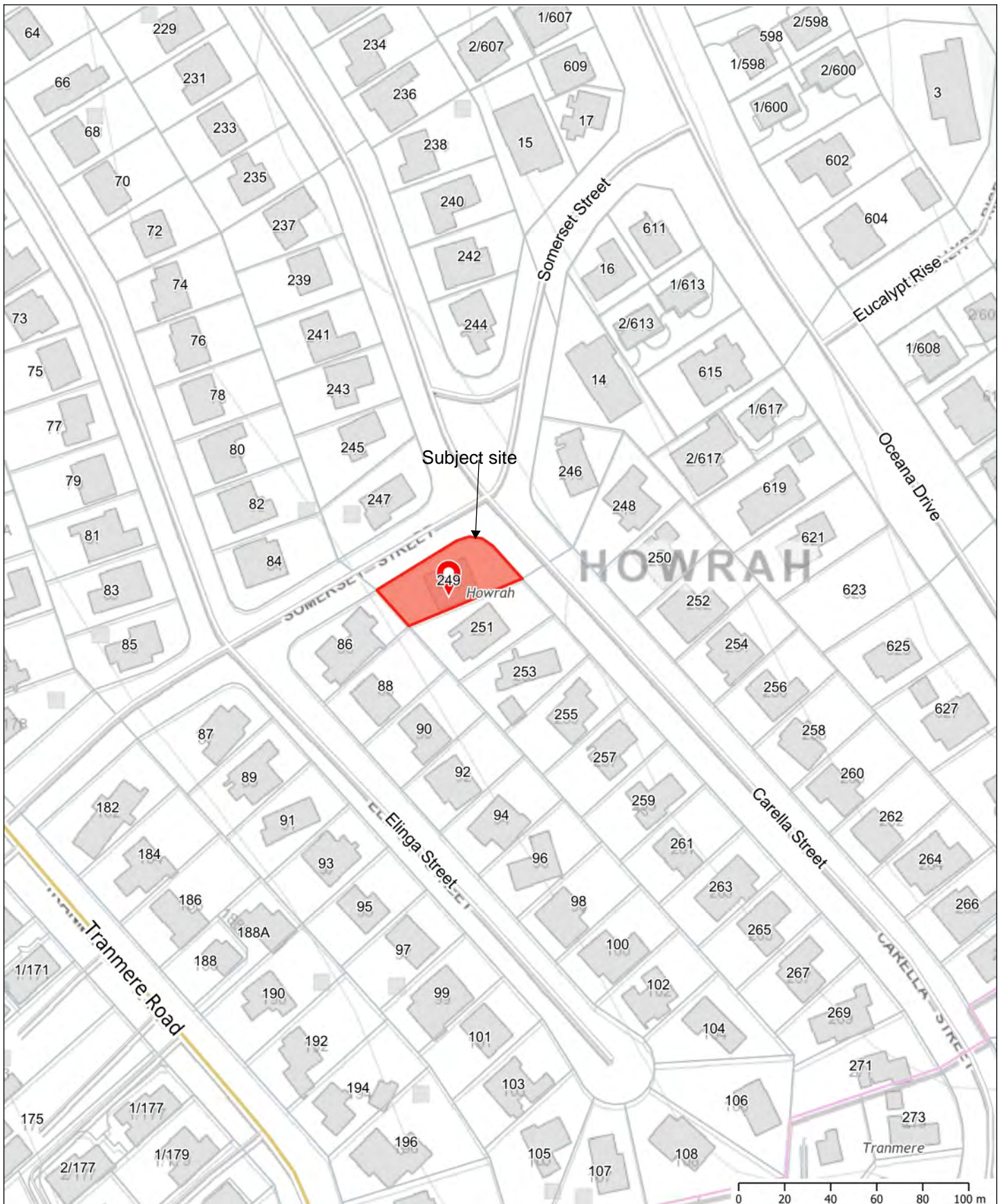
The proposal is considered to meet the relevant standards of the scheme and is therefore recommended for approval, subject to conditions.



Attachments: 1. Location Plan (1)
2. Proposal Plan (11)
3. Site Photo (1)

Robyn Olsen
ACTING HEAD OF CITY PLANNING

Attachment 1

LOCATION PLAN - 249 CARELLA STREET, HOWRAH



 <p>Clarence... a brighter place</p>	<p>This map has been produced by Clarence City Council using data from a range of agencies. The City bears no responsibility for the accuracy of this information and accepts no liability for its use by other parties.</p>	<p>27/09/2023</p> <p>1:2257</p>	
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INDEX OF DRAWINGS – BY DARRYN WHITE – CC1623W

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SITE INFORMATION

CERTIFICATE OF TITLE: 62430/759
LAND AREA: 656.98M²

EXISTING DWELLING	– 282.55M ²
PROPOSED ADDITION	– 79.69M ²
PROPOSED DECK	– 63.54M ²

OVERALL DEVELOPMENT FOOTPRINT – 218.25M²

PLANNING SCHEME: TASMANIAN PLANNING SCHEME – CLARENCE

ZONE: GENERAL RESIDENTIAL

OVERLAY: SAFEGUARDING AIRPORTS CODE, FLOOD-PRONE HAZARD CODE

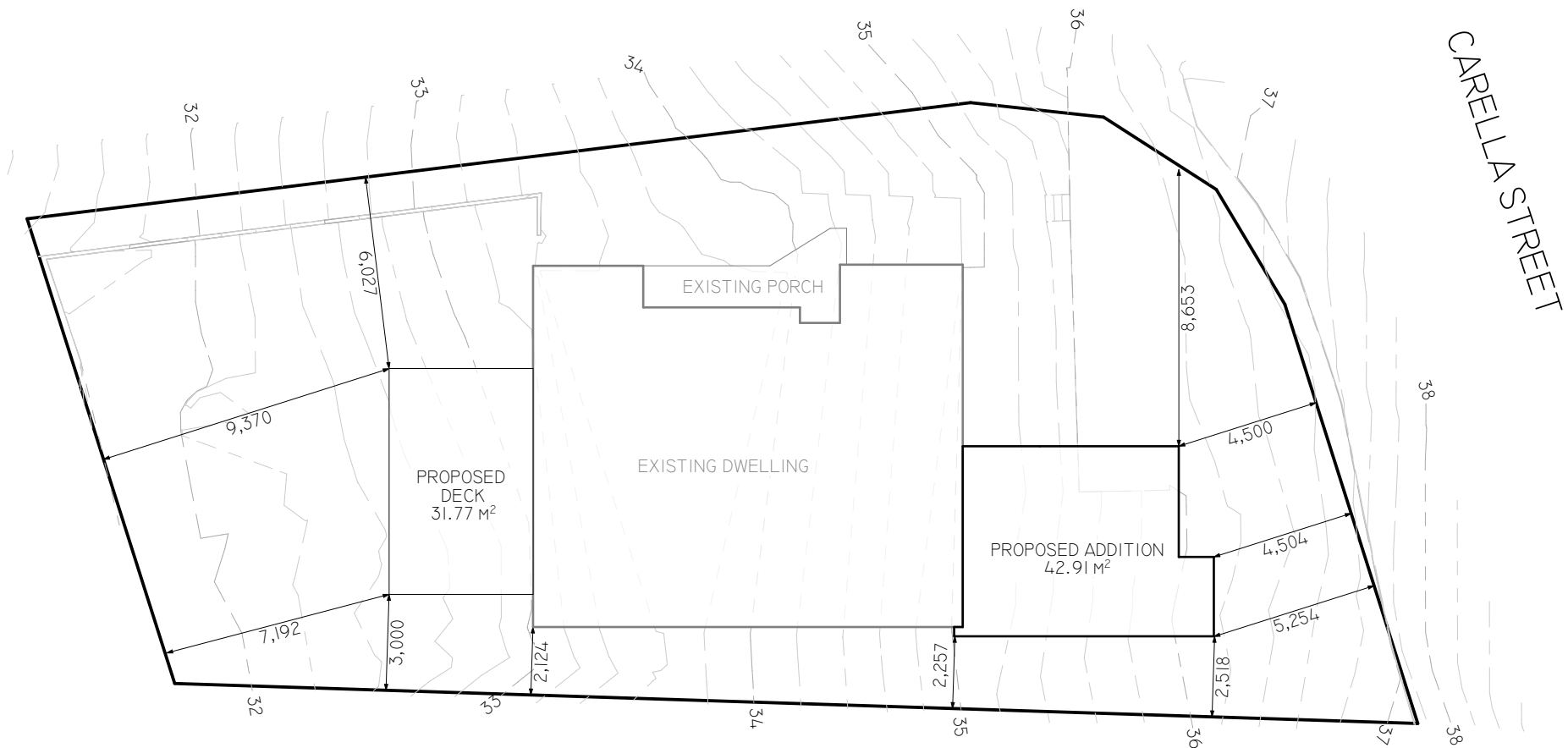
BAL RATING: Low – SEE LISTMAP FOR EXEMPTION

Attachment 2

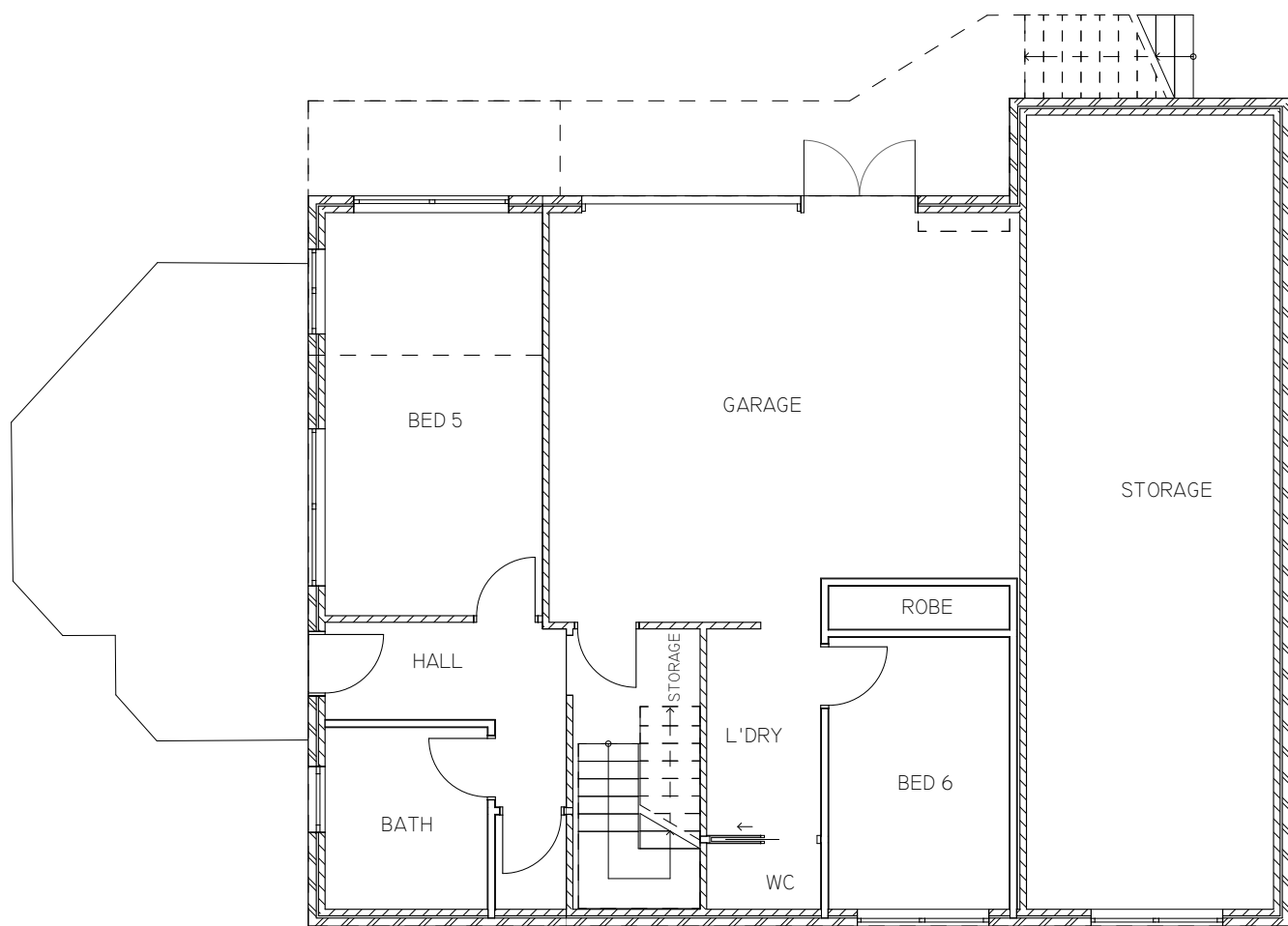


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Scale 1:200		Address 249 Carella Street Howrah, 7018					Job No 2218

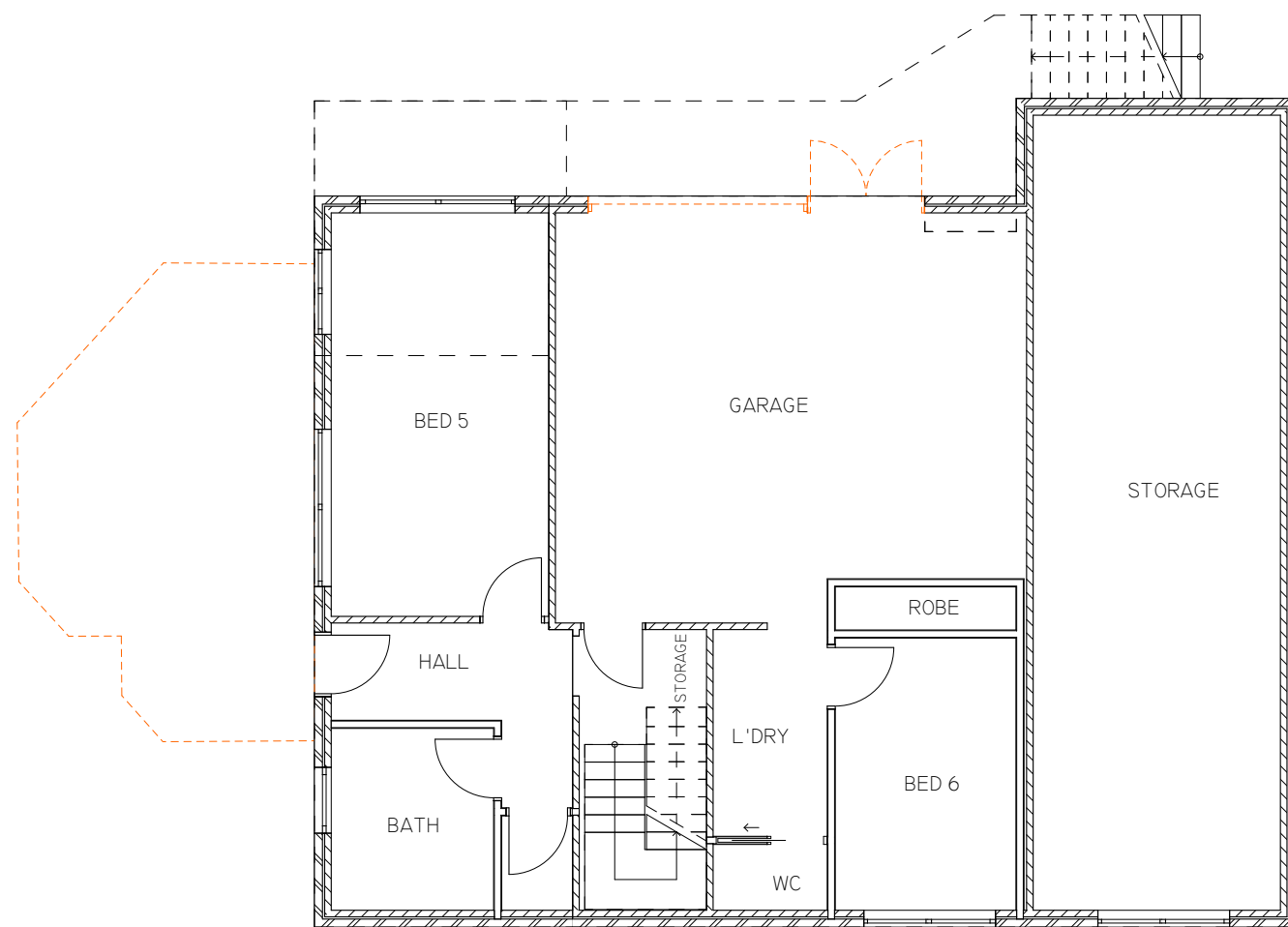
SOMERSET STREET



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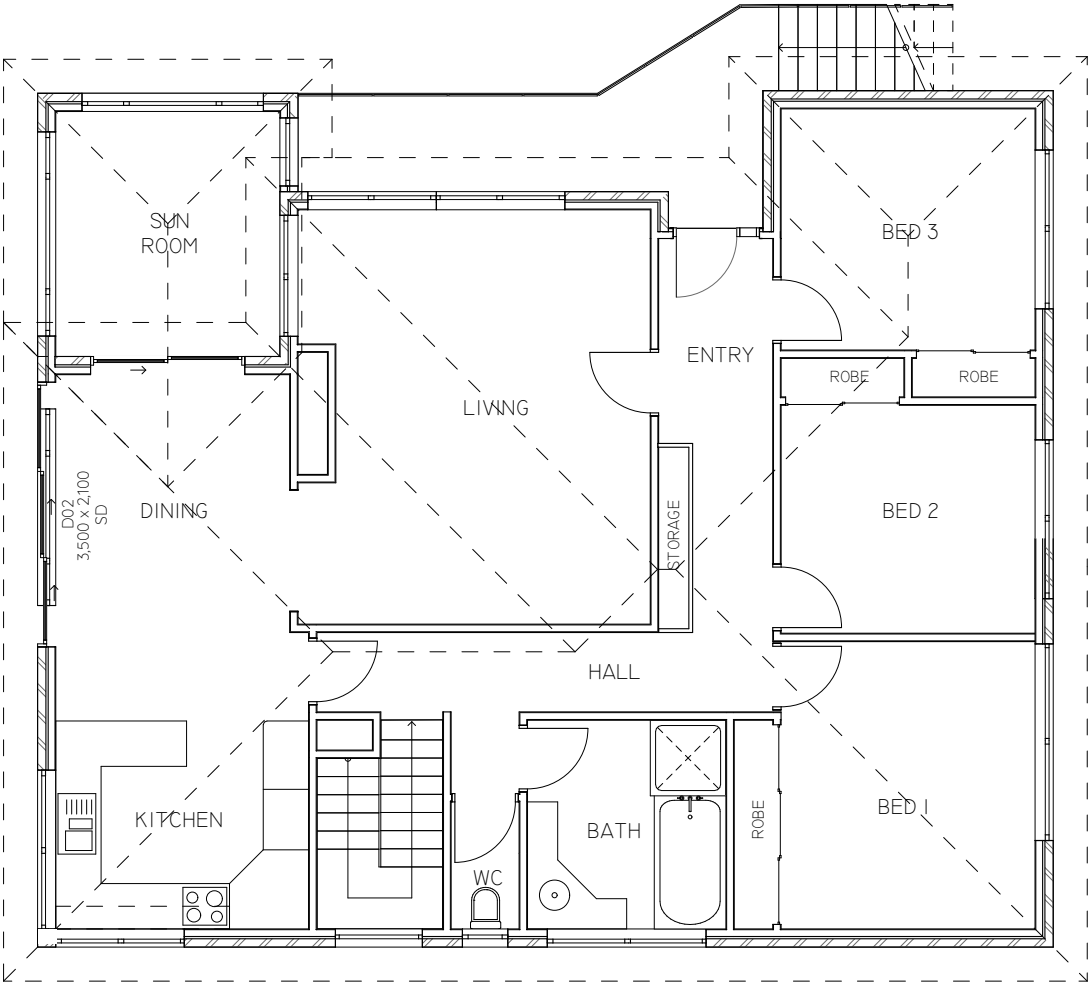


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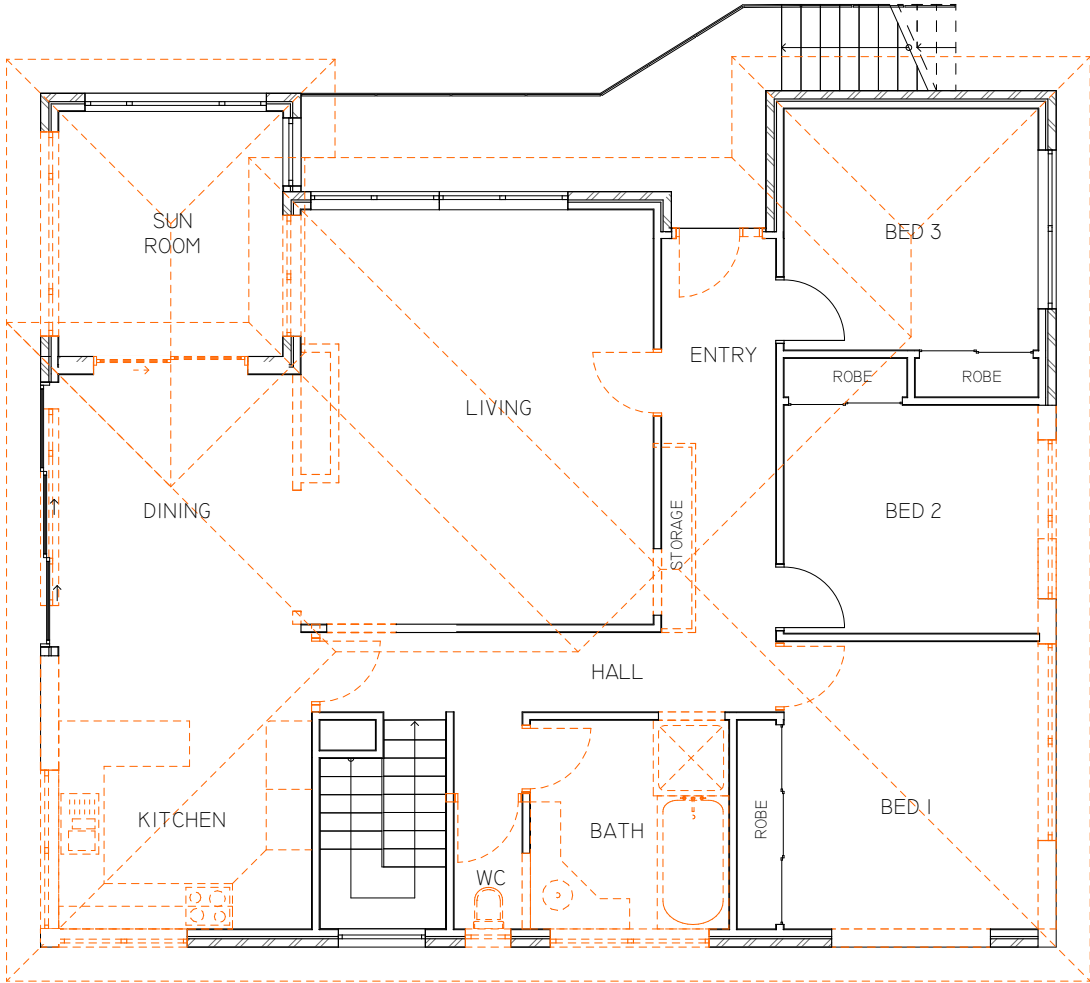


DEMOLITION PLAN

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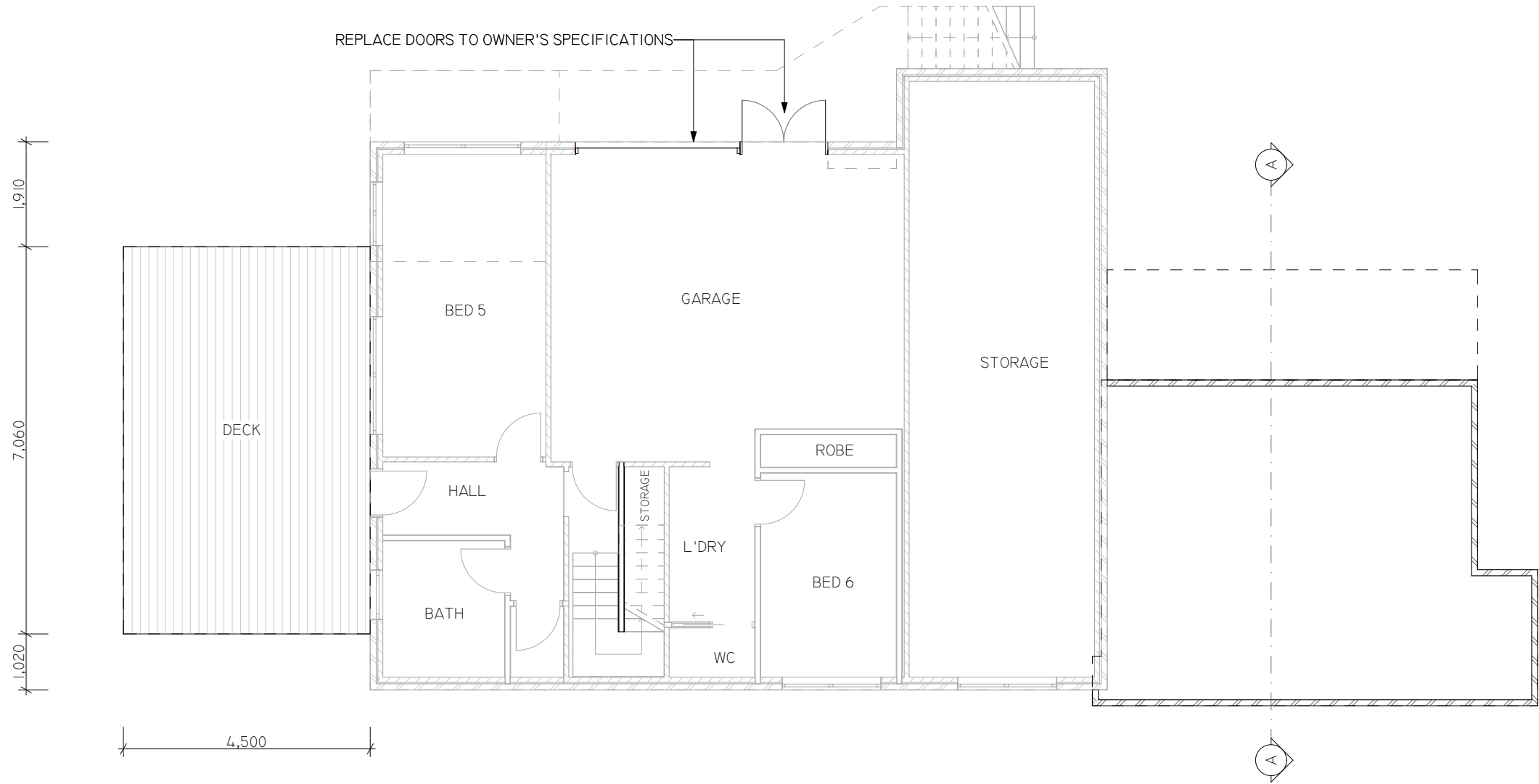


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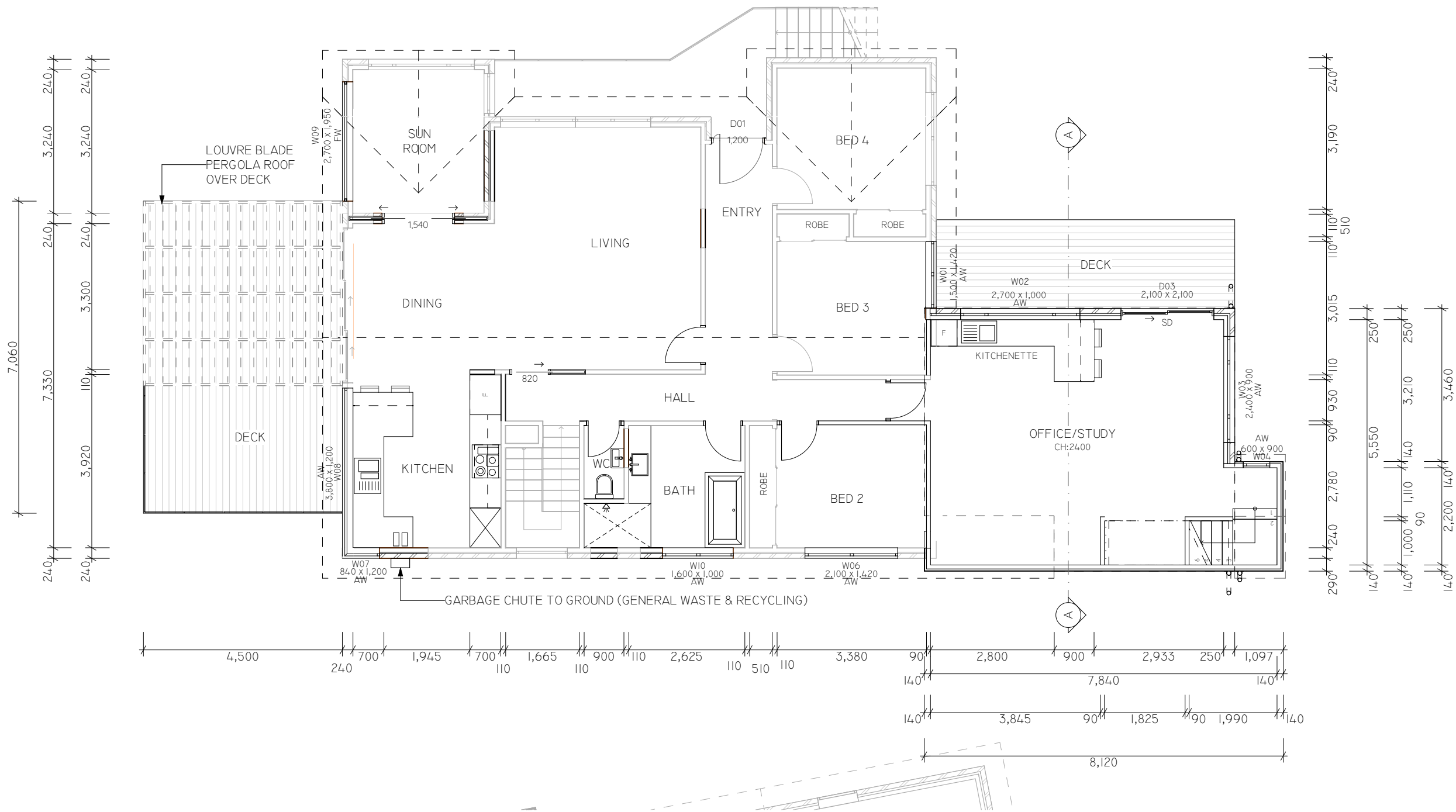


DEMOLITION PLAN

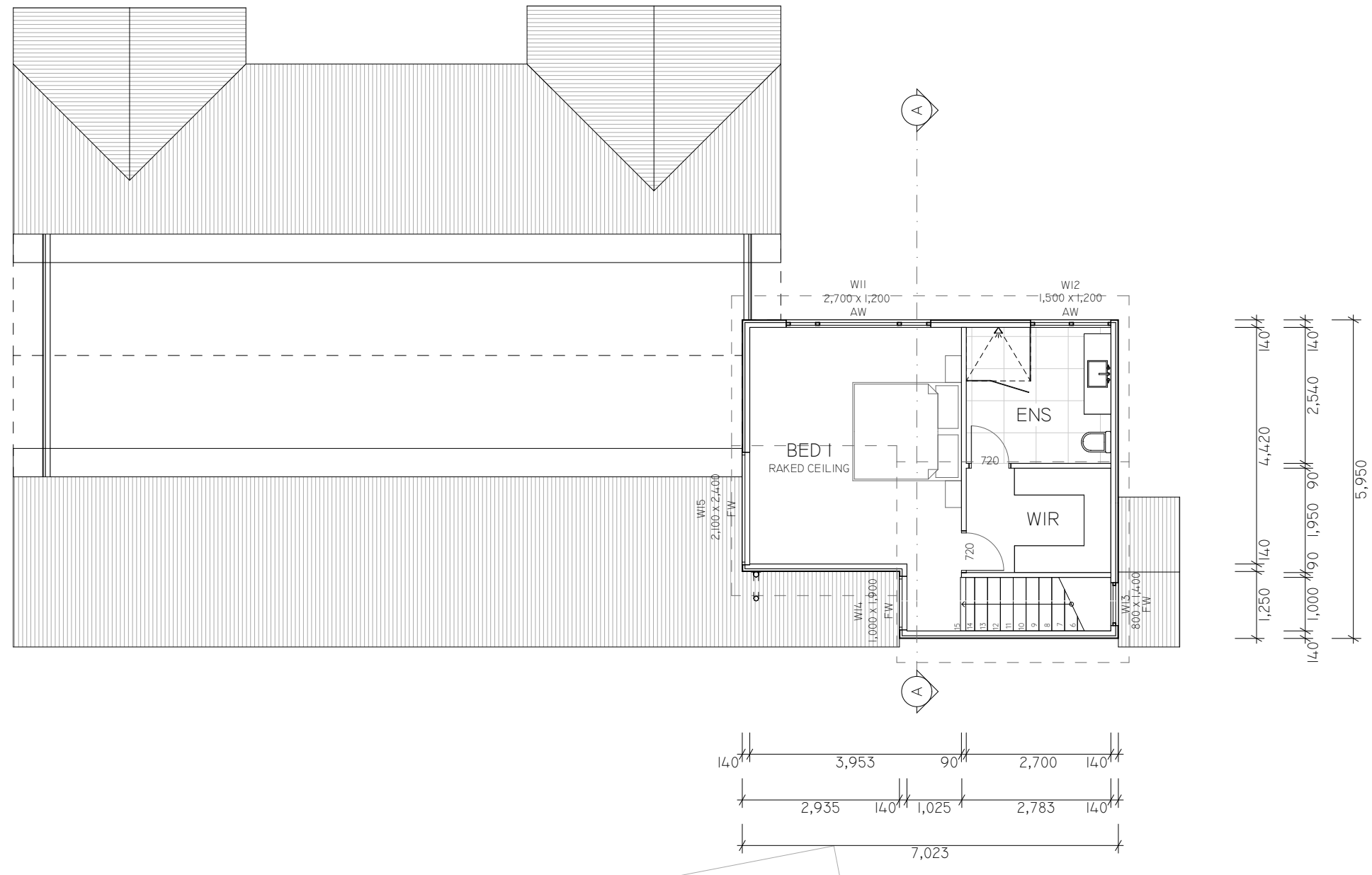
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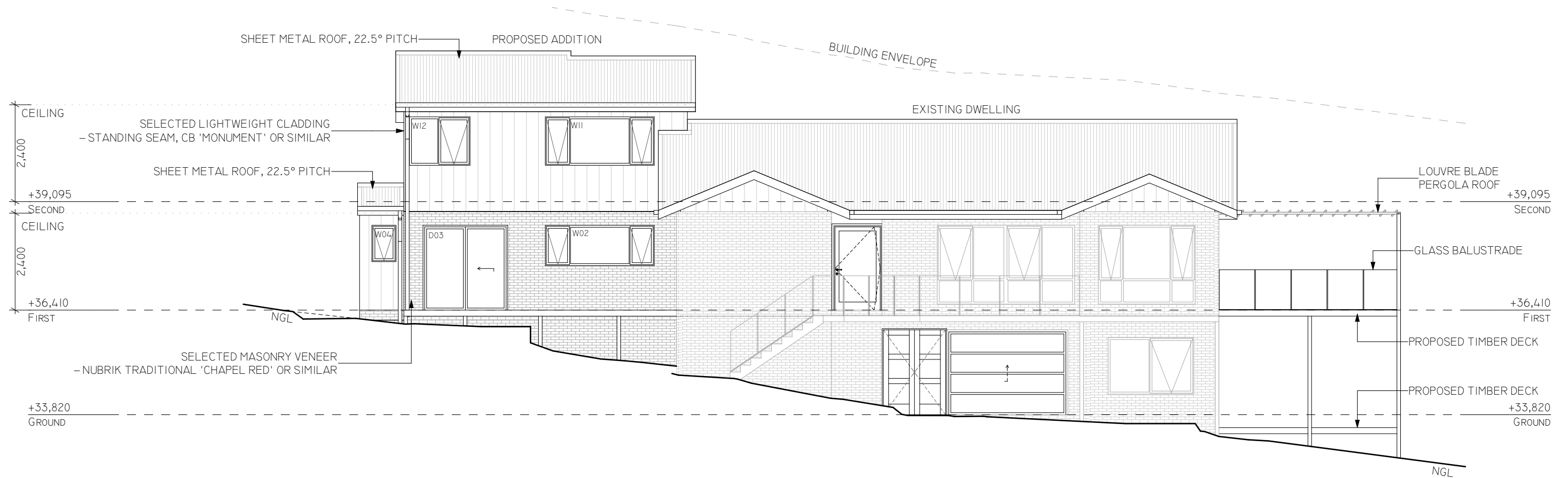
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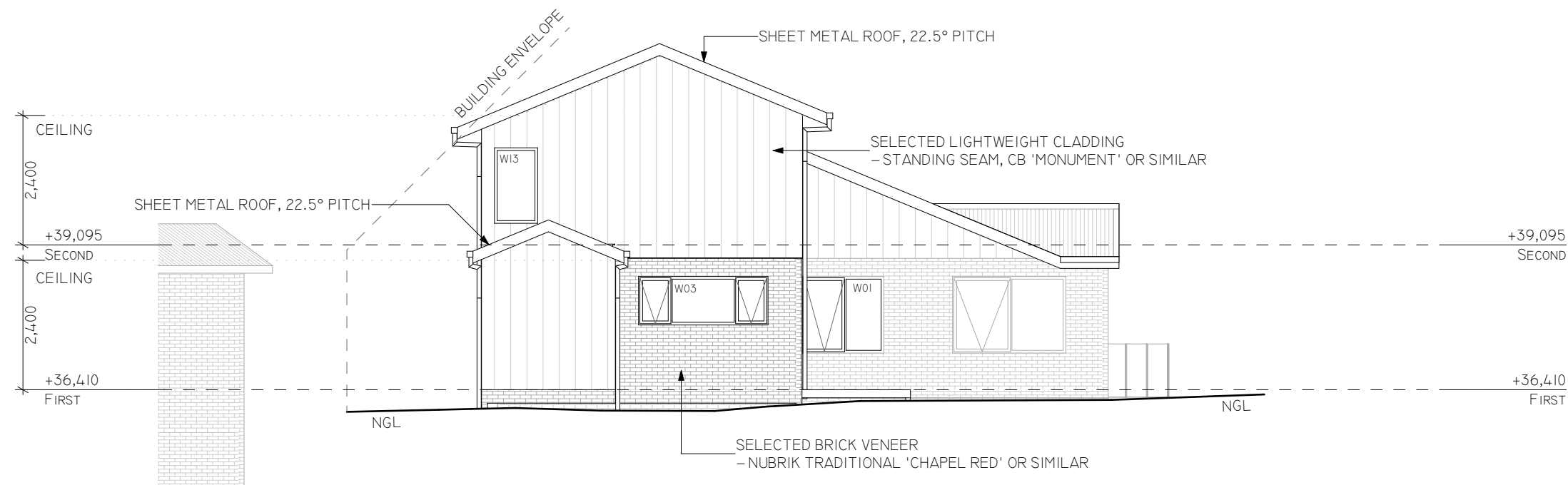
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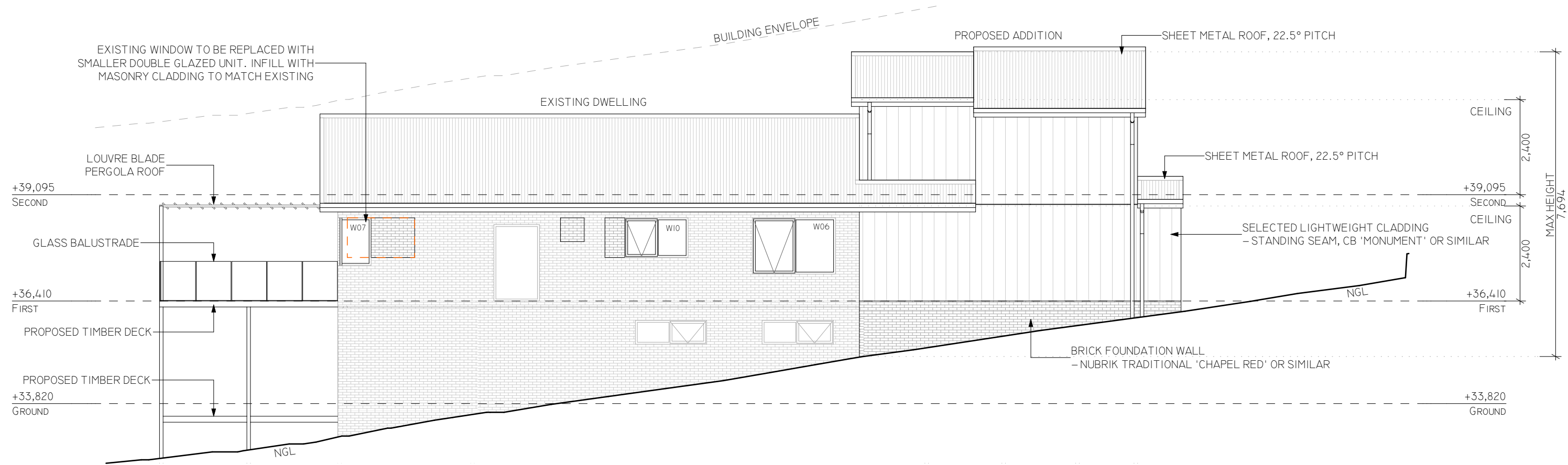


NORTH ELEVATION

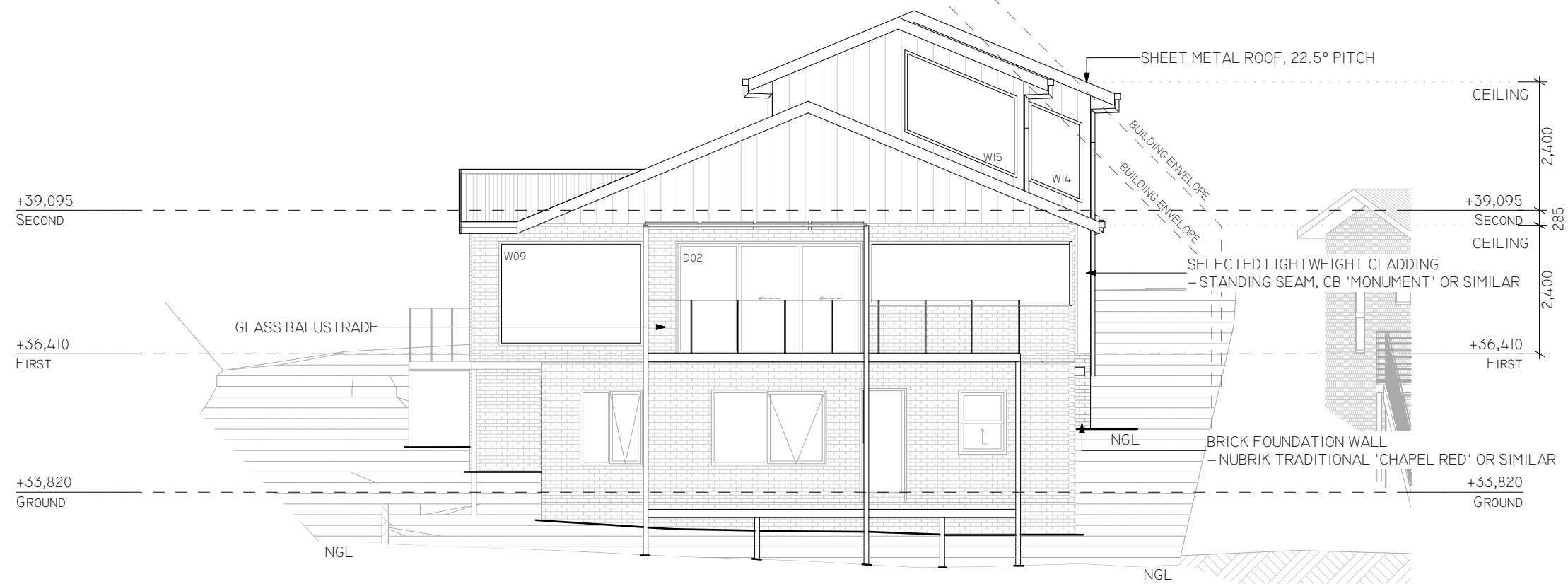


EAST ELEVATION

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SOUTH ELEVATION



WEST ELEVATION

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Job No 2218										



EXISTING 21st JUNE 9am



EXISTING 21st JUNE 10am



EXISTING 21st JUNE 11am



EXISTING 21st JUNE 12pm



EXISTING 21st JUNE 1pm



EXISTING 21st JUNE 2pm



EXISTING 21st JUNE 3pm

Date	28th August 2023	Page size <div>A3</div>	Client	A & J Buckland	Proposal <div>Addition/Alteration</div>	Drawing	SHADOW DIAGRAMS - EXISTING		© 2022	Page No	<div>1112</div>
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						ACCREDITATION NO: CC1623W		2218			



PROPOSED 21st JUNE 9am



PROPOSED 21st JUNE 10am



PROPOSED 21st JUNE 11am



PROPOSED 21st JUNE 12pm



PROPOSED 21st JUNE 1pm



PROPOSED 21st JUNE 2pm



PROPOSED 21st JUNE 3pm

Date	28th August 2023	Page size <div>A3</div>	Client	A & J Buckland	Proposal <div>Addition/Alteration</div>	Drawing	SHADOW DIAGRAMS - PROPOSED		© 2022	Page No	<div>1212</div>
Scale	1:500		Address	249 Carella Street Howrah, 7018		Darryn White - Building Design and Consulting. P O Box 381 Rosny Park Tasmania 7018 P: 0409 659 358 E: dwbdac@gmail.com W: www.everythingbuilding.com.au ABN: 56130097060	This drawing is the property of Darryn White Reproduction in whole or part is strictly forbidden without the written consent of Darryn White Failure in doing will result in legal action being taken.	Job No		2218	
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Attachment 3



Photo 1: South-western elevation of 249 Carella Street, taken corner Carella St and Somerset St



Photo 2: eastern elevation of 249 Carella Street, taken from Somerset Street

7.4 DEVELOPMENT APPLICATION PDPLANPMTD-2023/036283 – 1089 AND 1099 SOUTH ARM ROAD, SANDFORD (WITH ACCESS FROM SCHOOL ROAD AND STORMWATER DISCHARGE TO SOUTH ARM HIGHWAY) - 14 LOT SUBDIVISION AND BOUNDARY ADJUSTMENT INCLUDING PUBLIC ROAD AND PUBLIC OPEN SPACE LOTS**EXECUTIVE SUMMARY****PURPOSE**

The purpose of this report is to consider the application made for a 14 lot subdivision and boundary adjustment including public road and public open space lots at 1089 and 1099 South Arm Road, Sandford (with access from School Road and stormwater discharge to South Arm Highway).

RELATION TO PLANNING PROVISIONS

The land is zoned Rural Living and is subject to the Sandford Specific Area Plan, Bushfire-prone Areas Code, Coastal Inundation Hazard Code, Flood-prone Hazard Areas Code, Landslip Hazard Code, Natural Assets Code, Road and Railway Assets Code, Safeguarding of Airports Code under the Tasmanian Planning Scheme - Clarence (the Scheme). In accordance with the Scheme the proposal is a Discretionary development.

LEGISLATIVE REQUIREMENTS

The report on this item details the basis and reasons for the recommendation. Any alternative decision by Council will require a full statement of reasons in order to maintain the integrity of the Planning approval process and to comply with the requirements of the Judicial Review Act and the Local Government (Meeting Procedures) Regulations 2015.

Council is required to exercise a discretion within the statutory 42-day period which expires on 11 October 2023 as agreed with the applicant.

CONSULTATION

The proposal was advertised in accordance with statutory requirements and eight representations were received raising the following issues:

- Access from School Road;
- Continued operation of the Sandford Fire Station;
- Impact on rural ambience and views;
- Impact on property values; and
- Road layout.

The proposal was referred to the Environment Protection Authority (EPA) who provided advice on the current quarry permit activity at 100 School Road, concerns about potential environmental impacts on future dwellings on the proposed lots, and the potential for significant land use conflict to fetter the quarry operations. Correspondence is included in full in Attachment 4 of the Associated Report.

RECOMMENDATION:

- A. That the Development Application for a 14 lot subdivision and boundary adjustment including public road and public open space lots at 1089 and 1099 South Arm Road, Sandford (with access from School Road and stormwater discharge to South Arm Highway) (CI Ref PDPLANPMTD-2023/036283) be refused as the proposal is contrary to the provisions of the Tasmanian Planning Scheme – Clarence for the following reason.
1. The proposal does not comply with Clause C9.6.1 P1(a) and (b) Lot design within an attenuation area of the Attenuation Code, as the proposal results in potential for future sensitive use to be impacted by an existing activity listed in Table C9.1 (Extractive industry) and would constrain the operation of this existing activity.
- B. That the details and conclusions included in the Associated Report be recorded as the reasons for Council’s decision in respect of this matter.

ASSOCIATED REPORT**1. BACKGROUND****Zoning History**

Two Section 43A applications, A-2011/12 and SD-2011/36 and A-2012/2 and SD-2012/7, were initiated and certified simultaneously by Council at its meeting of 25 June 2012 and were supported following public consultation and comment at Councils meeting of 27 August 2012. The matter was heard by the Tasmanian Planning Commission (TPC) on 14 November 2012. On 21 January 2013, the TPC decided:

“that the draft amendments should be rejected as they are not considered to represent sustainable development and are not based on sound strategic planning principals for the following reasons:

1. *the conversion of agricultural land to rural residential use outside of the rural residential growth boundary in circumstances where adequate medium term rural residential land supply exists; and*
2. *that the potential for sensitive uses to be impacted by and impacted upon extractive industries is inconsistent with the objectives and strategies of the scheme.*” [Emphasis added]

The panel report expanded on the reasons and the key issues of concern included:

- The Southern Tasmania Regional Land Use Strategy (STRLUS) provides no capacity for the introduction of new rural residential land use beyond land which is currently zoned as such under the Scheme.
- The fact that the proposal is outside of the Rural Residential Growth Boundary in the Scheme's Planning Policy Framework.
- The failure of the proposal to adequately address the amenity with regard to the Sandford Quarry and the possible future intensification of its use.
- The failure of the proposal to adequately address the Protection of Agricultural Land Policy.

Subsequent to the decision, the applicants initiated a number of meetings with the TPC, Council and others. During the course of initial meetings, it transpired that the resolution of two key issues would help the TPC overcome its concerns and allow a new proposal to be approved. Those key issues were the resolution of amenity impacts from the Sandford Quarry and consistency with the STRLUS, which itself required amendment to allow for the rezoning. An amendment of the STRLUS was declared on 1 October 2013 and provided for the consideration of this proposal.

Following the above decision, the land was part of a rezoning (Planning Scheme amendment application reference A-2014/1) from Rural to Rural Living, enabled by amendments to the STRLUS, which included the creation of the Sandford Specific Area Plan (SAP), in part to enable the continued operation of the existing Sandford Quarry consistent with any Environmental Protection Notice (EPN) permit conditions that apply to the operation of the quarry.

In its decision, the TPC acknowledged that a change in the zoning will have implications for the assessment of any future application for the expansion of the quarry; however, it will retain a discretionary status through the provisions of the SAP and the standards of the SAP provide more protection by imposing requirements upon sensitive uses.

The TPC considered the evidence in relation to potential land use conflict and formed the view the SAP provided Council sufficient consideration to appropriate matters when exercising discretion for a single dwelling.

The site adjoins Rosevear Lane to the west containing five lots created as part of subdivision permit SD-2015/67, issued on 12 July 2021. The subdivision permit required covenants to be included on each title to ensure the future owners of the lots are aware of the potential for disturbance from the operation of the quarry, and conversely so that the operation of the quarry is not fettered by residential development.

A planning permit was granted by Council on 22 November 2021 for a dwelling and outbuilding at 4 Rosevear Lane, Sandford, which directly adjoins the northern boundary of 100 School Road containing the Sandford Quarry. The officer's recommendation for refusal was overturned at the council meeting. The refusal reasons included:

1. The proposal does not comply with Clause C9.5.2 Sensitive use within an attenuation area, as the proposal has the potential to constrain the operation of an existing activity listed in Table C9.1; and
2. The proposal does not comply with Clause CLA-S11.7.1 Residential amenity, as the proposal design and location is considered unlikely to be protected from unreasonable impacts from the quarry located at 100 School Road, Sandford and does not protect the quarry from potential land use conflict arising from the dwelling development.

In addition, the application for 4 Rosevear Lane was referred to the EPA who provided advice on the current quarry permit activity and raised concerns about potential environmental impacts on the proposed dwelling, and the potential for significant land use conflict to fetter the quarry operations. Also, a referral response from the Director of Mines advised that the development cannot be supported.

Sandford Fire Station

The initial version of the subdivision application involved a proposal to excise the fire station from proposed Lot 1 and for this lot to form part of the public open space contribution.

This lot was not considered suitable for public open space in accordance with section 5.11 of Council's Public Open Space Policy (2013), as the lot provides for utility services and provides no opportunity for Council to develop this land for public open space purposes.

The preferred approach was for this land to be transferred to the relevant state authority for the intended purpose, being emergency services. Following this advice, the subdivision application was revised to remove the excision of the fire station and is now absorbed within proposed Lot 1.

While it is recognised the fire station provides an important community service and the subdivision of 1089 South Arm Road presented an opportunity for the fire station to be transferred to the appropriate state authority to secure its long-term viability, Council is unable to impose a requirement for the transfer of this land without confirmation the relevant state authority would accept the transfer of the fire station to their ownership. The landowner was unable to provide confirmation of such an undertaking and can continue to allow for the fire station to operate on a lease basis as it currently does.

It is further noted the fire station could be subdivided from Lot 1 as part of a separate application should the negotiations between the landowner and relevant state authority prove successful.

A number of representations received in relation to the application have suggested the land on which the fire station is located is rezoned to Community Purposes to reflect the use of this land for emergency services.

The Tasmania Fire Service was contacted during the assessment process and has advised they are aware of the subdivision application and are exploring short- and long-term options to secure adequate service delivery within the area.

2. STATUTORY IMPLICATIONS

2.1. The land is zoned Rural Living under the Scheme.

- 2.2.** The proposal is discretionary because it does not meet the Acceptable Solutions under the Scheme.
- 2.3.** The relevant parts of the Planning Scheme are:
- Section 5.6 – Compliance with Applicable Standards;
 - Section 6.10 – Determining Applications;
 - Section 11.0 – Rural Living Zone;
 - Section C2.0 – Parking and Sustainable Transport Code;
 - Section C3.0 – Road and Railway Assets Code;
 - Section C7.0 – Natural Assets Code;
 - Section C9.0 – Attenuation Code;
 - Section C11.0 – Coastal Inundation Hazard Code;
 - Section C12.0 – Flood-prone Areas Hazard Code;
 - Section C13.0 – Bushfire-prone Areas Code;
 - Section C15.0 – Landslip Hazard Code; and
 - Section C16.0 – Safeguarding of Airports Code.
- 2.4.** The proposal is exempt from the Safeguarding of Airports Code in accordance with clause C16.4.1(a) of the Code, as the height of the development would be below the prescribed obstacle limitation surface height of 147m AHD.
- 2.5.** The upper slopes associated with the south-western corner of the site are covered by the landslip hazard overlay. The proposal is exempt from the Landslip Hazard Code in accordance with clause C15.4.1(e) of the Code.
- 2.6.** In accordance with clause C12.2.4 of the Flood-prone Areas Hazard Code, the code does not apply to land subject to the Coastal Inundation Hazard Code.

- 2.7.** Council's assessment of this proposal should also consider the issues raised in any representations received, the outcomes of the State Policies and the objectives of Schedule 1 of the *Land Use Planning and Approvals Act, 1993* (LUPAA).

3. PROPOSAL IN DETAIL

3.1. The Site

The site consists of two properties identified in Certificate of Title 175144 Folio 1 (1089 South Arm Road) and Certificate of Title 174930 Folio 2 (1099 South Arm Road).

The property known as 1089 South Arm Road is an irregular shaped lot of 34.06ha with its long axis running north-south. The property has frontage to South Arm Road to the east and School Road at the north-western boundary. At its highest point along the south-western property boundary, the lot has an elevation of 63m AHD and slopes down to the east with the low point being the boundary with South Arm Road.

The property is developed with an existing dwelling, outbuilding and unsealed driveway located within a 6m wide Right-of-Way accessed from School Road and shared with 1099 South Arm Road. A large dam occupies the northern end of the property.

The Sandford Fire Station is located at the northern end of the property with access to South Arm Road.

The property known as 1099 South Arm Road forms a 3.291ha lot with frontage to South Arm Road. The property is developed with a single dwelling located towards the rear of the site with access provided via a Right-of-way over 1089 South Arm Road to the north in the interests of minimising the number of direct accesses to South Arm Road.

The site is comprised of low grasslands, with the exception of the northern part of the site, which contains a scattering of remnant trees. The property is identified as Agricultural land in the TASVEG 4.0 [the Tasmanian-wide vegetation map produced by the Tasmanian Vegetation Monitoring and Mapping Program (TVMMP)].

The site is not within a TasWater service area for either potable water or sewerage.

School Road is sealed to the Kellow Place junction with the remaining length forming an unsealed council-maintained road.

The surrounding area includes vacant lots, and single residential dwellings. The general pattern of development shows existing dwellings being sited a minimum of 100m from the operational quarry at 100 School Road. The exceptions being dwellings approved at 2, 4 and 6 Rosevear Lane, under the provisions of the Clarence Interim Planning Scheme 2015 (CIPS2015), which respectively have rear boundary setbacks to the quarry site of 45m, 20m and 60m. These dwellings are now under construction.

3.2. The Proposal

The proposal is for a boundary adjustment between 1089 and 1099 South Arm Road and subdivision of 1089 South Arm Road resulting in the creation of 14 lots, public road lot and public open space lots.

The boundary adjustment would result in the transfer of a 1,590m² strip of land from 1089 South Arm Road to 1099 South Arm Road, resulting in the access to 1099 South Arm Road being contained wholly within the boundaries of this property and a Right-of-Way being created benefiting the existing dwelling on proposed Lot 4 of the proposed subdivision. The property known as 1099 South Arm Road would increase in land area to 3.45ha because of the boundary adjustment.

The subdivision of 1089 South Arm Road would result in the creation of 14 residential lots with each lot maintaining a minimum lot size of 2ha. A new internal subdivision road is proposed to be constructed from School Road to service the new lots. The intersection with School Road would be positioned to the south of Kellow Place and would lie parallel with the south-western property boundary before terminating in a cul-de-sac at the south-eastern corner of the property.

The existing fire station would be accommodated within proposed Lot 1 with no change proposed to the existing parking or access arrangements servicing the fire station.

The subdivision is proposed to be completed over four stages. Stage 1 would involve the completion of Lots 1, 2 and 3, all of which would obtain direct frontage and access to School Road. The boundary adjustment and creation of Lot 4 would form Stage 2 works. Stage 3 would involve the construction of the new subdivision road from School Road to Lots 7 and 14, with inclusion of temporary turning facilities between Lots 7 and 14. Stage 4 would form the final stage and would result in the completion of the subdivision road, Lots 8-13 and public open space lot.

Public open space is proposed in the form of a 10m wide walkway between proposed Lots 9 and 10, linking the proposed subdivision road to South Arm Road. The walkway entrance to South Arm Road would be proximate to a planned pedestrian crossing to the south of the intersection with Cremorne Avenue, with this location being deemed appropriate for a safe pedestrian crossing facility on South Arm Road to facilitate ease of access between Sandford and Cremorne.

The subdivision layout is consistent with the Sandford Master Plan as required by the Sandford Specific Area Plan.

A series of supporting reports was submitted as part of the application, including:

- Bushfire Hazard Report dated 7 February 2023
- Emissions Assessment dated 21 August 2023
- Stormwater Design Report dated 29 May 2023

The proposal involves the connection of the new subdivision road to School Road and discharge of stormwater to South Arm Road. Landowner consent has been obtained for the proposal from Council and the Department of State Growth (DSG).

4. PLANNING ASSESSMENT

4.1. Compliance with Applicable Standards [Section 5.6]

“5.6.1 A use or development must comply with each applicable standard in the State Planning Provisions and the Local Provisions Schedules.”

4.2. Determining Applications [Section 6.10]

“6.10.1 In determining an application for any permit for use or development the planning authority must, in addition to the matters required by section 51(2) of the Act, take into consideration:

- (a) all applicable standards and requirements in this planning scheme; and*
- (b) any representations received pursuant to and in conformity with section 57(5) of the Act, but in the case of the exercise of discretion, only insofar as each such matter is relevant to the particular discretion being exercised.”*

References to these principles are contained in the discussion below.

4.3. General Provisions

The Scheme contains a range of General Provisions relating to specific circumstances not controlled through the application of Zone, Code or Specific Area Plan provisions.

The proposal involves a minor boundary adjustment between the common boundary shared between 1089 and 1099 South Arm Road. General Provision 7.3.1 of the Scheme provides that an application for a boundary adjustment is Permitted, and a permit must be granted if subclause (a)-(f) below can be met. An assessment of the proposal's compliance with the boundary adjustment criteria is provided as follows.

“(a) no additional lots are created;”

The proposed boundary adjustment complies with subclause (a) in that no additional lots would be created through the boundary adjustment. The second element of the proposal is for a subdivision resulting in the creation of 14 additional lots.

“(b) there is only minor change to the relative size, shape and orientation of the existing lots;”

The boundary adjustment would result in a minor increase (less than 5%) in the land area of 1099 South Arm Road and a minor decrease (less than 5%) in the land area of 1089 South Arm Road. The adjustment between the common boundary between these titles would not result in any significant divergence from the existing lot areas or lot configuration. Further, the future development potential of either lot would not change because of the boundary adjustment. The boundary adjustment acts to legitimise the existing access arrangement for both lots. This being the case, the proposed adjustment is deemed minor, and subclause (b) is met.

“(c) no setback from an existing building will be reduced below the relevant Acceptable Solution setback requirement;”

The proposed boundary adjustment would not result in the altered boundary being reduced below the setback requirement detailed under the Rural Living Zone setback standards. The subclause is therefore met in this regard.

“(d) no frontage is reduced below the relevant Acceptable Solution minimum frontage requirement;”

The proposed boundary adjustment would result in marginal changes to the frontage dimensions for both lots to South Arm Road with the resultant dimension being well in excess of the minimum frontage requirements for Rural Living zoned land.

“(e) no lot is reduced below the relevant Acceptable Solution minimum lot size unless already below the minimum lot size; and”

Both lots subject to the boundary adjustment would retain more than the minimum lot size being 2ha.

“(f) no lot boundary that aligns with a zone boundary will be changed.”

Both lots are zoned Rural Living, therefore the boundary adjustment would not result in a misalignment with a zone boundary.

Based on the above assessment, the proposal has been assessed as compliant with the requirements of General Provision 7.3.1 of the Scheme.

4.4. Compliance with Zone and Codes

The proposal meets the Scheme’s relevant Acceptable Solutions of the Rural Living Zone, Sandford Specific Area Plan, Bushfire-prone Areas Code, Coastal Inundation Hazard Code, Flood-prone Hazard Areas Code, Natural Assets Code, Parking and Sustainable Transport Code, Road and Railway Assets Code with the exception of the following.

Rural Living Zone

- **Clause 11.4.2 A3 in relation to side setback.** Specifically, the existing outbuildings located on Lot 4 are located within 10m of the northern boundary with Lot 3. The proposal must therefore be assessed against the corresponding performance criteria P3 of clause 11.4.2 as follows.

Clause	Performance Criteria	Proposal
11.4.2 P3	<i>“Buildings must be sited to not cause an Unreasonable loss of amenity to adjoining properties, having regard to:</i>	The siting of the existing outbuildings on Lot 4 and the proximity to the boundary with proposed Lot 3 would not cause an unreasonable loss of amenity to adjoining properties for the following reasons:
	<i>(a) the topography of the site;</i>	The location of the outbuildings associated with the existing residence located within the boundaries of proposed Lot 4 forms gently sloping terrain. The established nature of the buildings, non-habitable use and location on level ground will act to minimise visual impacts resulting from the proximity to proposed Lot 3.
	<i>(b) the size, shape and orientation of the site;</i>	Proposed Lot 4 would have a land area of 3.57ha and forms the largest of the lots within the subdivision. The long axis of the lot is oriented east/west with the subdivision resulting in this lot obtaining frontage to South Arm Road and the new subdivision road. The lot has an irregular shape, in part the result of retaining the large dam on the eastern half of the site.
	<i>(c) the setbacks of surrounding buildings;</i>	The setbacks of the existing dwelling and outbuildings located on proposed Lot 4 and 1099 South Arm Road are consistent with or lesser than the resultant setbacks to the boundary with Lot 3 resulting from the proposed subdivision.

	<i>(d) the height, bulk and form of existing and proposed buildings;</i>	The existing outbuildings form small footprint structures with a low height profile and for this reason would not affect the residential amenity associated with the future occupation of Lot 3.
	<i>(e) the character of the development existing on established properties in the area; and</i>	<p>The surrounding area forms an established rural living setting where single dwelling use and development prevails.</p> <p>The siting of the outbuildings on Lot 4 is consistent with the scale and siting of other outbuildings in the area and will contribute to the retention of a rural setting.</p>
	<i>(f) any overshadowing of adjoining properties or public places.”</i>	<p>The location of the outbuildings to the south of the boundary shared with Lot 3, together with the large lot size attributed to Lot 3, will ensure no overshadowing impact because of the proximity of the outbuildings on Lot 4 to Lot 3.</p> <p>Based on the above assessment, the proposal is considered to satisfy the performance criteria and for this reason the standard is met.</p>

Rural Living Zone

- **Clause 11.5.1 A1(a)(i) in relation to subdivision design.** Specifically, because the existing outbuildings on proposed Lot 4 do not meet the setback standard at clause 11.4.2 A2 of the Rural Living Zone because of the subdivision design, the proposal does not meet the acceptable solution. The proposal must therefore be assessed against the corresponding performance criteria P1 of clause 11.5.1 as follows.

Clause	Performance Criteria	Proposal
11.5.1 P1	<i>“Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have sufficient useable area and dimensions suitable for its intended use, having regard to:</i>	As discussed under the assessment above addressing the requirements of clause 11.4.2 A2/P2, it has been determined the proposed design of Lot 3 and Lot 4 would enable the establishment of a residential use and development on Lot 3, which is not unreasonably impacted by way of loss of amenity because of the proximity of the outbuildings located on Lot 4.
	<i>(a) the relevant requirements for development of existing buildings on the lots;</i>	Lot 4 has been designed to encapsulate the existing buildings and established uses on the site including a dwelling and numerous outbuildings. It is reasonable for these buildings to be retained on Lot 4 given the established use and lot size attributed to this lot, which is well more than the minimum lot size requirement of 2ha. Similarly, Lot 3 is of an appropriate size, shape and orientation to accommodate a residential development which is not impacted by the siting of the outbuildings on Lot 4.
	<i>(b) the intended location of buildings on the lots;</i>	As per above.
	<i>(c) the topography of the site;</i>	Both Lots 3 and 4 are gently undulating, meaning future development of these lots would not be constrained by topographical considerations.
	<i>(d) any natural or landscape values;</i>	Both Lots 3 and 4 form cleared grassland and are of an appropriate size, shape and configuration to retain the rural living landscape qualities of the area. The lot size is consistent with that of surrounding properties and will act as a logical rural living infill development.

	(e) <i>adequate provision of private open space; and</i>	Both Lots 3 and 4 are in excess of 2ha and are not constrained by topography, natural or landscape values. For these reasons, both lots will provide ample private open space to service the reasonable needs of the occupants of the lots.
	(f) <i>the pattern of development existing on established properties in the area, and must be no more than 20% smaller than the applicable lot size required by clause 11.5.1 A1.”</i>	The lot size, shape and orientation are consistent with established properties in the area with the majority of lots being marginally in excess of the minimum lot size of 2ha. The proposed subdivision design acts as a reasonable continuation of the established subdivision pattern displayed within surrounding Rural Living zoned land. The lots within the proposed subdivision are each in excess of the 2ha minimum lot size. For the above reasons, the proposal is considered to satisfy the performance criteria and the standard is met in this regard.

Rural Living Zone

- **Clause 11.5.1 A2 in relation to subdivision design.** Specifically, proposed Lot 11 would not meet the minimum frontage requirement of 40m, in that this lot would have a 12.8m frontage to the new subdivision road. The proposal must therefore be assessed against the corresponding performance criteria P2 of clause 11.5.1 as follows.

Clause	Performance Criteria	Proposal
11.5.1 P2	<i>“Each lot, or a lot proposed in a plan of subdivision, must be provided with reasonable vehicular access to a boundary of a lot or building area on the lot, if any, having regard to:</i>	The proposed frontage arrangement for Lot 11 is considered sufficient to service a future residential use for the following reasons:

	<i>(a) the width of frontage proposed, if any;</i>	<p>The lot would be provided with a 12.8m direct frontage to the proposed subdivision road. The preliminary engineering design drawings indicate the site is not constrained by topography or other natural features which would impede the design of an access to service a future residence.</p> <p>The frontage for Lot 11 has been designed to accommodate the construction of a vehicular access capable of meeting the design and construction standards at clause C2.6.1 A1 and C2.6.2 A1 of the Parking and Sustainable Transport Code.</p> <p>The frontage arrangements will also ensure adequate pedestrian access to the lots.</p>
	<i>(b) the number of other lots which have the land subject to the right of carriageway as their sole or principal means of access;</i>	Proposed Lot 11 would be provided with fee simple frontage to the proposed subdivision road and would not be relied upon by any other lot as their sole or principal means of access.
	<i>(c) the topography of the site;</i>	As per discussion under sub-clause (a).
	<i>(d) the functionality and useability of the frontage;</i>	As per discussion under sub-clause (a).
	<i>(e) the ability to manoeuvre vehicles on the site; and</i>	The proposed lot would have a land area of 2ha which is of ample size to accommodate the parking of vehicles on-site and forward egress from the site.
	<i>(f) the pattern of development existing on established properties in the area;</i>	The frontage arrangement is comparable to the frontage dimension provided to the lots located at the cul-de-sac end of Rosevear Lane directly to the west of the proposed subdivision.

		The reduction in frontage to Lot 11 is a direct result of the positioning of this lot at the outer edge of the proposed subdivision road cul-de-sac and forms a reasonable response to a road layout which is limited to a cul-de-sac arrangement.
	<i>And is not less than 3.6m wide.”</i>	Proposed Lot 11 would be provided with a 12.8m frontage to Rosevear Lane, therefore complies. For the above reasons, the frontage arrangement for Lot 11 is considered to satisfy the performance criteria and the standard is met in this regard.

Rural Living Zone

- **Clause 11.5.2 A1 in relation to roads.** Specifically, the proposal includes a new subdivision road therefore is unable to meet the acceptable solution. The proposal must therefore be assessed against the corresponding performance criteria P1 of clause 11.5.2 as follows.

Clause	Performance Criteria	Proposal
11.5.2 P1	<i>“The arrangement and construction of roads within a subdivision must provide an appropriate level of access, connectivity, safety and convenience for vehicles, having regard to:</i>	See below assessment.
	<i>(a) any road network plan adopted by the council;</i>	Council’s development engineers are satisfied that the proposed internal road network would be subject to the inclusion of detailed engineering design conditions, in accordance with Council’s requirements.

		<p>The subdivision is consistent with the road layout shown on the Master Plan in the Sandford Specific Area Plan and provides the desired connection to School Road in the interests of minimising the creation of new intersections to South Arm Road.</p> <p>The road design provides adequate turning provision at the end of each stage. Engineering detail has been provided with the application demonstrating that the road has also been designed to conform to current Council standards, with the permit recommended to be conditioned to reflect such standards.</p> <p>The shape and size of the subject site, the current configuration of School Road, and the lack of through access leaves no alternative to a cul-de-sac termination.</p> <p>The dysfunctions attributed to a cul-de-sac design will be partially improved through the incorporation of proposed public open space at the southern end of the subdivision road providing pedestrian connectivity to South Arm Road.</p>
	(b) <i>the existing and proposed road hierarchy;</i>	<p>The proposed subdivision provides for the required road connectivity as prescribed under the Sandford Specific Area Plan and will complement the existing road hierarchy.</p>

		<p>DSG encourage access to the subdivision from School Road and acknowledge the South Arm Road/School Road junction does not have a dedicated right turn lane and the left turn into School Road is not compliant with road geometry specifications due to drainage structures.</p> <p>It is however, noted that South Arm Road does have widening adjacent to School Road to allow passing of a right turning vehicle in the form of a BAR treatment.</p>
	<i>(c) maximising connectivity with the surrounding road network;</i>	<p>There is no neighbouring or nearby land with subdivision potential that requires future connector road provision as identified in the Sandford Specific Area Plan.</p> <p>While the adjacent property at 100 School Road is provided with limited subdivision potential, the site has a 20+ year supply of resources and is likely to retain its existing use as a quarry for some time. Connectivity to this property is not considered necessary, given any future subdivision of this site would be more appropriately serviced by an extension to School Road.</p> <p>Pedestrian connection from School Road to South Arm Road is proposed and would form part of the Tangara Trail.</p>
	<i>(d) access for pedestrians and cyclists.”</i>	<p>The proposed subdivision layout would facilitate efficient vehicular and pedestrian permeability between the subject site, School Road and South Arm Road.</p>

		<p>The proposed subdivision and road layout incorporates pedestrian, cycling and public transport facilities that will facilitate the efficient and safe movement of pedestrians, cyclists and public transport.</p> <p>The lots within the subdivision will have convenient access to the South Arm Road bus route through the inclusion of a 10m wide pedestrian connection from the southern end of the subdivision to South Arm Road.</p> <p>For the above reasons, the road design is considered to satisfy the performance criteria and the standard is met in this regard.</p>
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Rural Living Zone

- **Clause 11.5.3 A2 in relation to services.** Specifically, the lots within the subdivision are unable to be connected to a reticulated sewerage system in the absence of such infrastructure in Sandford. Accordingly, the acceptable solution is not met, and the proposal must therefore be assessed against the corresponding performance criteria P2 of clause 11.5.3 as follows.

Clause	Performance Criteria	Assessment
11.5.3 P2	<i>“Each lot, or a lot proposed in a plan of subdivision, excluding within Rural Living Zone C or Rural Living Zone D or for Public open space, a riparian or littoral reserve or Utilities must be capable of accommodating an on-site wastewater treatment system adequate for the future Use and development of the land.”</i>	<p>A wastewater plan has been provided with the application demonstrating each lot can accommodate an on-site wastewater treatment system adequate for the future use and development of the land that satisfies relevant Australian Standards.</p> <p>There are no identified issues which would impede the siting of an on-site wastewater system that would limit the siting of building envelopes.</p>

		For the above reasons, the lot layout is considered to satisfy the performance criteria and the standard is met in this regard.
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Sandford Specific Area Plan

The site is located within the Sandford Specific Area Plan (SAP). The SAP over-rides all other parts of the Scheme to the extent of any inconsistency.

- **Clause S11.8.2 A1 in relation to future public open space connections.** Given there is no acceptable solution, the application must be assessed against the associated performance criteria P1 of Clause S11.8.2 as follows.

Clause	Performance Criteria	Proposal
S11.8.2 P1	<i>“Subdivision must provide for public open space lots:</i> <i>(a) connecting the existing public open space lot between Germain Court and School Road to Delphis Drive, generally in accordance with the future trail connection shown in Figure CLA-S11.1; and</i>	The public open space linkages shown in Figure CLA-S11.1 have been completed as a result of previous subdivision approvals and the proposed subdivision is independent of the existing linkages. However, the proposed subdivision seeks to incorporate an additional linkage providing for a pedestrian connection from the new subdivision road to South Arm Road. The new linkage is shown in Figure 1 below. The provision of this linkage will provide convenient access to the Tangara Trail from Cremorne Avenue and will form an alternative to the use of Delphis Drive to access the Tangara Trail which is steep.
	<i>(b) along the western side of School Road, generally in accordance with the future trail connection shown in Figure CLA-S11.1.</i>	As per above discussion.

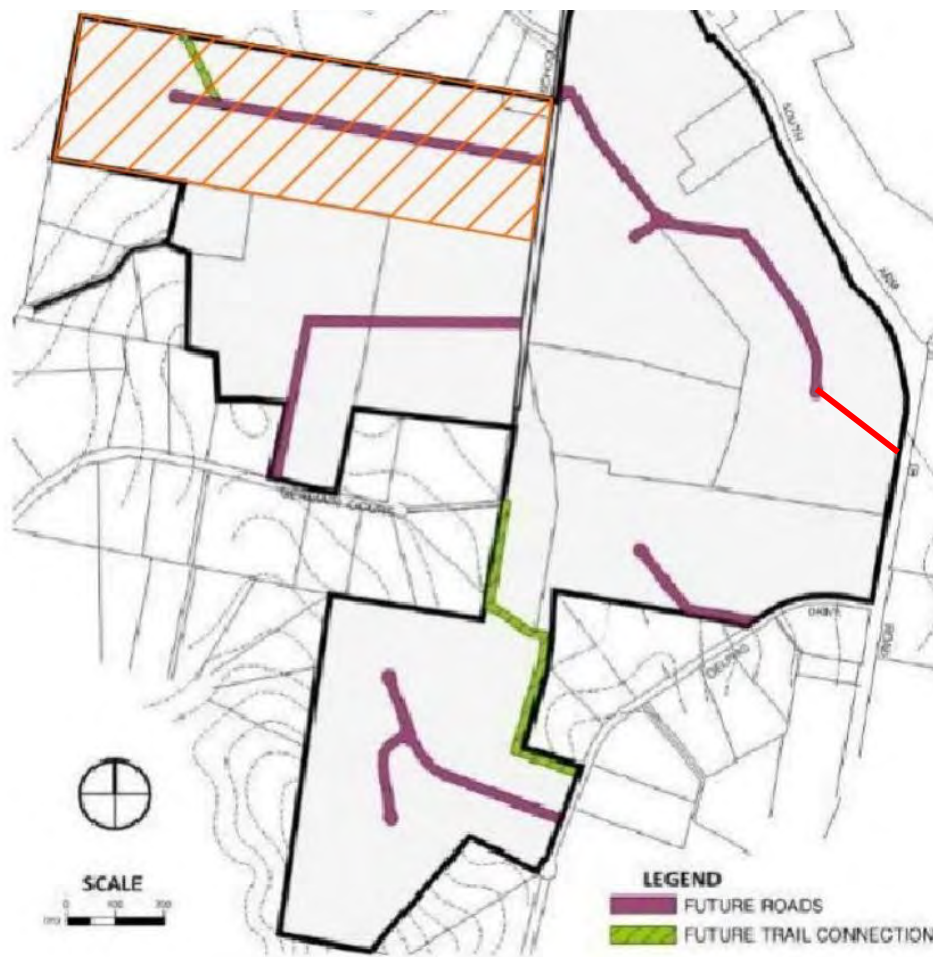


Figure 1: CLA-S11.1 The Road and Trail Layout Plan within the Sandford SAP identifying the approximate proposed public open space linkage from the new subdivision road to South Arm Road in red.

Sandford Specific Area Plan

- **Clause S11.8.2 A2 in relation to future public open space connections.** Given there is no acceptable solution, the application must be assessed against the associated performance criteria P2 of Clause S11.8.2 as follows.

Clause	Performance Criteria	Proposal
S11.8.2 P2	<i>“Additional public open space may be provided, if it does not conflict with the links required in P1 and the Future Road Connections at Figure CLA-S11.1.</i>	<p>As discussed above, clause F11.8.2 P2 of the Sandford SAP allows for additional public open space to be provided, if it does not conflict with the identified future road connections identified within the SAP.</p> <p>Council’s Tracks and Trails Register identifies a 10m wide public open space corridor in the location between Lot 9 and 10 to form a direct and convenient trail connection from the Tangara Trail to Cremorne Avenue.</p> <p>Preliminary negotiations with DSG are underway in relation to a road crossing just south of the Cremorne intersection, as the sight lines are adequate. The location of the Tangara Trail connection to the South Arm Highway is consistent with the proposed pedestrian crossing location.</p> <p>The performance criteria is considered to be met through the provision of the additional public open space linkage.</p>

Road and Railway Assets Code

- **Clause C3.5.1 A1.4 in relation to traffic generation at a vehicle crossing.** The proposed intersection to School Road will increase vehicle movements to and from the site by more than 20% or 40 vehicles per day, as prescribed by the acceptable solution. The proposal must therefore be considered pursuant to the Performance Criteria P1 of Clause C3.5.1 as follows.

Clause	Performance Criteria	Proposal
C3.5.1 A1.4	<i>“Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:</i>	See below assessment.
	<i>(a) any increase in traffic caused by the use;</i>	Council’s development engineer has advised the existing road network has capacity for the increased traffic demand resulting from the proposed subdivision.
	<i>(b) the nature of the traffic generated by the use;</i>	The traffic generated by the future use of the lots would primarily be residential vehicles and associated service vehicles. The road network would be designed for the nature and frequency of movements.
	<i>(c) the nature of the road;</i>	School Road is a local residential road with some heavy traffic due to the local quarry operation. The traffic volume is reasonably small on School Road and would not be compromised by the proposed internal layout.
	<i>(d) the speed limit and traffic flow of the road;</i>	The subdivision would have speed limits applied consistent with those in the area and it is considered that the proposed road network would not compromise flows at this location.
	<i>(e) any alternative access to a road;</i>	Connections would be provided from School Road as required by the Sandford SAP.
	<i>(f) the need for the use;</i>	The proposed junction at the north-western corner of the site to School Road is necessary to facilitate the proposed residential subdivision of the subject lot and connectivity with the surrounding road network. Council’s development engineer supports the proposed development on traffic grounds.

	(g) <i>any traffic impact assessment; and</i>	<p>A Traffic Impact Assessment was not provided with the application as it was not considered necessary by the DSG, or Council's development engineer. This is because the surrounding transport network can cater for the traffic generation associated with the proposed subdivision, with the surrounding transport network having progressively been modified to cater for increased traffic growth. This includes the upgrade to School Road between Kellow Place and the new subdivision road.</p> <p>Further, the proposed intersections have been assessed as compliant with Austroads safe intersecting sight distance requirements.</p>
	(h) <i>any advice received from the rail or road authority."</i>	<p>Council is the road authority in this case and, on the basis that access is restricted to the internal road network to be constructed, is satisfied that the proposed layout would not have an unreasonable impact upon the efficiency of the road network at this location.</p>

Attenuation Code

- **Clause C9.6.1 A1 in relation to lot design within an attenuation area.**

The site adjoins the Sandford quarry (Level 2 Activity) located at 100 School Road. The quarry undertakes extraction and crushing/screening activities and occasional blasting with all lots within the subdivision being within the attenuation distance of 1,000m as prescribed by Table C9.1 of the Code. The subdivision will result in lots intended for a sensitive use within the attenuation area therefore does not meet the acceptable solution. The proposal must therefore be considered pursuant to the Performance Criteria P1 of Clause C9.6.1 as follows.

Clause	Performance Criteria	Proposal
C9.6.1 P1	<i>“Each lot, or a lot proposed in a plan of subdivision, within an attenuation area must not result in the potential for a sensitive use to be impacted by emissions, having regard to:</i>	The quarry at 100 School Road, Sandford is an existing Level 2 extractive activity listed in Table C9.1 of the Scheme. The subject site is fully contained within the 1,000m attenuation distance specified in Table C9.1.
	<i>(a) the nature of the activity with the potential to cause emissions, including:</i> <i>(i) operational characteristics of the activity;</i> <i>(ii) scale and intensity of the activity; and</i> <i>(iii) degree of emissions from the activity; and</i>	<p>The quarry is operating under a Level 2 permit issued by the EPA and includes crushing, grinding and occasional blasting, with the latter requiring consent from the Director of the EPA.</p> <p>The potential emissions include, noise, dust, ground vibrations and shock waves. The quarry’s operating hours conform with the Quarry Code of Practice (QCoP), which are restricted to:</p> <ul style="list-style-type: none"> • 0700 to 1900 hours, Monday to Friday; • 0800 to 1600 hours, Saturdays; and • No operations on Sunday. <p>The Level 2 activity has not explicitly conditioned the activity to comply with noise emissions limits; however, permit condition 6 requires compliance with the QCoP.</p> <p>The quarry lease provides for 20,000m³ of material to be extracted per annum and 1,600m³ of materials to be crushed. The Department of Mines has advised that at this rate of extraction the quarry currently has a 20-year life span.</p> <p>The emissions of noise, dust and vibrations are optimally managed via separation for the source, hence the recommended attenuation distances.</p>

		<p>Although topography and building design can mitigate impacts. Ground vibrations and shock waves are most optimally mitigated via setback.</p> <p>The QCoP establishes an acceptable standard for level of noise being 45dB(a) from 0700 to 1900 hours (daytime).</p> <p>The advice received from the EPA indicates noise emissions set down in the QCoP can reasonably be expected to be complied with, where the separation distances and Standard Recommended Attenuation Distance (SRAD) can be maintained. This means that where a new building is developed within these distances, the noise emission limits are not likely to be complied with or enforceable.</p> <p>In the case of the subdivision, this means it is possible the proposed subdivision and future sensitive use (if approved) may experience noise more than the above emission limits given all lots are within the attenuation area.</p> <p>A Noise Emissions Assessment (Attachment 3) was provided with the application and has been reviewed by the EPA. The EPA has advised the following (As per Attachment 4):</p> <ul style="list-style-type: none">• Table 2 of the Noise Emissions Assessment demonstrates that there are lots namely, Lots 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13 and 14 which will be impacted by the Level 2 Quarry located on School Road, as they will experience noise that exceeds 45dB(A); and
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		<ul style="list-style-type: none">• As such, the EPA recommends considering construction of noise barriers as a mitigation measure to ensure that 45dB(A) limits are met at all lot boundaries of the proposed development. <p>Noise barriers, as deemed required by the EPA to attenuate noise and associated impacts from the quarry, do not form part of the proposal.</p> <p>It therefore remains that the subdivision will cause potential for land use conflict and potentially sterilise portions of the quarry operations and the proposal is not supported in its current form.</p> <p>Considering the above, it is considered that the proposed subdivision and future use will constrain an existing activity listed in Table C9.1 of the Scheme and is not compliant with Performance Criteria P1(a) and (b). On this basis the proposal cannot be supported.</p> <p>Should Council decide to approve the proposal, a covenant ought to be included on the titles requiring future development for sensitive use to be subject to compliance with the noise emissions limits established under Section 7.2.2.2 of the QCoP, as demonstrated by a Noise Emissions Report prepared by a suitably qualified acoustic engineer.</p>
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		<p>Further, if Council decides to approve the proposal, a condition requiring noise barriers to be included in the subdivision design would result in uncertainty about what additional works might occur by way of noise attenuation, to the effect of significantly altering the development beyond that currently proposed (noting noise barriers are not included in the design). This principle is referred to as the Mison principle and forms a relevant test in condition drafting to ensure the fulfilment of a condition imposed will not significantly alter the development in respect of which the application was made for, as it is deemed consent has not been provided for these additional works.</p> <p>Where the Mison principle applies, the result is not that the conditions may be set aside by the appeal tribunal, but that there was never a valid permit. A condition requiring noise barriers would result in a substantial alteration to the proposed development and may invalidate the approval.</p>
	(b) <i>the intended use of the lot.</i> "	<p>As per the above assessment, the future use of the subdivided lots would be subjected to unreasonable emissions associated with the adjacent quarry operations and for this reason the proposal cannot be supported.</p>

5. REPRESENTATION ISSUES

The proposal was advertised in accordance with statutory requirements and eight representations were received. The following issues were raised by the representors.

5.1. Access from School Road

Concern is raised in relation to the currently unsealed section of School Road beyond Kellow Place, which will service the new sealed subdivision road and the impact of additional traffic volumes on the safety and amenity of School Road. Without the necessary upgrades to the currently unsealed section of School Road providing access to the new subdivision road, the concern relates to the increased usage of School Road, as this will create additional wear and tear. Concern is also raised in relation to the use of School Road by heavy vehicles associated with the operation of the Sandford quarry and increased usage of the unsealed section of School Road will reduce traffic safety. The representor has suggested the frontage of the subject site to School Road from South Arm Road to the new subdivision road be sealed.

- **Comment**

The unsealed section of School Road does not currently comply with Council standards. If approved, a condition is recommended requiring the frontage of the lot from Kellow Place to the new subdivision road to be upgraded, or for the payment of a proportional headworks contribution. This approach addresses the concern raised by the representor.

5.2. Continued Operation of the Sandford Fire Station

Significant concern has been raised in relation to the impact of the proposed subdivision on the continued operation of the Sandford Quarry.

- **Comment**

This matter has been addressed in detail above under the “Background” heading.

5.3. Impact on Rural Ambience and Views

The representor has raised concern the subdivision will impact upon the semi-rural outlook and views.

- **Comment**

Under the Tasmanian Planning Scheme – Clarence, the Rural Living Zone applies to this area and the proposed subdivision is consistent with what can be expected under this zone. The Scheme has no specific provisions relating to the retention of views for existing properties adjacent to a proposed subdivision. Therefore, this matter is not of determining weight.

5.4. Impact on Property Values

The representor has raised concern the subdivision will impact upon the semi-rural outlook and views.

- **Comment:**

The impact on property values is not a relevant planning consideration.

5.5. Road Layout

The representor has raised concern the lack of road connection with South Arm Road to reduce vehicle movements through School Road.

- **Comment:**

South Arm Road is a State road with higher traffic volume and higher traffic speed. It is desirable to use the internal roads for servicing a development of this nature and it is considered there is not sufficient demand to require additional connection from the State road. An emergency exit via a laneway and a pedestrian way has been allocated from the proposed turning head, which is considered to be sufficient to service residents within the event of a bushfire.

6. EXTERNAL REFERRALS

The proposal was referred to the EPA who has provided advice to be considered by Council in making a determination on the application (see Attachment 4).

The Tasmania Fire Service has raised minor concerns with the assessment and recommendations made within the bushfire hazard management plan report accompanying the application. The Tasmania Fire Service has requested an amended report and bushfire hazard management plan be prepared for certification by the Tasmania Fire Service. The amendments to the report and associated plan do not result in fundamental changes to the original report and plan. For this reason, procedural fairness would not be compromised through the implementation of the amended bushfire report and plan and a condition requiring an amended bushfire hazard management plan be submitted prior to the sealing of the final plan of survey for Stage 1, is recommended, if approved.

The proposal is not located within a serviced area, therefore a referral to TasWater was not required.

The proposal was referred to TasNetworks which provided advice that it consented to the application, on the basis that certain conditions are met, and measures taken as part of the development to cater for existing infrastructure. Being that this advice is non-statutory and unrelated to Scheme provisions, it is appropriate to include these requirements as advice as part of a planning permit, if granted.

The application was referred to the DSG who advised:

“The Department notes that there are no new proposed accesses to the State road with all new properties accessing South Arm Road via School Road.

It is also noted the development application does not include a Traffic Impact Assessment/Statement.

The South Arm Road/School Road junction does not have a right turn lane and the left turn into School Road is fairly tight due to drainage structure/s. It is however, noted that South Arm Road does have widening adjacent to School Road to allow passing of a right turning vehicle - in the form of a BAR treatment.

It is also noted the speed limit on South Arm Road has recently been reduced from 100km/h to 80km/h.

The Department does not currently have any funds for further improvement of this junction.”

The application was referred to the Department of Natural Resources (DNR) and the Environment (Conservation Assessments) (CAS) who have provided extensive advice relating to the conservation of remnant vegetation containing *Eucalyptus morrisbyi* (endangered flora species) in the north-east corner of the site, in addition to orchids and ephemeral species. CAS have recommended a survey be undertaken at a suitable time to capture these species.

If any listed threatened flora species are identified and will be impacted by the proposed development, a permit to take under the *Threatened Species Protection Act 1995* will be required.

The site is not covered by the Natural Assets Code (Priority Vegetation Area) therefore Council is not provided with a head of power to require a natural values assessment to determine the presence of natural values on the site. It is therefore recommended advice be included in the permit advising the applicant of the DNR requirements.

The site is also within a Swift Parrot Important Breeding Area therefore CAS has recommended any tree, which is 70cm diameter at breast height (DBH) or more be retained, as these trees have the capacity to bear hollows large enough to be suitable nesting habitat. If the avoidance of native vegetation communities is not possible, CAS recommends that clearing of native vegetation communities should be avoided during Swift Parrot breeding season (September to January), if the species is breeding in the area. Given the site is not covered by the Natural Assets Code, it is beyond Council's power to require the retention of suitable nesting habitat. It is therefore recommended advice be included in the permit advising the applicant of the DNR requirements.

An additional threat to Swift Parrots is colliding with man-made objects such as windows and chain-link fences. CAS recommends that infrastructure is designed to minimise collision risks to Swift Parrots.

Given the site is not covered by the Natural Assets Code, it is beyond Council's power to condition for future buildings to be designed to minimise collision risk. It is therefore recommended advice be included in the permit advising the applicant of the DNR requirements.

7. OTHER MATTERS

In accordance with Council's Headworks Levy Policy, a headworks charge for the upgrading of School Road, from the junction of Kellow Place to the proposed new subdivision road, of \$41,026.71 associated with the upgrading of School Road is recommended by way of permit condition, if approved. The headworks charge would be payable prior to the sealing of the final plan for Stage 3.

Alternatively, the developer can opt to carry out the necessary upgrades, including the upgrade of School Road from the junction of Kellow Place to the proposed subdivision road.

Council has consistently applied headworks charges for subdivisions involving access to School Road, as School Road has not been constructed to a suitable standard to absorb the additional traffic generated by subdivision and subsequent development approvals.

The headworks charges are calculated along the principle of Council's adopted Headworks Policy and has been updated as necessary to reflect the cost of upgrade at the time of the subdivision application. The headworks amount applied for this development is also based on the method applied for the original calculation.

8. STATE POLICIES AND ACT OBJECTIVES

8.1. The proposal is consistent with the outcomes of the State Policies, including those of the State Coastal Policy.

8.2. The proposal is inconsistent with the objectives of Schedule 1 of LUPAA as it would create the potential for land use conflict.

9. COUNCIL STRATEGIC PLAN/POLICY IMPLICATIONS

The primary purpose of Council's *Public Open Space Policy (2013)* is to ensure the delivery of adequate and appropriate Public Open Space (POS) to serve the needs of the existing and future population in Clarence. The policy is used to assist Council to exercise its discretion and provide a framework to deliver a consistent approach to the consideration of POS, or alternatively the payment of cash-in-lieu of it.

Irrespective of the underlying zoning, the *Local Government (Building and Miscellaneous Provisions) Act 1993* (LGBMPA) provides for up to 5% of the area of the site to be taken as POS through the subdivision process (greater than this can be required by Council, provided that the landowner is appropriately compensated) or alternatively, 5% of the value of the site can be required as a cash-in-lieu of POS. Importantly, each subdivision proposal must be assessed on its merits reflecting the likely demand on existing (or future) POS related facilities. On this basis, it is not appropriate to apply the maximum 5% contribution indiscriminately across the board without considering actual POS demand generated/facilitated by the proposal. To do so would leave developments vulnerable to being unfairly taxed in terms of demand for POS.

This being so, it is important to recognise that demand for POS exists at both the local and regional level and that any subdivision proposal that generates a demand for, or benefit from, POS (in all its forms) ought to contribute towards it. Given that an area of POS can function at both the regional and local level it is difficult to quantify the ratio between the two. However, a review of Council's POS expenditure indicates that in Clarence approximately 80% of all funds allocated towards POS (both land and capital improvements) is attributed to POS that provides a significant regional function.

In this case, the land is not well serviced by any existing local POS and the proposed lots are of sufficient size to service the likely future population. For this reason, it is considered that each of the proposed lots ought only contribute to the regional component which would equate to 4% (80% of 5% - see above) of the value of the land.

The proposal provides opportunity to secure POS identified as being required in Council's *Tracks and Trails Strategy 2012* and *Tracks and Trails Action Plan*. Specifically, the trail identified across the site includes a 10m wide linkage from the subdivision road connecting School Road and South Arm Road and would form part of the Tangara Trail. The POS shown on the subdivision plan is reflective of this requirement and would further Council's *Public Open Space Strategy 2012* and Council's *Tracks and Trails Action Plan* enhancing Council's POS network and associated facilities.

The area of land proposed as public open space represents an area of 1% of the land area of 1089 South Arm Road. As Council has formed the opinion that the subdivision will or is likely to increase the demand for public open space at a regional level and the land contribution is less than 4%, it has been determined that payment of a cash contribution (“the Contribution”) in-lieu of public open space equal to 3% of the value of the area of land in the approved plan described as Lots 1-3 and 5-14 is required in accordance with the provisions of Section 117 of the LGBMPA.

Should the application be approved, a condition is recommended requiring the POS to be transferred to Council prior to the sealing of the Final Plan of Survey for Stage 4 and for a cash contribution.

10. CONCLUSION

The proposal is recommended for refusal because the proposed development does not comply with clause C9.6.1 P1(a) and (b) of the Scheme for reasons detailed within this report.

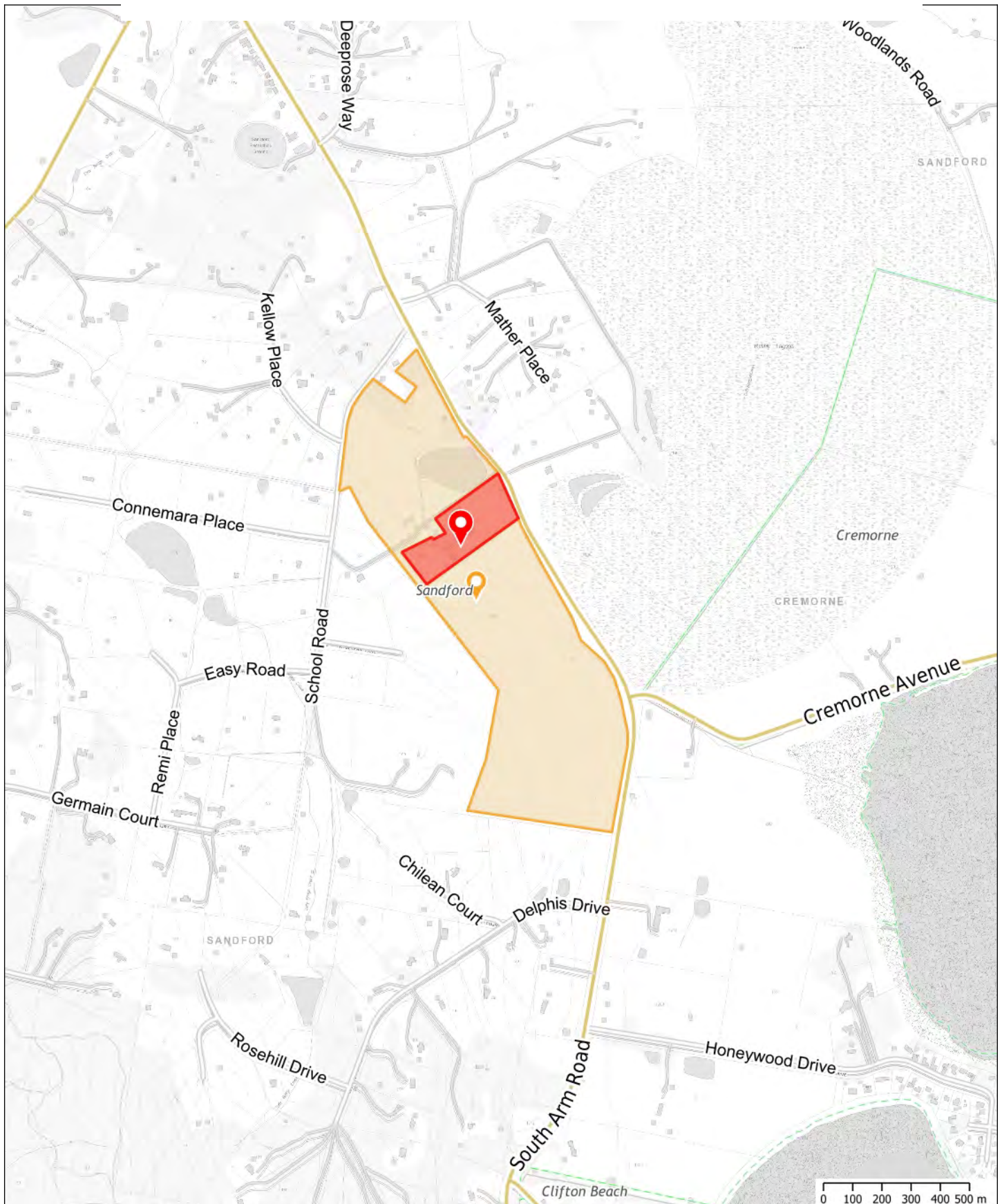
It is important to note approving the subdivision in its current form will result in uncertainty for future lot owners in terms of the costs and ability to achieve compliance with the relevant noise emission levels that will need to be considered as part of the assessment of a future planning application for sensitive use on the lots. Alternate options for the applicant include revisiting the layout and design of the subdivision to include the required noise barriers or to exclude parts of the site from the subdivision that are unable to meet the specified noise emission limits.

Attachments: 1. Location Plan (1)
2. Proposal Plan (1)
3. Noise Emissions Assessment (10)
4. EPA Correspondence (4)
5. Site Photos (3)

Robyn Olsen
ACTING HEAD OF CITY PLANNING

Council now concludes its deliberations as a Planning Authority under the Land Use Planning and Approvals Act, 1993.

Location Plan



This map has been produced by Clarence City Council using data from a range of agencies. The City bears no responsibility for the accuracy of this information and accepts no liability for its use by other parties.

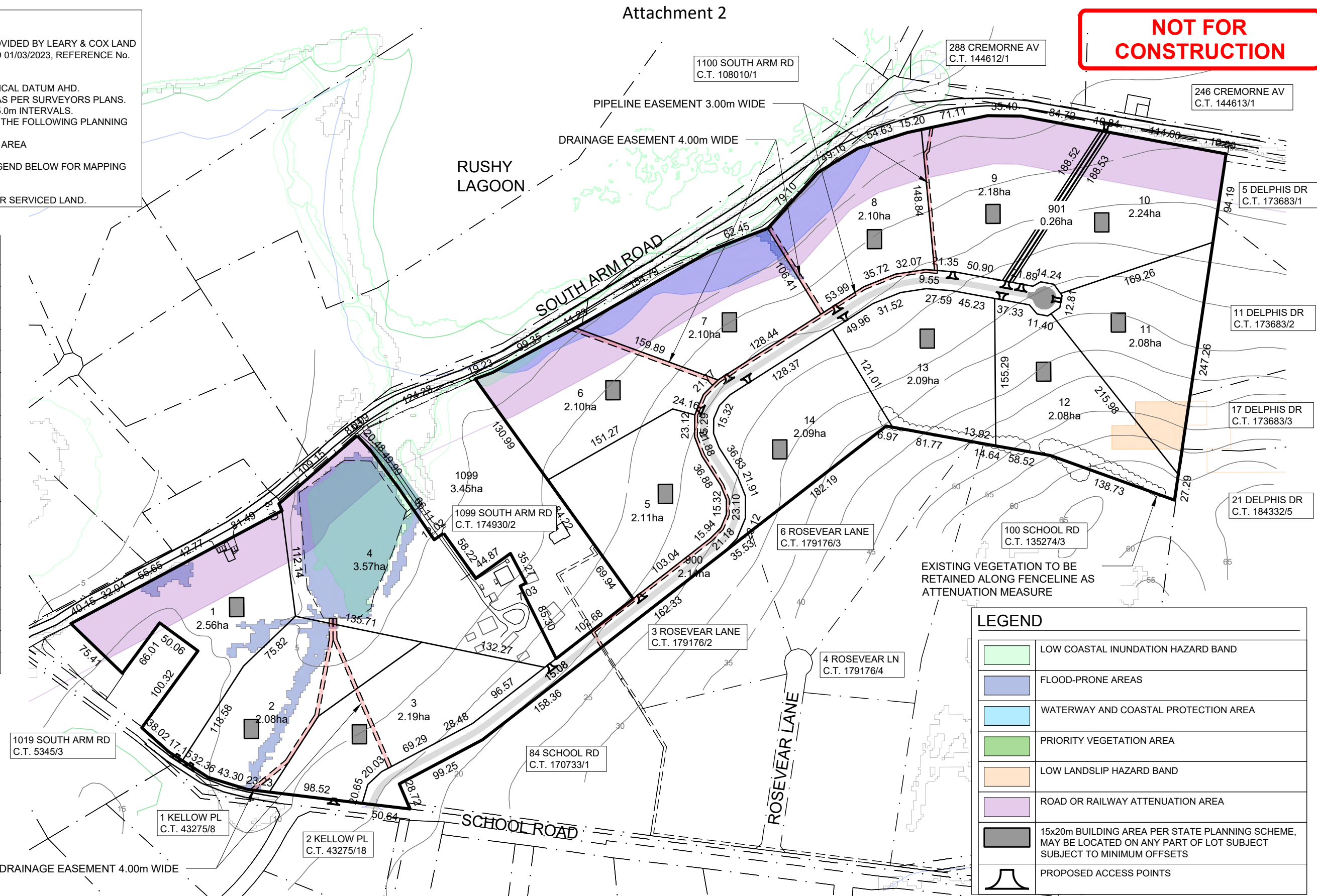
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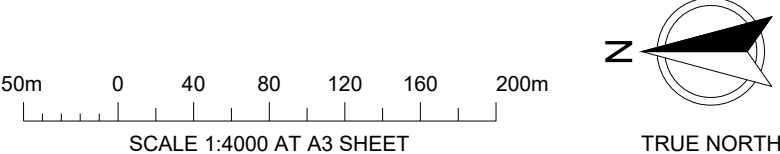


- NOTES
1. SURVEY DATA COMPLETED AND PROVIDED BY LEARY & COX LAND & ENGINEERING SURVEYORS, DATED 01/03/2023, REFERENCE No. 11664.
 2. SURVEY DATA CONTOURS AT 0.5m.
 3. HORIZONTAL DATUM GDA2020, VERTICAL DATUM AHD.
 4. TITLE BOUNDARY AND EASEMENTS AS PER SURVEYORS PLANS.
 5. SITE CONTOURS FROM LISTMAP AT 5.0m INTERVALS.
 6. ENTIRE SUBDIVISION IS SUBJECT TO THE FOLLOWING PLANNING OVERLAYS:
 - AIRPORT OBSTACLE LIMITATION AREA
 - BUSHFIRE-PRONE AREAS
 5. REFER TO THE PLAN AND HATCH LEGEND BELOW FOR MAPPING OF PLANNING OVERLAYS.
 6. LOT IS ZONED RURAL LIVING.
 7. LOT IS NOT WITHIN WATER OR SEWER SERVICED LAND.

LOT AREAS	
LOT No.	AREA (ha)
1	2.560
2	2.077
3	2.189
4	3.574
5	2.106
6	2.099
7	2.100
8	2.100
9	2.175
10	2.244
11	2.083
12	2.083
13	2.088
14	2.088
900	2.143
901	0.189
1099	3.446
TOTAL	37.344



PROPOSED BOUNDARY PLAN
SCALE: 1:4000



REV	DESCRIPTION	BY	CHK	DATE
E	FOR PLANNING APPROVAL - RFI AMENDMENTS	DG	MH	12/07/23
D	FOR PLANNING APPROVAL - BOUNDARY CHANGES	DG	MH	29/06/23
C	FOR PLANNING APPROVAL - RFI AMENDMENTS	DG	MH	22/06/23
B	FOR PLANNING APPROVAL	DG	MH	29/05/23
A	FOR INFORMATION	DG	MH	29/03/23



CHECKED
M. HORSHAM CC5865 I

CIVIL ENGINEER/DESIGNER
D.GRANNETIA

STATUS

SCALE
AS SHOWN

HYDRAULIC ENGINEER
R. HORNER

SIZE
A3

PROJECT
PROPOSED SUBDIVISION
1089 & 1099 SOUTH ARM ROAD
SANDFORD

DRAWING TITLE
PROPOSED BOUNDARY PLAN SH.1

PROJECT NO
23E99-01

DWG NO
C02A

REV
E



**1089 South Arm Road
Subdivision**

Emissions Assessment

Prepared for
Estate of the Late Mrs DG Morrisby

Client representative
Kerry Morrisby



Date
21 August 2023

Rev 00



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Prepared by — Alexander Seen		Date — 12/08/2023
Reviewed by — Douglas Ford		Date — 12/08/2023
Authorised by — Andy Turner		Date — 12/08/2023

Revision History

Rev No.	Description	Prepared by	Reviewed by	Authorised by	Date
0	Issued	A. Seen	D. Ford	A. Turner	12/08/2023

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Executive Summary

This emissions assessment has been prepared to support a development application for a 14 lot residential subdivision at 1089 South Arm Road, Sandford. The assessment is required due to the majority of the site falling within the 750 metre attenuation zone, specified by the *Tasmanian Planning Scheme – Clarence*, of a sand and gravel quarry at 100 School Road, Sandford, which adjoins part of the southern boundary of the proposed subdivision.

The quarry is currently operated by Jenkins Hire, who undertake operations from 7:30am to 4:00pm Monday to Friday year-round, using a jaw crusher, vibratory screen, excavator and bulldozer. Blasting is not undertaken at the site.

Predicted noise emissions were modelled using SoundPLAN environmental noise modelling. The level of the predicted noise emissions from the quarry at the proposed subdivision meets the Tasmanian EPP (Noise) guideline indicator level for avoiding daytime outdoor “annoyance” at the location of the proposed building areas within the subdivision.

The level of dust emissions from the quarry is sufficiently low, that it is not expected to create an environmental nuisance at the nearest residence. The quarry is required to carry out dust control measures to minimise dust emissions, under its environmental permit.

Ground vibration from the quarry is unlikely to be detectable at the nearest residence.

On this basis it may be concluded that potential sensitive uses of the proposed subdivision will not suffer from environmental harm or a loss of amenity due to noise and dust emissions or ground vibration from the quarry, and therefore the performance criteria of Clause C9.6 of the *Tasmanian Planning Scheme – Clarence* is met.

1. Introduction

This emissions assessment has been prepared to support a development application for a 14 lot residential subdivision at 1089 South Arm Road, Sandford. The assessment is required as the property falls within the attenuation zone of a sand and gravel quarry located at 100 School Road, Sandford, adjoining the southern boundary of the subdivision site. The quarry is adjacent to two other recently approved subdivisions to the north and the south.

The quarry is currently operated by Jenkins Hire, who undertake operations from 7:30am to 4:00pm Monday to Friday year-round, using a jaw crusher, vibratory screen, excavator and bulldozer. Blasting is not undertaken at the site.

The property extends approximately 1.2km south of the South Arm Road and School Road intersection, 3.5km south of Lauderdale. The surrounding area is zoned as a mixture of "Rural" and "Rural Living" under the *Tasmanian Planning Scheme – Clarence*. The locations of the proposed subdivision and quarry are shown in Figure 1 below. The proposed layout of the subdivision is shown in Figure 2 below.

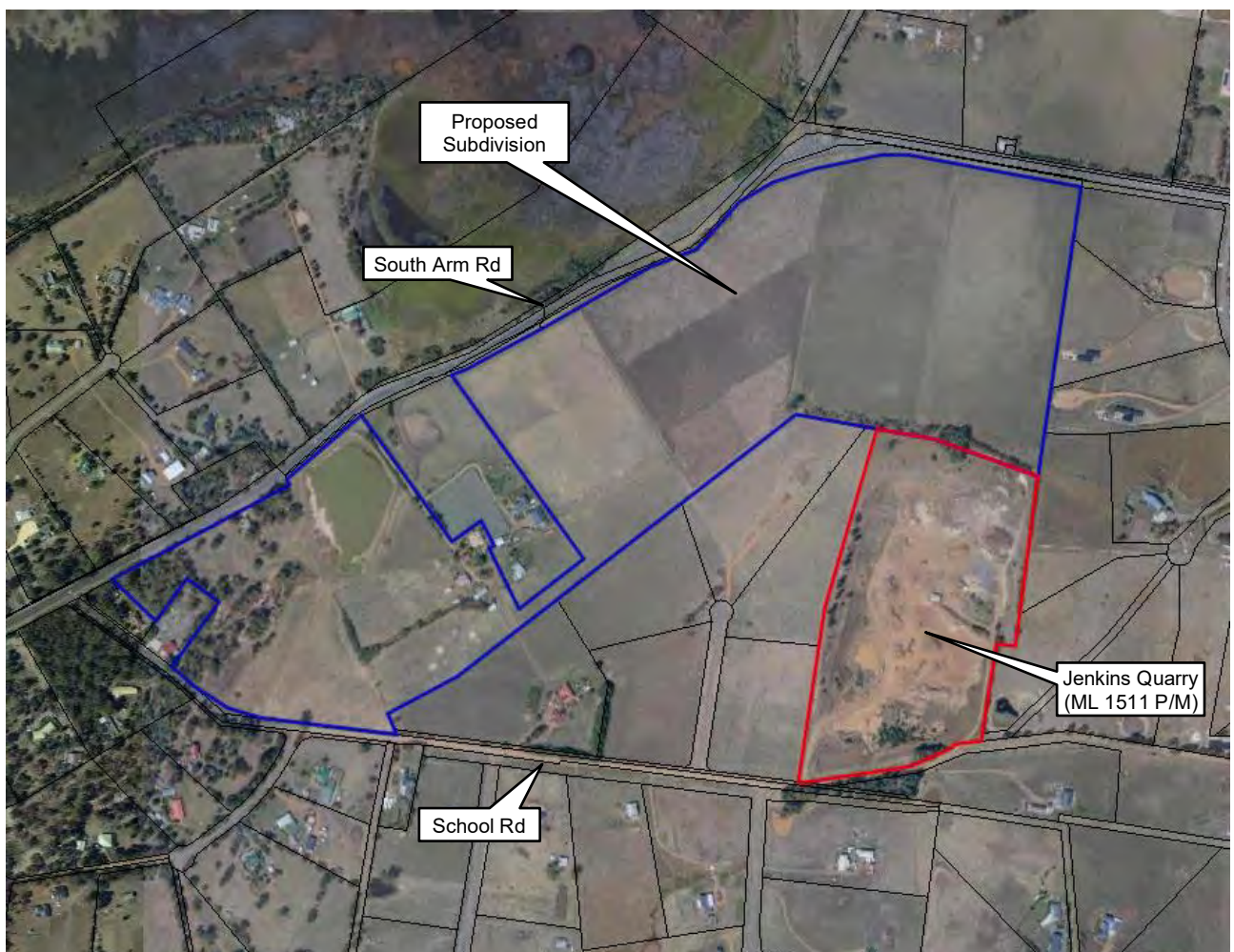


Figure 1: Proposed subdivision location (Basemap source: theList)

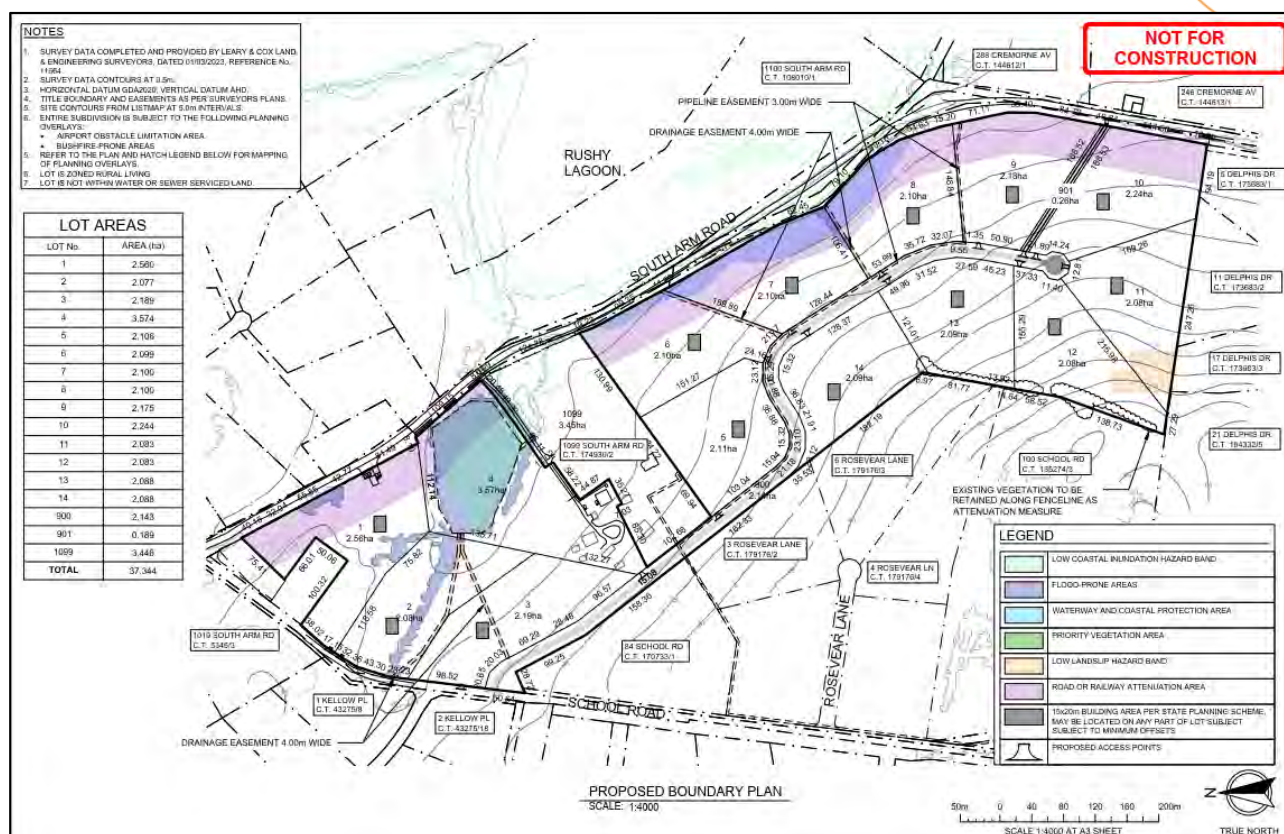


Figure 2 - Proposed subdivision layout. Extracted from project drawing set.

2. Planning Scheme Criteria

The *Tasmanian Planning Scheme – Clarence*, Table C9.1 defines an attenuation zone of 750 metres for a quarry or extractive pit where crushing or grinding are undertaken, the nearby quarry uses a vibratory screen and jaw crusher, but no blasting is undertaken.

Clause C9.6 provides development standards for subdivisions intended for sensitive use, within attenuation zones. It is reproduced below:

Objective: To provide for subdivision so that a lot intended for a sensitive use: <ol style="list-style-type: none"> is located to avoid an activity with potential to cause emissions and enable appropriate levels of amenity; and does not conflict with, interfere with, or constrain an existing activity with potential to cause emissions. 	
Acceptable Solutions	Performance Criteria
A1 Each lot, or a lot proposed in a plan of subdivision, within an attenuation area must: <ol style="list-style-type: none"> be for the creation of separate lots for existing buildings; be for the creation of a lot where a building for a sensitive use can be located entirely outside the attenuation area; or not be for the creation of a lot intended for a sensitive use. 	P1 Each lot, or a lot proposed in a plan of subdivision, within an attenuation area must not result in the potential for a sensitive use to be impacted by emissions, having regard to: <ol style="list-style-type: none"> the nature of the activity with the potential to cause emissions, including: <ol style="list-style-type: none"> operational characteristics of the activity; scale and intensity of the activity; and degree of emissions from the activity; and the intended use of the lot.

As the proposed lots are located within the attenuation area of the quarry and are intended for sensitive use (residences), the proposed subdivision cannot meet the acceptable solution and the performance criteria must be addressed.

3. Noise Assessment

3.1 Noise Criteria

The Tasmanian *Environmental Protection Policy (Noise) 2009 (EPP)* provides a table of guideline noise levels which may be used to evaluate the impact of noise on sensitive uses, such as residential dwellings.

Two measures are provided for avoiding “Moderate Annoyance” and “Serious Annoyance” for people engaged in for “outdoor daytime living” activities in their yards, of L_{eq} equals 50dB(A) and 55dB(A) respectively. L_{eq} is the “equivalent continuous noise level” which can be thought of as the average noise level over a specific period of time¹. L_{max} is the maximum noise level recorded in a specific period of time. These measures relate to the combined total noise level experienced at a location, which is made up of noise from the activity being considered as well as noise from all other sources in the area, such as traffic and other commercial premises, etc.

The quarry only operates during daytime hours. Noise monitoring was not undertaken as part of this assessment. A daytime ambient noise level (excluding noise from the quarry) of 40dB(A), which is typical for rural areas with modest levels of traffic has been assumed.

3.2 Noise Sources

Typical equipment utilised at the quarry include a crusher, screen, excavator (nominally 20 tonne) and a bulldozer. Table 1 details the sound power levels, used to characterise the noise sources, for the equipment used at the quarry as advised by the quarry operator. Sound power levels are based on noise measurements of equivalent equipment made by pitt&sherry or referenced from other noise assessments of quarrying equipment.

All pitt&sherry noise measurements are made using a Rion NL-42 sound pressure meter, mounted on a tripod. All measurements were made in accordance with the Tasmanian DEPHA *Noise Measurement Procedures Manual*, 2008.

Table 1: Equipment sound power levels.

Noise Source	Description	Quantity	Height Above Ground – m	Sound Power Level – dB(A)
Crusher	Terex Finlay J1175 Jaw Crusher	1	2	119.6
Screen	Fintex 570 Screen	1	2	109.0
Excavator	CAT 375 Excavator	1	2	107.7
Bulldozer	CAT D8 ²	1	2	110.0

3.3 Methodology and Assumptions

Noise modelling was carried out in accordance with the Tasmanian DEPHA *Noise Measurement Procedures Manual*, 2008. Noise level calculations were implemented using SoundPLAN 8.2 environmental noise modelling software. Modelling assumptions and settings include:

- The ISO 9613-2 noise calculation standard was used within SoundPLAN. This standard incorporates “worst case” meteorological conditions for noise propagation.
- Terrain topography was obtained from 2 metre elevation data sourced from the ELVIS online elevation database. No modifications to the digital ground model have been made to account for further excavation at the quarry site or stockpiles.
- Buildings were included based on aerial imagery of the area from February 2023.

¹ Noise levels measured in decibels are averaged logarithmically.

² Hegley Acoustics Consultants, McPherson Quarry Mangatawhiri Assessment of Noise Effects, Report No. 18185, October 2018.

- A ground absorption factor was set to 70% soft for the entire modelling area, reflecting the existing environment.
- The excavator, bulldozer, screen and crusher have been modelled as single point sources, located 2 metres above ground level.

Figure 3, below shows the layout of the SoundPLAN noise model, including the location of the quarrying equipment.

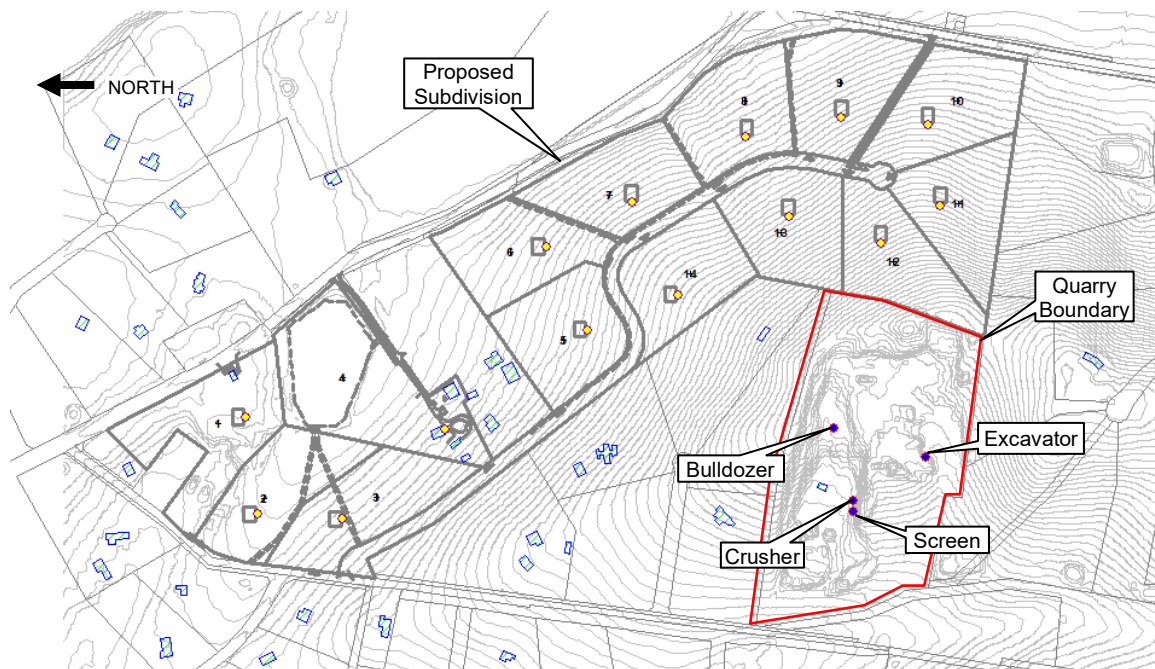


Figure 3: SoundPLAN Noise Model including the locations of quarrying equipment.

3.4 Intrusive or Dominant Noise Characteristics

Various characteristics of noise can increase the level of annoyance that is caused. These include Tonality, Impulsiveness, Modulation and Low Frequency content. Tonality is where particular frequency bands or “Tones” are present within the noise, such as the “whine” of a circular saw. Impulsiveness is where noise has rapid large changes in amplitude such as gunshots or jackhammers. Modulation is where the noise level cycles up and down rapidly. Low frequency noise is considered a problem when there is significant energy in the 20 Hz to 250 Hz frequency range.

Quarrying equipment typically generates moderate levels of tonal or low frequency noise, these effects are attenuated by distance and partially masked when combined with traffic noise and environmental noise, including from wind, trees and birds. Intrusive noise characteristics are taken into account in the noise assessment by adding ‘penalty’ corrections to the predicted noise level results, in accordance with the *Noise Measurement Procedures Manual*.

3.5 Noise Modelling Results

The predicted levels of noise emissions from the quarry, at the proposed building areas within the subdivision, are given in Column 2, of Table 2 below. The closest lots to the quarry incur a penalty for tonal noise of +2.5 to +4dB(A) and a penalty for low frequency noise of +5dB(A). These penalties are included in the Table 2 results.

Column 3 of Table 2 shows the predicted ambient noise level, calculated by logarithmically adding the ambient noise level in the area³ (without the quarry) to the predicted quarry noise. The combined noise level results meet the 50 dB(A) EPP (Noise) guideline level for avoiding “annoyance” in outdoor living areas.

³ The daytime ambient noise level in the area, without the quarry has conservatively been assumed to be 40 dB(A), as on-site noise logging was not undertaken for this assessment.

Table 2 Modelled noise levels at proposed building areas in the subdivision.

Lot	Predicted Quarry Noise Level dB(A)	Predicted Combined Quarry & Daytime Ambient Noise Level dB(A)
Lot 01	47.1	47.8
Lot 02	47.7	48.4
Lot 03	48.1	48.7
Lot 04	43.5	45.1
Lot 05	46.8	47.6
Lot 06	45.8	46.8
Lot 07	46.4	47.3
Lot 08	47.7	48.4
Lot 09	45.5	46.6
Lot 10	44.8	46.1
Lot 11	45.1	46.3
Lot 12	48.7	49.2
Lot 13	49.0	49.6
Lot 14	48.7	49.3

A noise level contour map showing the distribution of quarry noise in the area, is shown in Figure 4 below.

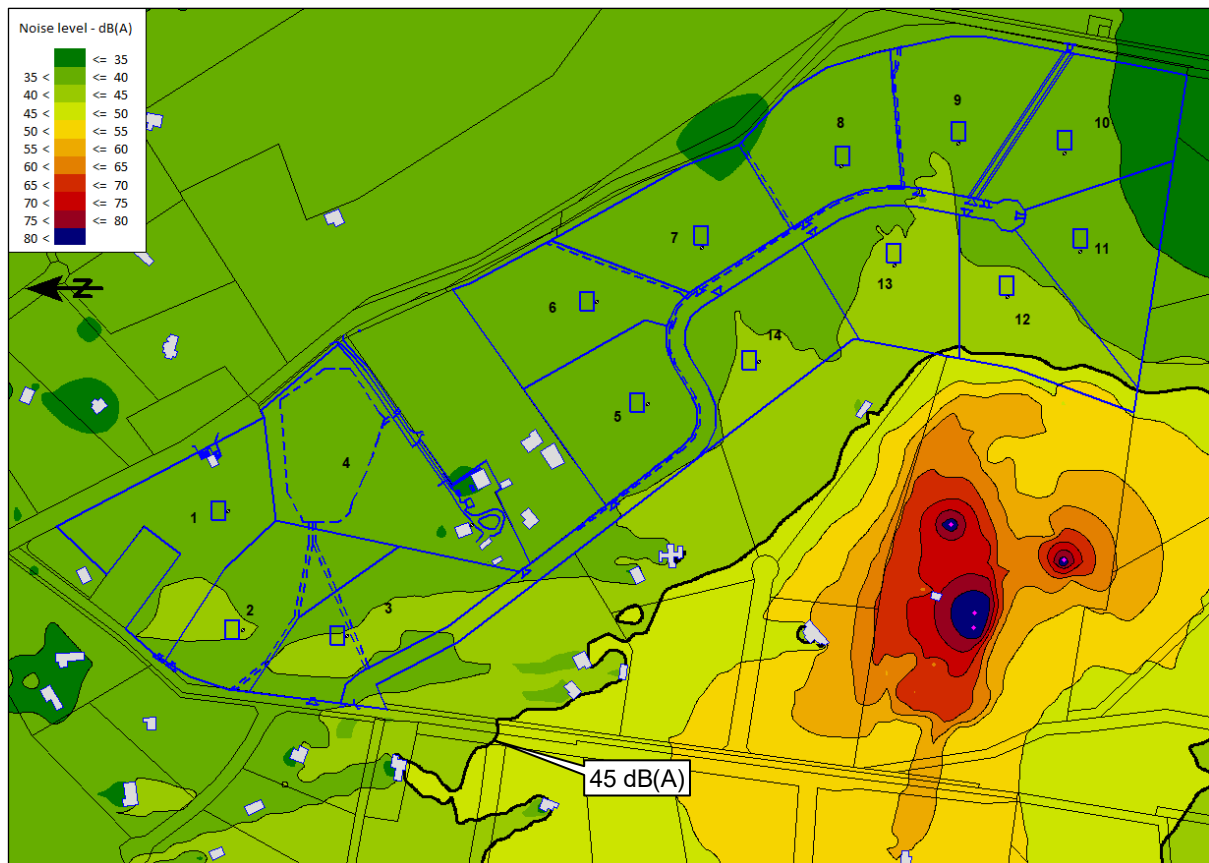


Figure 4 – Quarry noise level contour map - 45dB(A) contour shown in bold, and proposed subdivision shown in blue.



4. Dust Emissions

Quarrying equipment and processes have the potential to generate significant dust emissions, especially during extended periods of dry weather. The level of potential dust nuisance reaching the nearest building areas in the proposed subdivision is minimised by the distance to the working areas of the quarry, intervening trees and vegetation along the northern boundary of the quarry, and the topography of the quarrying site and surrounding area. In addition the quarry is obliged to carry out a number of dust control measures that are specifically listed in its environmental permit⁴. These include including the covering or dampening of vehicle loads that might blow or spill, and the use of water sprays, dust extraction systems, and/or dust enclosures to control dust emissions from crushing, screening and conveying plant.

As a result, dust generated from quarrying activities is unlikely to reach levels that will cause a nuisance at the location of nearest houses within the proposed subdivision.

5. Ground Vibration

Blasting is not undertaken at the Quarry. Quarrying equipment such as screening and crushing plant can generate modest levels of ground vibration, but this is rapidly attenuated as it travels through the ground. At the distance of the nearest houses in the proposed subdivision ground vibration will be reduced to undetectable levels.

6. Conclusions

The predicted noise emissions from the quarry are lower than the Tasmanian EPP (Noise) guideline level for avoiding daytime outdoor “annoyance” at the nearest residences in the proposed residential subdivision. Operations are not undertaken at night.

The level of dust emissions from the quarry is sufficiently low, that it is not expected to create an environmental nuisance at the nearest residence. The quarry is required to carry out dust control measures to minimise dust emissions, under its environmental permit.

Ground vibration from the quarry is unlikely to be detectable at the nearest residence.

On this basis it may be concluded that potential sensitive uses of the proposed subdivision will not suffer from environmental harm or a loss of amenity due to noise and dust emissions or ground vibration from the quarry, and therefore the performance criteria of Clause C9.6 of the *Tasmanian Planning Scheme – Clarence* is met.

⁴ EPN 8990/1 – 20 May 1993



1089 South Arm Road Subdivision

Emissions Assessment

**Pitt & Sherry
(Operations) Pty Ltd**
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Our Ref: [D23-195197/007](#)



24 July 2023

Vanessa Tomlin
Manager City Planning
Clarence City Council
38 Bligh St / PO Box 96
ROSNY PARK TAS 7018

Email: cityplanning@ccc.tas.gov.au

Dear Vanessa Tomlin

PDPLANPMTD-2023/036283 – SUBDIVISION OF 1089 & 1099 SOUTH ARM RD, SANDFORD NEAR QUARRY – EPA COMMENT

I acknowledge receipt of Clarence City Council's email dated 5 July 2023, requesting comment from the Director of the Environment Protection Authority (EPA) about a planning application for the subdivision of 1089 & 1099 South Arm Rd, Sandford. I also acknowledge receipt of the following documents in relation to the subdivision:

- Correspondence from Clarence City Council to EPA date 5 July 2023;
- Completed form titled *Clarence City Council, Application for Development / Use or Subdivision*;
- Site and Folio Plans; and
- Correspondence from JSA Science in Design to Clarence City Council dated 3 July 2023.

Jenkins Hire Pty Ltd (Jenkins Hire) is currently authorised to operate a quarry on Mining Lease (ML) 1511P/M located at Sandford. The quarry undertakes extraction and crushing/screening activities and the current operations, which are carried out within the ML have a separation distance of approximately 150 – 250 metres from the ML boundary to Lots 11, 12, 13 and 14, and with all Lots being within approximately 750 metres from the ML boundary.

As per Section 6.1.2 of the Quarry Code of Practice (QCoP) of May 2017 published by the EPA:

It is suggested that the planning authorities and operators seek to maintain the following separation distances, measured from the maximum extent of the quarry operations to any sensitive use:

- 1. where regular blasting takes place 1,000 metres;*
- 2. where material is crushed only 750 metres;*
- 3. where vibrating and trommel screens alone are utilised 500 metres;*
- 4. where no blasting, crushing or screening occurs 300 metres.*

The approval authority may consider variations of the above distances where the nature or manner of the operation can justify this. Modelling noise from proposed quarrying and processing activities and the measurement of pre-existing noise levels can be used to support such variations. Ground vibration and air blast overpressure should also be modelled where blasting is expected within 1,000 metres of a sensitive use.

The abovementioned separation distances referenced in the QCoP are reflected in the State Planning Provisions, Code 9.0 – Attenuation Code.

In the case of the abovementioned quarry in relation to blasting, the current Level 2 permit states blasting must not be carried out on *The Land* without the prior written approval of the Director.

Furthermore, the current Level 2 permit also limits operating hours, whereby activities associated with the extraction of rock, gravel, sand, clay or minerals, and loading of product, and screening/crushing must not be undertaken outside the hours of 0700 hours to 1900 hours on weekdays and 0800 hours to 1600 hours on Saturdays.

Please note, that whilst the Level 2 permit has not explicitly conditioned the activity to comply with noise emission limits, condition G6 requires the activity undertaken on *The Land* to comply with the Acceptable Standards provisions of the *Quarry Code of Practice*.

As per Section 7.2.2.2 of the QCoP, an acceptable standard for level of noise (i.e. noise emission limits) has been established.

Noise from quarrying activities and associated activities, including equipment maintenance, when measured at any neighbouring sensitive use must not exceed the greater of:

- *The A-weighted 10 minute L₉₀, excluding noise from the quarry, plus 5 dB(A), or*
- *The following levels:*
 - *45 dB(A) from 0700 to 1900 hours (daytime)*
 - *40 dB(A) from 1900 to 2200 hours (evening), and*
 - *35 dB(A) from 2200 to 0700 hours the following day (night time)*

when measured as a 10 minute L_{eq}.

Section 7.2.2.3 of the QCoP referenced separation distances from quarry activities.

The distances provided in section 6.1.2 are generally consistent with avoiding unreasonable noise impacts.

The EPA is of the belief that the noise emissions limits set down in section 7.2.2.2 of the QCoP can reasonably be expected to be complied with, where the separation distances and Standard Recommended Attenuation Distance (SRAD) to sensitive receptors can be maintained. This means that where a new premises is built within these distances, the noise emission limits are not likely to be complied with or enforceable.

Based on the above, the EPA advises that under the current permit conditions, it is possible that the proposed subdivision at 1089 & 1099 South Arm Rd, Sandford, and future proposed development (if approved) may experience noise in excess of the above emission limits given that all the Lots are within approximately 750 metres of *The Land* on which the activity may be carried out. Due to the proximity to the existing quarry, the proposed subdivision development has the potential to be affected by quarry associated activities.

To minimise the potential for the proposed subdivision at 1089 & 1099 South Arm Rd, Sandford, and future proposed development to interfere with or constrain the operation of the quarry, the EPA advises that:

- It is recommended to undertake a Noise Impact Assessment Report assessing impact of quarry operation scenarios including but not limited to; the cumulative impact of drilling, blasting and quarrying activities (ripping, crushing, screening, loading product etc). The report should clearly discuss potential noise nuisance that may be expected to be experienced at each Lot and, should clearly state the expected nuisance from the worst-case scenario for each Lot;
- The Noise Assessment Impact Assessment Report should identify appropriate mitigation measures (i.e., noise barriers, external walls, glazing and ceiling/roof facades and construction details) to meet the required indoor levels of each habitable space;
- The Noise Assessment Impact Report should also include appropriate mitigation measures (i.e., noise barriers) to protect the outdoor recreation areas;
- If a permit is granted for the subdivision appropriate conditions should be applied, to ensure that the proposed develop and any future proposed development is designed and constructed appropriately to attenuate noise and associated impacts from the quarry;

- Future residential owners of the subdivision should be informed about potential noise nuisance that they may experience, and the associated planning application documents should clearly state the expected nuisance from the worst-case scenario.

The EPA also advises that due consideration should be given to local conditions such as terrain and meteorology, including annual rainfall, and the direction and strength of prevailing winds, as these factors have a bearing on the potential for dust related nuisance issues to occur in the vicinity of the proposed subdivision and future proposed development.

Yours sincerely



Dr Martin Read

A/DIRECTOR, ENVIRONMENT PROTECTION AUTHORITY

cc: Amanda Beyer, Clarence City Council: abeyer@ccc.tas.gov.au

From: Alexander, Rachel <Rachel.Alexander@epa.tas.gov.au>
Sent: Tuesday, 26 September 2023 2:28 PM
To: Amanda Beyer
Subject: RE: Subdivision of 1089 and 1099 South Arm Road - Sandford Near Quarry - EPA Comment

This Message Is From an External Sender

This message came from outside your organization.

Afternoon Amanda,

Apologies for the delay and I very much appreciate the extension of time you have provided us.

The EPA Noise Specialist has reviewed the submitted Noise Impact Assessment Report and advises the following:

- Table 2 demonstrates that there are lots namely, Lot 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13 and 14 will be impacted by the Level 2 Quarry located on School Rd as they will experience noise that exceeds 45dB(A).
- As such, the EPA recommends considering construction of Noise barriers as a mitigation measure to ensure that 45dB(A) limits are met at all lot boundaries of the proposed development.

Furthermore, it is noted that the conclusion drawn in the first paragraph of the *Conclusions* section is correct when utilising the Tasmanian EPP (Noise). As such, this appears to be the reason as to why the report has not provided any mitigation measures. However, as the development is located adjacent to a level 2 activity the EPA recommended a noise limit of 45dB(A) in place of 50dB(A).

Secondly, the modelled noise levels depicted in Table 2 demonstrates that all lots will experience noise level beyond 45dB(A) yet, Figure 4 depicts only lots 1, 2, 3, 9, 11, 12, 13 and 14 will experience noise level beyond 45dB(A). This is an inconsistency which is of concern.

Thank you,

Rachel Alexander | Senior Environment Officer
Southern Industrial Regulation | Industrial Regulation Branch
Environmental Regulation Division
Environment Protection Authority
GPO BOX 1550, HOBART, TAS, 7001
T: (03) 6165 4538
E: rachel.alexander@epa.tas.gov.au
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From: Amanda Beyer <abeyer@ccc.tas.gov.au>
Sent: Thursday, September 21, 2023 1:52 PM

SITE PHOTOS – 1089 AND 1099 SOUTH ARM ROAD, SANDFORD



Photo 1: The site when viewed in a westerly direction from the southern end of the property frontage to South Arm Road. The Sandford Quarry is located behind the tree line visible on the hill top. The area contained within proposed Lots 8-13 (cul-de-sac head) is visible in this image.

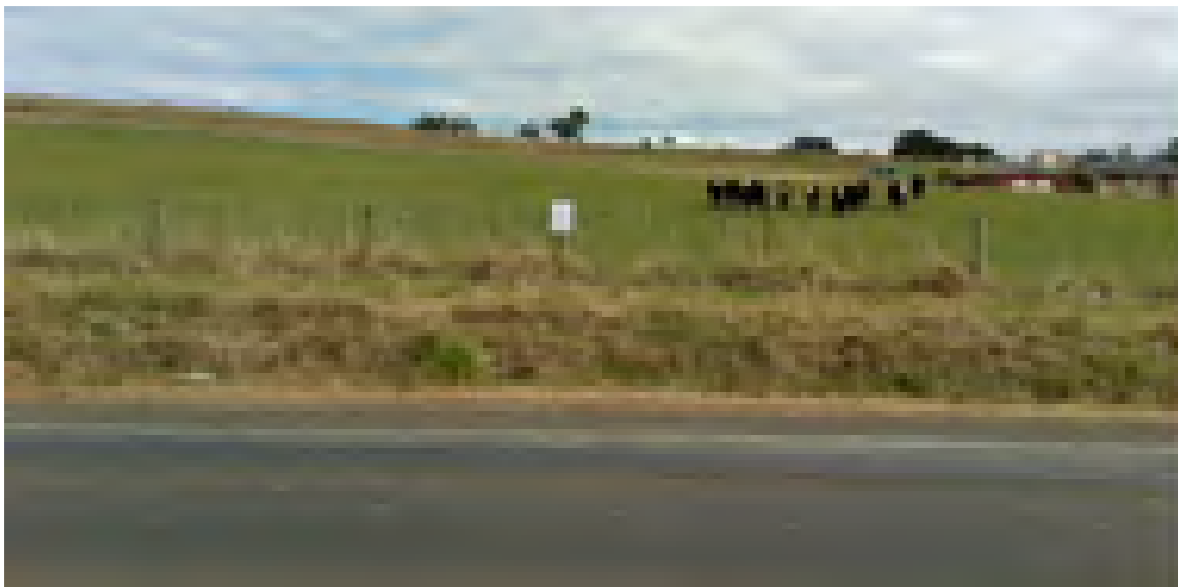


Photo 2. The view towards Rosevear Lane when viewed from South Arm Road. The area visible in this image would be occupied by proposed lots 5, 6, 7 and 14. The existing dwelling at 1099 South Arm Road is visible on the right hand side of the image.



Photo 3: The view of the site from the Kelson Place / School Road junction. The dam on proposed lot 4 is visible in the background. The remnant vegetation on the site is also visible to the left of the dam.



Photo 4: The view over the site when viewed upslope from the entrance to 3 Rosevear Lane. The dwelling located at 1099 South Arm Road is visible to the left of centre.



Photo 5: The site when viewed from the entrance to 6 Rosevear Lane and beyond to the salt marshes located on the eastern side of South Arm Road.

8. REPORTS OF OFFICERS

8.1 DETERMINATION ON PETITIONS TABLED AT PREVIOUS COUNCIL MEETINGS
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Nil Items.

8.2 ASSET MANAGEMENT

Nil Items.

8.3 FINANCIAL MANAGEMENT

Nil Items.

8.4 GOVERNANCE**8.4.1 DOG MANAGEMENT POLICY 2021 – CONSULTATION ON PROPOSED CHANGES TO DECLARED AREAS****EXECUTIVE SUMMARY****PURPOSE**

To seek Council's approval to undertake public notification of its intention to declare two areas under the Dog Management Policy 2021 in accordance with the *Dog Control Act 2000 (Tas.)*.

RELATION TO EXISTING POLICY/PLANS

Council's Dog Management Policy 2021, Council's Strategic Plan 2021 – 2031, Recreation Needs Analysis 2019 and Active Living Strategy 2022 are relevant.

LEGISLATIVE REQUIREMENTS

The *Dog Control Act 2000* is relevant.

CONSULTATION

Preliminary consultation with stakeholders has been conducted. Public notification of Council's intention to declare the areas will be undertaken if the recommendations are adopted. The public notification process will advise the community that submissions may be made to Council which it must consider.

FINANCIAL IMPLICATIONS

Any costs associated with consultation and implementation will be accommodated within existing approved budgets.

RECOMMENDATION:

That Council:

- A. Authorises the Chief Executive Officer to undertake the statutory notification and consultation process in accordance with section 24 of the *Dog Control Act 2000 (Tas.)* to give public notice of Council's intention to declare the following areas:
 - (i) the parts of South Street Reserve:
 - a. shown in purple on Attachment 1 of the Associated Report be declared a restricted area, a condition of use is that greyhounds are the only dogs permitted in the restricted area and may be exercised off-leash and un-muzzled;
 - b. shown in yellow on Attachment 1 of the Associated Report be declared a restricted area, a condition of use is that vulnerable dogs (being dogs which are small, geriatric or experiencing or recovering from an injury or illness) are the only dogs permitted in the restricted area and may be exercised off-leash;

- c. shown in teal on Attachment 1 of the Associated Report be declared an exercise area, a condition of use is that dogs may be exercised off-leash in this area at times when this area is not being used by the Eastern Shore Dog Club in accordance with its lease terms; and
 - d. which are enclosed by fences (being those shown in purple, yellow, teal and green on Attachment 1 of the Associated Report) be declared a training area at the times a permit issued to the Eastern Shore Dog Club authorises them to be used to conduct dog obedience, agility or other training activities by the club.
 - (ii) the area on the land side of the Bellerive Boardwalk perimeter shown in blue on Attachment 1 of the Associated Report is declared a restricted area, a condition of use is that dogs are permitted in this area if they are on a leash, otherwise they are restricted from entering the area off-leash, or if the conditions of an event permit declare dogs are not permitted at the event.
- B. Notes that the results of the public notification and consultation process and any submissions received will be the subject of a future council agenda for Council's consideration.

ASSOCIATED REPORT

1. BACKGROUND

- 1.1.** The report recommends Council approves the undertaking of public notification and community consultation on the following proposed changes to “declared areas” under its Dog Management Policy 2021. The changes are to accommodate the following initiatives, which are proposed in response to community needs and expectations around the management of dogs in Clarence:
- (a) Declaring parts of South Street Reserve in Bellerive as an off-leash greyhound exercise area to accommodate a proposed 12-month trial, another part as an area for vulnerable dogs, and changing the declarations for the balance of the area from a “training” area to an off-lead exercise area; and
 - (b) Changing the restrictions on the area on the land side of the Bellerive Boardwalk to permit dogs to be walked on-leash, unless prohibited by the conditions of a permit issued by Council for the staging of an event.

- 1.2. The proposed changes to the declared areas are shown on the plans in Attachment 1.
- 1.3. Before making a declaration, Council is required to conduct a public notification and community consultation process in accordance with the Act.

2. **REPORT IN DETAIL**

- 2.1. The schedule of declared areas in Council’s *Dog Management Policy 2021* sets out public areas in Clarence which are subject to declarations under the *Dog Control Act 2000* (**the Act**) around the regulation of dogs. The current schedule of declared areas is Attachment 2. The proposed changes to schedule of declared areas seek to strike a balance between greyhound owners, other dog breed owners and non-dog owners’ uses of the relevant areas.

South Street Reserve

Council proposes to conduct a 12-month trial of fencing part of the South Street Reserve at Bellerive for use as an off-leash greyhound exercise area, with an additional off-leash exercise area for vulnerable dogs.

- 2.2. The off-leash greyhound exercise area trial is being implemented in response to requests from the re-homed greyhound owner community for a fenced, off-leash area where they can exercise their dogs. The Dog Management Policy 2021 provides for the construction of a fenced, off-leash greyhound exercise area at 17 Goodwins Road, Clarendon Vale. However, current allocated funding is not sufficient to complete that project, meaning it is likely several years away. The South Street trial will, in the short term, provide a fenced area for greyhounds to exercise off-leash and provide valuable data and information to assist in future planning of greyhound exercise areas.
- 2.3. The vulnerable dog area will provide an area for vulnerable dogs to exercise in public with less exposure to larger or stronger dogs that may pose a risk. It is proposed to adopt the definition of “vulnerable dog” adopted by the City of Launceston at its Heritage Park dog exercise area, which is: “*a dog which is small, geriatric or experiencing or recovering from an injury or illness*”.

- 2.4.** This definition would provide guidance to users of the area but would not be definitive in the case of a prosecution or infringement.
- 2.5.** The other areas of the reserve would remain available as off-leash exercise areas for all other dogs and are not subject to other restrictions under the Act. However, it is proposed to change the declaration for those areas from a full-time “training” area to an “off-leash exercise” area, except when booked for training activities. This is because the areas are only used for formal dog training activities by the Eastern Shore Dog Club for several hours each week, and, in practice, are used by the public as off-leash exercise areas at all other times.

Bellerive Boardwalk

The part of the Bellerive Boardwalk shaded in red on Page 2 of Attachment 1, is currently a declared “restricted” area where dogs are restricted from entering at all times, effectively creating a prohibition.

- 2.6.** The schedule of declared areas identifies the reason for the restriction as:
- “...the recreation of dogs on this area is contrary to the development plans for the area and inconsistent with the use of this area for outdoor dining.”*
- 2.7.** When the restriction was originally put in place on 25 February 2008, the area was intended to be the premiere events space for Clarence, and the Council at the time wanted to prevent dogs at events. In practice, the area is rarely used for events, and if it were, restrictions on dogs during events would be able to be managed through Council’s event permit application process.
- 2.8.** Further, attitudes towards allowing leashed dogs in outdoor dining areas have changed significantly and it is now common for outdoor dining areas to be “dog friendly”. A new business has recently opened in the area and has outdoor dining fronting the restricted area. The business has advised officers that the restriction makes it difficult for customers with dogs to access their premises without contravening the restriction, although lawful access is still possible via the laneway off Cambridge Road next to the Waterfront Hotel.

- 2.9.** Having reviewed the current restriction, officers are of the view that it is more appropriate to change the restriction to allow dogs to be leashed, unless the conditions of a permit for a specific event prohibit dogs at the event.

3. CONSULTATION

3.1. Community Consultation Undertaken

Preliminary consultation with key stakeholders (including the Eastern Shore Dog Club Inc) has been undertaken. Officers are working closely with the Club, which is supportive of the trial, provided it can still book the entirety of South Street Reserve for its dog training and agility activities on Sunday mornings (which it will be able to do).

Consultation with the wider community is proposed in this report.

3.2. State/Local Government Protocol

Not applicable.

3.3. Other

Not applicable.

3.4. Further Community Consultation

Public notification of the proposed changes is required in accordance with the Act. Wider stakeholder and community consultation is also proposed as part of this process.

Council is required under section 24 of the Act to undertake a public consultation process, which includes notifying the public by providing a notice setting out the area, any proposed condition relating to the use of that area, and in the case of a restricted area or prohibited area, the reasons for the declaration. The public has the opportunity to make a submission within 15 working days after publication of the notice.

Attachments 3 and 4 are the Consultation Promotion Plans for the notification and consultation processes for South Street Reserve and Bellerive Boardwalk.

Community consultation will also ask additional questions around the use of the areas to assist in future planning and decision making.

Following the notification and consultation period, a further report would be brought back to Council to consider any submissions received prior to any declarations being made in respect of either South Street Reserve or Bellerive Boardwalk and changes taking effect. Officers aim to report back to Council at the ordinary meeting on 20 November 2023.

In relation to the proposed off-leash greyhound exercise area and vulnerable dogs' area, further and ongoing consultation would take place during the trial with an additional report back to Council closer to the conclusion of the 12-month trial.

4. STRATEGIC PLAN/POLICY IMPLICATIONS

4.1. Council's *Recreation Needs Analysis 2019* recommended that Council support the Eastern Shore Dog Club to continue its operations from South Street Reserve and work closely with the club to understand future facility requirements. This will be addressed as part of the consultation.

4.2. Council's Strategic Plan 2021-2031 within the Strategic Goal Area *A Well-Planned Liveable City* contains the following Strategy:

“2.11 In line with our Sport and Recreation Strategy, work with local clubs, state and regional organisations and other levels of government to meet the sport and recreation needs of our community.”

4.3. Council's *Active Living Strategy 2022* within *Priority 2: Great Spaces* identifies that Council will Provide contemporary sport, recreation, exercise, and leisure facilities and spaces, and specifically: *“Identify projects that maximise use and increase diversity of use of existing facilities and spaces.”*

5. EXTERNAL IMPACTS

- 5.1.** Any member of the public will have the opportunity to make a submission on the proposed changes. Owners of neighbouring properties, business operators and any interested community members will have the opportunity to review and comment on proposed changes to the declared areas if they have any concerns.
- 5.2.** The proposal for South Street Reserve would not result in any significant loss of space for regular users of the park, given that two general off-leash exercise areas would remain, with only the southern one being reduced in size.

6. RISK AND LEGAL IMPLICATIONS

Council is required to comply with the public notification process set out under section 24 of the Act. If Council determines to make the proposed changes, it will be required to notify the date on which the declaration takes effect and the period during which the declaration remains in force.

7. FINANCIAL IMPLICATIONS

- 7.1.** There are no material financial implications. Costs associated with public notification and community consultation will be accommodated within existing approved budgets.
- 7.2.** Council allocated funding in its 2022/23 budget for construction of an off-leash greyhound area which will be used to construct fencing and signage upgrades at South Street Reserve, necessary to facilitate the proposed changes, if approved.

8. ANY OTHER UNIQUE ISSUES

Not applicable.

9. CONCLUSION

For the reasons outlined above, it is recommended Council approves the commencement of the public notification and community consultation process, with a view to considering making the proposed changes to the Dog Management Policy 2021 and declared areas outlined in this report.

- Attachments:
1. Plans showing Proposed Changes to Declared Areas at South Street Reserve and Bellerive Boardwalk (2)
 2. Dog Management Policy 2021 – Schedule of Declared Areas (7)
 3. Consultation Promotion Plan - Greyhound Off-Lead Exercise Area Trial and Changes to Declared Areas – South Street Reserve, Bellerive (6)
 4. Consultation Promotion Plan - Change to Declared Dog Exercise Area Bellerive Boardwalk (4)

Ian Nelson

CHIEF EXECUTIVE OFFICER

Attachment 1

Proposed Changes to Declared Areas (*Dog Control Act 2000*)

1. South Street Reserve, Bellerive

Current declared areas



Proposed declared areas

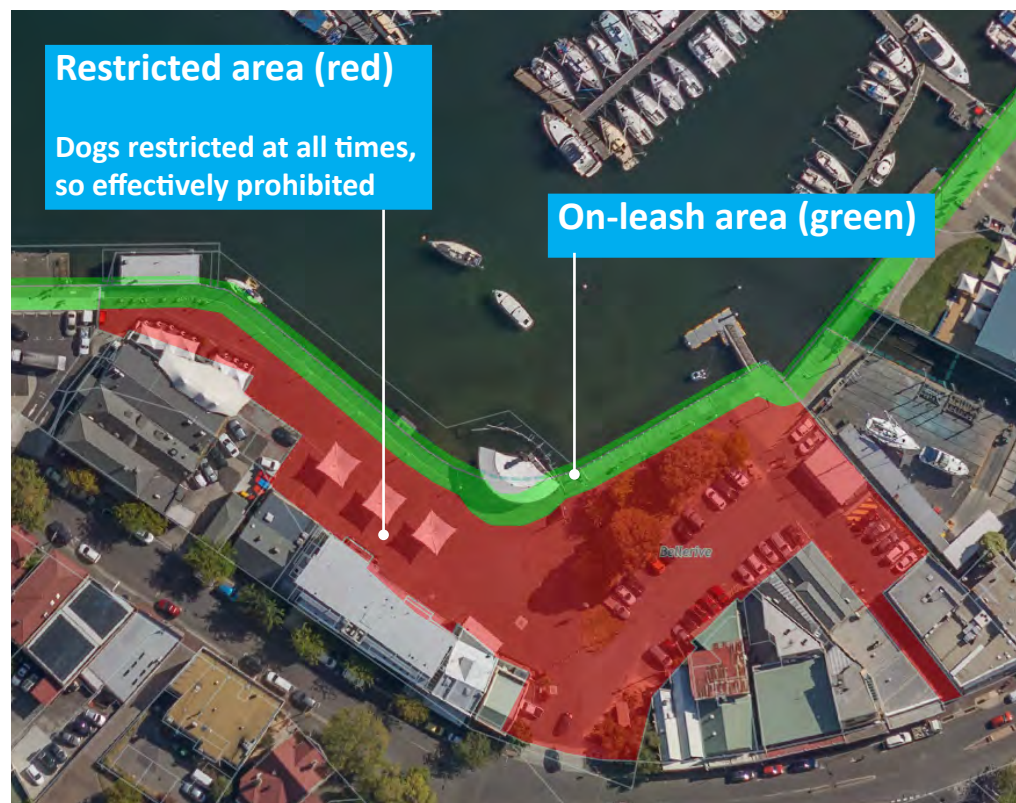


Declared training areas at certain times

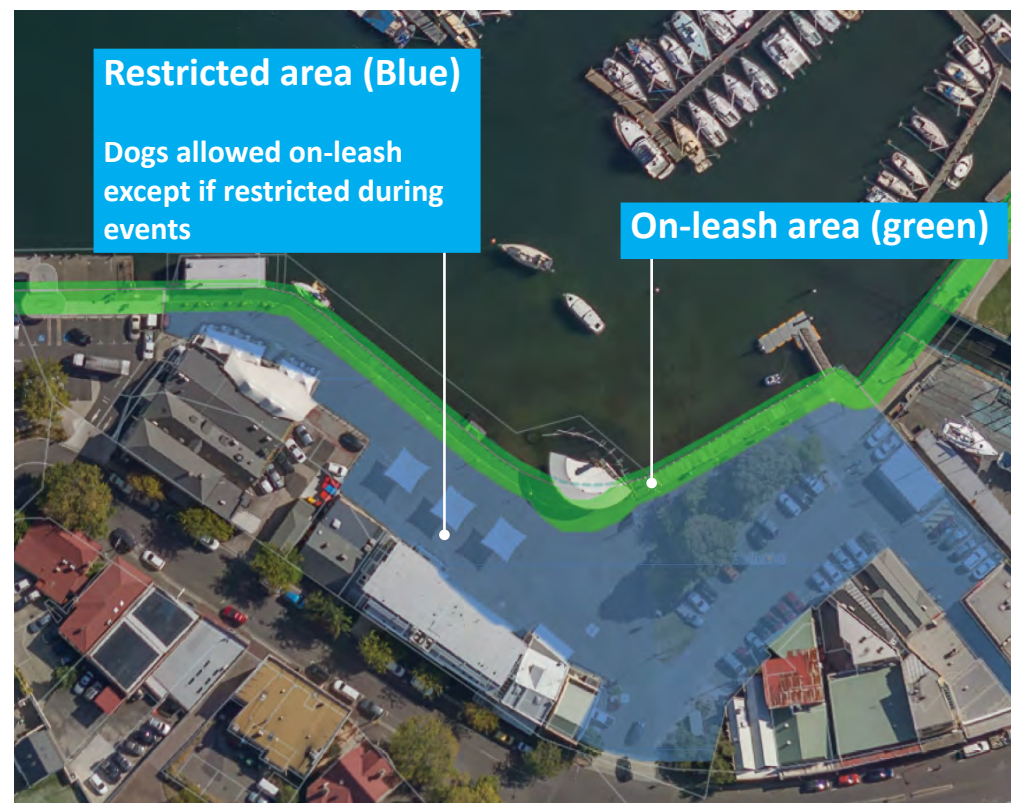
All fenced areas of the reserve will be declared as 'training' areas at any time it is booked for dog training activities by the Eastern Shore Dog Club Inc. The club typically books the reserve on Sunday mornings until 1pm. The park is unavailable for use by the general public at these times.

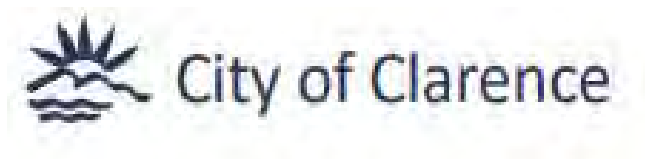
2. Bellerive Boardwalk

Current declared areas



Proposed declared areas





Schedule of Declared Areas

Land to be declared under the provisions of Section 20 of the Dog Control Act 2000

From 1 December 2021
to 31 December 2028

Classes of Declared Areas

The Dog Control Act 2000 defines specific classes of declared areas

Exercise Areas

Areas where a dog may be exercised subject to any specified conditions, and either:

- On-lead
- Off-lead

Training Areas

An area where a dog may be trained subject to any specified conditions. Dogs being exercised in these areas are not expected to be under effective control when taken off lead if their owner is actively engaged with them.

Restricted Area

Areas where dogs are restricted from entering during specified hours, days, or seasons or at all times.

Prohibited Areas

Areas containing sensitive habitat for native flora or fauna where dogs are prohibited



On-Lead Exercise Areas

Shared Trails and Tracks

This declaration is due to the variety of users and the physical characteristics of tracks, trails and multi-user pathways, and includes the following track classifications:

Clarence Coastal Trails

Clarence Foreshore Trails

Rivulet Tracks

Tangara Trail

Roadside Tracks

This declaration includes areas immediately adjacent to the trail or track to a distance of 2 metres where fencing within 2 metres is not present.

Mortimer Bay Coastal Reserve (excluding beach area)

Due to the on-lead status of the Tangara Trail through the reserve, and with the popularity of this area as a horse exercise area, the full Mortimer Bay Reserve will be on-lead. Exercise of dogs is restricted to the defined track areas to protect natural flora, fauna, including habitat and/or areas of cultural significance in accordance with council's reserve activity plan.

Single Hill Bushland Reserve

To protect natural flora, fauna, including habitat and/or areas of cultural significance. This approach is consistent with Council's Reserve Activity Plan. Exercise of dogs is restricted to the defined track areas.

Dune tracks

All access ways onto council beaches are on-lead until the dog reaches the beach proper to protect the dune systems and to protect native flora and fauna.

Bellerive, Boardwalk Perimeter

Dogs are permitted on the perimeter only, to allow dogs to remain on the multi-user pathway from Kangaroo Bay Drive to Victoria Esplanade.

Bellerive, 1a Beach Street

Area immediately behind Luttrell Avenue from Beach Street through to Alexandra Esplanade.

Bellerive, Second Bluff

This declaration includes all areas on the bluff from Alexandra Esplanade to Silwood Avenue entrance.

Howrah, Wentworth Park

Due to the family orientated focus and proximity of schools, childcare and sporting facilities.

Lauderdale, Roches Beach

From Bambra Street entrance, north to a point opposite number 11 Kirra Road (from reef to reef).

Lauderdale, Roscommon Reserve (including Lauderdale Wetland reserve)

Due to the shared usage and sporting activities in this area and to protect local flora and fauna.

Lindisfarne, Simmons Park

Due to numerous family orientated facilities in this park

Richmond, Richmond Riverbanks

This declaration extends to both sides of the bridge and river due to the resident duck population.

Rokeby, Chipmans Road

This area lies adjacent to a significant wetland area.

Rosny, Rosny Hill Nature Recreation Area

Due to the existing wildlife and future development in this area.

South Arm, South Arm Recreation Ground

This recreation area has historically been regarded as a sports ground and prohibited to dogs at all times, but now widely used by a range of community users. To be reclassified as a dog on-lead area.



Off-Lead Exercise Areas

Cambridge, Cambridge Road

Cambridge Dog Park, to be developed behind the soccer grounds

Clarendon Vale, 45 Goodwins Road

Council land between Goodwins and Reynolds Road, excluding sports oval.

Lauderdale, Lauderdale Saltmarsh reserve – to be developed

An area within the rehabilitated tip site land is to be established as dog off-lead

Lindisfarne, ANZAC Park

Dogs must be on-lead when being walked on, or within 2 metres, of the Clarence Foreshore Trail

Seven Mile Beach

Under the flight path - between Day Use Area 2 and Day Use Area 3

Montagu Bay, Rosny Esplanade

Conara Foreshore Reserve.

Oakdowns, Oakdowns Park

Internal area accessed from Woodleigh Drive, Oakdowns Parade, Horsham Road or Cavenor Drive.

Dogs are not permitted to be within 10 metres of any play equipment.

Opossum Bay, Shelly Beach

From Bangor Road entrance east to Icehouse Bluff.

Richmond, Richmond Recreation Ground

Victoria Street, excluding the Skate Park area

Risdon Vale, 6a Sugarloaf Road

Grasstree Rivulet Reserve situated east of Sugarloaf Road, bounded by Grass Tree Hill Road.

Rosny Park, Charles Hand Park

Area bounded by Bastick Street, Riawena Road and Rosny Hill Road, excluding the Skate Park and College grounds.

Greyhound off-lead exercise area

17 Goodwins Road, Clarendon Vale



Restricted Areas

Beaches

Dogs will be restricted from entering the following beaches between the hours of 10.00am and 6.00pm during the period from 1 December to 1 March each year. At all other times dog must be under “effective control” as defined under the Dog Control Act 2000.

- Howrah Beach
- Bellerive Beach from the beach access at Beach Street access east to Second Bluff
- Cremorne Beach
- Opossum Bay Beach
- Roches Beach, Lauderdale from Bambra Street entrance to Mays Point
- South Arm Beach
- Seven Mile Beach - council managed area from Esplanade up to Day Use Area 2

The reason for this declaration is because the above beaches are popular with families during the summer period.

Dogs will be restricted from entering the following beaches at all times:

- Little Howrah Beach due to its size and sheltered nature as well as its historical use.
- Bellerive Beach - from the beach access at Beach Street west to First Bluff to provide a dog free alternative in an area which is popular with families.

Bellerive Beach Play (Rotary) Park

Dogs will be restricted from entering the Bellerive Play Park at all times. The reason for this restriction is due to numerous family orientated facilities in this park, and the adjoining section of Bellerive Beach which is restricted to dogs at all times. However, dogs may be walked on-lead through this area on the Clarence Foreshore Trail only.

Sporting Recreation Grounds and Perimeters

Dogs will be restricted from entering all council sports grounds and perimeters at all times in order to assist in maintaining the grounds to an acceptable level.

In Kangaroo Bay, access will be allowed on-lead on council land adjacent to Rosny College for the purpose of accessing the multi-user pathway.

Village Green, Richmond

Dogs will be restricted from entering The Village Green at Richmond at all times. The reason for this restriction is that this area is used by residents and visitors to enjoy the facilities nearby and traditionally dogs have not been permitted in this area.

Bellerive Board Walk

Dogs will be restricted from entering all areas beyond the waterside perimeter of the Bellerive Boardwalk multi-user pathway at all times. The reason for this restriction is the recreation of dogs on this area is contrary to the development plans for the area and inconsistent with the use of this area for outdoor dining.

Greyhound exercise area

A Greyhound off-lead exercise area is to be fenced and developed on 17 Goodwins Road, Clarendon Vale. This area is for the sole purpose of exercising greyhounds, no other breed of dog is to be taken into this space.

Dune Access Tracks

Dogs will be restricted from entering the dune areas of a beach and any dune track that runs parallel to the foreshore at any time on any day of the year. The reason for this restriction is in order to protect the dune environment from excessive erosion, as well as protect native flora and fauna.

Training Areas

Bellerive, South Street Reserve.

Prohibited Areas

Pipe Clay Lagoon

The council owned area of Pipe Clay Lagoon, from a point opposite number 91 Cremorne Avenue west to a point adjacent to number 201 Cremorne Avenue, dogs will be prohibited from entering as it provides an important habitat for resident and native migratory wading birds.

Racecourse Flats

The council owned area east of South Arm Road known as Racecourse Flats, dogs will be prohibited from entering as it provides an important habitat for resident and native migratory wading birds.

Public Areas not declared in this schedule

For public land not specifically declared under the provisions of the Dog Control Act 2000, and where land is not already defined under Section 28 of the Act as a prohibited public area, a dog is required to be under effective control.

It is not intended that these areas be declared under the Dog Control Act, as this provision of the legislation is sufficient to cover their intended use.

Effective control is a term defined under Section 4 of the Dog Control Act 2000.

- For public space areas a dog is under effective control if it is on a fixed lead not exceeding 2 metres long held by a person of sufficient age and strength to control the dog.
- When in an area where the dog is not required to be on a lead, a dog is under effective control if the dog is:
 - in line of sight
 - within close proximity
 - and immediately responsive to the person's command.
- If tethered to a fixed object by a lead not exceeding 2 metres long for no more than 30 minutes.



Consultation Promotion Plan

Off-Leash Greyhound Exercise Area Trial and Changes to Declared Areas – South Street Reserve, Bellerive

Purpose

The purpose of the promotion of consultation of the trial of an off-lead greyhound exercise area at South Street Reserve in Bellerive is to:

1. Inform regular users of South Street Reserve and owners of Greyhounds about the upcoming trial and seek feedback about the proposed reconfiguration of South Street Reserve.
2. Comply with the statutory process for changing the declarations under the *Dog Management Act 2000* for the proposed greyhound and other exercise areas at South Street Reserve.

Consultation is being carried out concurrently with the statutory notification process for the proposed change to the declared area at Bellerive Boardwalk. However, the anticipated high level of interest in the changes to South Street Reserve and potential for positive media coverage means there will be additional promotional activities carried out for the South Street consultation.

Anticipated start date: 10 October 2023

Anticipated End Date: 7 November 2023 (4 Weeks)

Note: Additional, ongoing, consultation will take place during the 12-month trial. This will be the subject of a separate consultation plan in the leadup to the commencement of the trial.



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
STAGE 1: Visioning									
Media	Media event	✓		Wider community with interest in Council matters.	During week beginning 10 Oct 2023	✓			Potential to pitch a story/photo opportunity with the Mayor and a Greyhound Advocate publicising Council's 12-month trial and consultation.
	Media release	✓		To go with media alert for event (see above)	During week beginning 10 Oct 2023	✓			
	Eastern Shore Sun		✗					✗	
Mailout	Letter to residents	✓		Letter drop to Residents in streets surrounding South Street reserve.	During week beginning 10 October 2023	✓		✗	Non-addressed letter with QR Code link to YSC page and consultation.
	Letter/direct email to Greyhound Owners	✓		Addressed mailout to all owners of registered greyhounds in Clarence	During week beginning 10 October 2023	✓			Individually addressed letter or emails to all registered owners.



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
Council publications	City of Clarence News		×					×	Note: Will submit article with results of consultation and start of trial for next issue, but this consultation will be complete before that is released.
	Other (specify)	✓		Clarence Dog News	Already published in latest issue			×	Will also include follow-ups in later issues as the trial gets underway
Advertisement	Mercury	✓		Requirement to advertise under <i>Dog Control Act</i> . Advertisement for South Street and Bellerive areas to be combined.	Wednesday, 11 October 2023	✓		✓	Declaration cannot take effect until 20 working days after publication of advertisement. Follow-up notification/advert is required.
	Eastern Shore Sun		×					×	
	Facebook	✓		Small budget for paid promotion of Facebook posts, below.	Throughout consultation. Dates to be finalised with	✓			



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
					communications team.				
Social media	CCC Facebook page	✓		Users of South Street Reserve and other interested stakeholders (nearby residents, advocacy groups etc)	Throughout consultation. Dates to be finalised with communications team.	✓		X	Post directing to information and survey on YSC Page.
	Facebook groups (specify)	✓		C-Dog Friends of Bellerive Dog Park	Throughout consultation.	✓		X	Share above post to these Facebook Groups. Administrators of Greyhound advocacy groups on Facebook will also be contacted and invited to share post to members.
Online	Your Say Clarence website	✓		Broader community.	Go-live on 10 October 2023 (if approved by Council)	✓		X	Your Say Survey - 4 weeks commencing on 10 October 2023.
	CCC main website article	✓		Broader community.	Go-live on 10 October 2023 (if	✓		X	Brief news article directing to YSC page. If proposed media opportunity is



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
					approved by Council) Available throughout consultation				picked up, can use that instead.
Signage	Corflute signage	✓		Users of South Street Reserve	Throughout consultation.	✓			4 x Corflute Signs at main entrances to South Street Reserve dog park.
	Posters		✗						
	Postcards		✗						
Stakeholder engagement	Letters to Key Stakeholder Groups	✓		Eastern Shore Dog Club Greyhound Advocacy Groups Hobart Dog Walking Association C-Dog		✓		✗	
On-site promotion	Pop-up Information sessions – South Street Reserve	✓		Users of South Street Reserve	<ul style="list-style-type: none"> 17 October 26 October 4:00pm – 7:00pm	✓	✓	✗	Opportunity to engage directly with users of the area and gather feedback about: <ul style="list-style-type: none"> The trial itself



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
									<ul style="list-style-type: none">• South Street Reserve more generally. <p>The areas of the proposed trial will be line-marked at the site to give a real-world view of proposed changes.</p>

STAGE 2: Measuring Success of Trial

There will be a separate and ongoing consultation process during the proposed trial to measure its success and various other factors to help determine whether it should continue.



Consultation Promotion Plan

Change to Declared Dog Exercise Area Bellerive Boardwalk

Purpose

The purpose of the promotion of consultation of the trial of an off-lead greyhound exercise area at South Street Reserve in Bellerive is to:

1. Inform users of the Bellerive Boardwalk area in front of the stage of the proposed change to the declaration under the *Dog Control Act 2000* for that area from a restricted area (dogs not permitted) to a restricted area (dogs permitted on-leash).
2. Comply with the statutory process for changing the declarations under the *Dog Management Act 2000* for any proposed changes to declared areas.

Anticipated start date: 10 October 2023

Anticipated End Date: 7 November 2023 (4 Weeks)

Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
Media	Media event		×						Potential to include a line about the change in any media around the proposed off-leash greyhound exercise area (see separate



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
									Consultation Promotion Plan).
	Media release		×						
	Eastern Shore Sun		×						
Mailout	Letter to residents		×						See Stakeholder Engagement, below. There will be a limited mailout/letter drop to surrounding businesses and stakeholder / advocacy groups.
Council publications	Clarence News		×						
Advertisement	Mercury	✓		Requirement to advertise under <i>Dog Control Act</i> . Advertisement for Bellerive Boardwalk and South Street declaration changes to be combined	Wednesday, 11 October 2023	✓		✓	Declaration cannot take effect until 20 working days after publication of advertisement. Follow-up notification/advert is required.



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
	Eastern Shore Sun		×					×	
	Facebook	✓		Small budget for paid promotion of Facebook posts, below.	Throughout consultation. Dates to be finalised with communications team.	✓			
Social media	CCC Facebook page	✓		Users of the area	Facebook post during consultation period.	✓		×	Post directing to information and survey on YSC Page.
	Facebook groups (specify)	✓		C-Dog Friends of Bellerive Dog Park	Throughout consultation.	✓		×	Share above post to these Facebook Groups.
Online	Your Say Clarence website	✓		Broader community.	Go-live on 10 October 2023 (if approved by Council)	✓		×	Your Say Survey - 4 weeks commencing on 10 October 2023. Very brief, with invitation to make written submissions.



Promotion tool	Platform	Y	N	Target audience	Proposed timeframe	Primary comms	Follow up comms	Statutory requirement	Comments
	CCC main website article	✓							Article about both sets of proposed declared area changes with invitation to make submissions and link to Your Say Page
Signage	Corflute signage	✓		Users of the immediate area	Throughout consultation.	✓			2 x Corflute Signs at visible locations in the vicinity of the area the subject of the proposed declaration.
	Posters		✗						
	Postcards		✗						
Stakeholder engagement	Letters to Key Stakeholder Groups	✓		Hobart Dog Walking Association C-Dog Local businesses in the immediate area		✓		✗	Letter drop to advocacy groups and businesses in the immediate area, notifying of consultation and the opportunity to make submissions.
On-site promotion	Pop-up Information sessions		✗						

8.4.2 COMMUNITY CONSULTATION TO GAUGE INTEREST IN AN AFL HIGH PERFORMANCE CENTRE IN CLARENCE**EXECUTIVE SUMMARY****PURPOSE**

To approve a consultation process to understand levels of community interest in having the AFL High Performance Centre located within Clarence, specifically at the Rosny Parklands site (formerly the Rosny Golf Course), but also consider other possible sites within Clarence.

RELATION TO EXISTING POLICY/PLANS

Council's Strategic Plan 2021-2031 and Community Engagement Policy 2020 are relevant.

LEGISLATIVE REQUIREMENTS

There are no legal or legislative considerations associated with the proposed consultation.

CONSULTATION

Consultation conducted in April-May 2023 for the City Heart Plan has informed a draft design for the parklands and is relevant to any consideration of alternative uses for the Rosny Parklands, including as a site for the Tasmanian AFL/AFLW High Performance Centre (High Performance Centre).

Consultation across council departments has informed this report, with inputs from City Planning; Infrastructure and Natural Assets; Strategic Development, Communications and Engagement; and Community and Culture.

Consultation has occurred with officers from Department of State Growth, and Department of Education, Children and Young People.

Community consultation is now proposed to understand the community's appetite for the AFL High Performance Centre to be located in Clarence and more specifically, Rosny Parklands, or with facilities spread across Rosny Parklands and Charles Hand Park.

FINANCIAL IMPLICATIONS

There are no significant financial impacts beyond the cost of preliminary investigations, which are being largely met by the Department of State Growth.

The proposed consultation would be undertaken using existing resources.

RECOMMENDATION

That Council:

- A. Authorises the CEO to undertake community consultation with the aim of providing Council with a strong understanding of Clarence community views about having an AFL High Performance Centre: (a) in Clarence generally, (b) fully located at the Rosny Parklands specifically, (c) at dual sites located at Rosny Parkland and Charles Hand Park, and (d) at other sites within the City that may be identified as suitable for the purpose.
- B. Authorises the CEO to continue to work with the Department of State Growth to explore sites that have high potential suitability for an AFL High Performance Centre within the City.
- C. Requests that the CEO provide a report to Council as soon as practical setting out the outcome from the consultation (Recommendation A above) and any further developments or refinement of design options and/or locations (in accordance with Recommendation B above).

ASSOCIATED REPORT**1. BACKGROUND**

- 1.1.** On 3 May 2023, the Tasmanian Government signed a Club Funding and Development Agreement with the Australian Football League (AFL) for the establishment of a Tasmanian-based AFL and AFLW Club.
- 1.2.** Development of a High Performance Centre near the Hobart CBD forms part of the Tasmanian Government's agreement with the AFL.
- 1.3.** On 9 August 2023, Department of State Growth (DSG) representatives met with senior council staff and provided a high-level briefing on the State's process to identify a preferred High Performance Centre site within Greater Hobart.
- 1.4.** At the briefing, DSG representatives outlined the expected social and economic benefits that a High Performance Centre would be expected to bring to the community in which it is ultimately located.

- 1.5.** At the briefing, DSG representatives also advised that:
- Several sites in Greater Hobart were being considered.
 - Rosny Parklands (former Rosny Golf Course) was considered a potential site with a high degree of suitability due to its size and location.
 - While several other sites within Clarence had been considered by DSG, they were not rated as favourably due to a mix of factors, including distance from Hobart CBD, displacement of existing user groups, site amenity, available space, cultural and heritage issues, etc.
- 1.6.** CEO advised DSG representatives that:
- The draft City Heart Plan is nearing completion and includes a vision for Rosny Parklands as a high quality public open space and green corridor for the enjoyment of the community. The City Heart project page on Your Say Clarence was shared with representatives.
 - Community engagement would be needed to test social licence for a High Performance Centre on the Rosny Parklands site as a preliminary issue, subject to Council approval.
- 1.7.** At a workshop on 14 August 2023, councillors provided guidance to staff, to work with DSG representatives to explore the viability of a High Performance Centre within Rosny Parklands, subject to the specifications of both Council and AFL at the Rosny Parklands.
- 1.8.** DSG representatives advised they were prepared to cover the reasonable consulting fees that Council may incur for this planning work.
- 1.9.** MGS Architects, consultants preparing the City Heart Plan, were briefed, and engaged under a variation to the existing City Heart Plan contract, to investigate an option for Rosny Parklands that included the specifications of the High Performance Centre, to allow the site suitability and impacts to be tested.

- 1.10.** On 17 August 2023, Greater Hobart Mayors wrote to the Minister for Stadia, The Honourable Nic Street MP, to request that the State Government liaise with the Greater Hobart Strategic Partnership around the High Performance Centre site selection, noting that a Greater Hobart perspective should underline such a strategic land-use and infrastructure decision, for the benefit of the entire region.
- 1.11.** At a council workshop on 18 September 2023:
- DSG project representatives provided a briefing to councillors about the benefits that a High Performance Centre could bring to a local area. (Attachment 1)
 - MGS Architects presented analysis of a high-level concept within the north-east section of the Parklands, incorporating the AFL's requirements for the High Performance Centre and design criteria provided by Council staff (These are listed in section 3 of this report).
 - MGS Architects highlighted site constraints associated with achieving the specifications of the High Performance Centre at the Rosny Parklands. (These are outlined in section 4 of this report).
- 1.12.** DSG representatives later advised that the concept tested by consultants may be cost prohibitive due to the extent of earthworks and retaining walls required to achieve flat ovals within the sloping topography of Rosny Parklands, and the option is unlikely to represent a feasible option.
- 1.13.** DSG representatives have recently advised that an alternative may be considered under the Site Assessment Process. For example, one MCG sized oval with an adjacent main building, on the Rosny Parklands, plus shared use of a secondary oval in close proximity, may be appropriate to put forward for consideration.
- 1.14.** Concept investigations on the alternative option outlined in 1.13 above, have not yet been completed, and are pending the outcome of this Council decision.

1.15. At a workshop on 25 September, councillors received an update on the project, and briefings from staff on:

- A proposed community consultation approach.
- Other locations within Clarence raised in discussion include Seven Mile Beach, Geilston Bay Recreation Area and Bayview Secondary College Sporting Precinct, none of which is owned by Council. They may be eligible for selection under the State's process, but any expression of interest to the State Government would need to be made by the relevant landowner.

2. AFL HIGH PERFORMANCE CENTRE IN CLARENCE – ECONOMIC BENEFITS

2.1. The establishment and operation of a High Performance Centre in Clarence would be expected to yield significant economic benefits including job creation, increased tourism, and local business expansion in the area.

2.2. Having a High Performance Centre located in Rosny Park, amongst existing business activities, is likely to offer the greatest flow-on economic benefit as it would be expected to leverage existing businesses and infrastructure in the area.

2.3. Alternatively, in an area such as Clarence Plains, the High Performance Centre could be a catalyst for economic development – acting as an anchor institution in a rapidly growing area, driving further investment and complementary social programs relevant to its local community.

2.4. A High Performance Centre would be expected to create jobs in Clarence, including:

- Construction sector jobs during the development of the facility.
- Employment for up to 150 Full-Time Equivalents (FTEs) has been estimated once the centre is operational, including players, coaches, and administrative staff.

- If located in Rosny Park, such an uplift would be expected to create a ripple effect, fostering increased spending, patronage and business opportunities for shops, restaurants, and services in the local area. Future capital investment would also be expected to be a flow-on in the surrounding precincts.

2.5. The High Performance Centre may serve as a focal point for sports tourism, attracting fans who contribute to the local economy by spending on accommodation, shopping, dining, and entertainment.

2.6. Attraction of elite athletes, coaches, and sports scientists to the area can be expected to enrich the local workforce with skilled and educated professionals. High Performance Centres can serve as vibrant hubs for sports science research and development, potentially leading to innovative breakthroughs that further bolster the local economy.

3. AFL HIGH PERFORMANCE CENTRE - LOCATION AND DESIGN REQUIREMENTS

3.1. DSG representatives advised Council of the criteria that will inform the site selection process and be used to assess different sites put forward by Greater Hobart Councils (and potentially other landowners) for consideration:

- Sufficient space to house the required field/s, building facilities and car parking;
- Access via a service/main road;
- Close proximity to Hobart CBD;
- Access to a second ground – preferably on the same site but if not nearby;
- Connection to local amenity – shops, services, public transport and housing;
- Not displacing or relocating existing sporting club use;
- A supportive local government/landowner that can see the benefit that such a facility could bring to their community.

3.2. AFL High Performance Centre Site Assessment Process is included for information as **Attachment 2**.

3.3. DSG representatives have also provided requirements for a High Performance Centre, and these were the starting point for MGS's investigations:

- Gross ground floor area of approximately 9,000m² (approximately 7,000m² footprint with 2-storey design potentially possible;
 - An indoor training area/s of at least 1,400m²;
 - Strength and conditioning areas of at least 500m²;
 - Gender neutral locker rooms (AFL/AFLW) of at least 120m² for each;
 - Medical, physio, and treatment facilities of at least 200m²;
 - Wet recovery facilities of at least 250m²;
 - Football and administrative staff office facilities of at least 400m²;
 - Player/staff breakout and lounge facilities of at least 120m²;
 - Meeting rooms and auditorium of at least 200m²;
 - Club dining area of at least 200m²;
 - Full sized, MCG dimension fenced oval (162m x 139m boundary to boundary) approximately 22,000m², oriented north-south;
- Additional grassed training area adjacent to the main oval or close by, approximately 150m long by 120m wide boundary to boundary where the training area is an oval shape, and, where the training area is not an oval shape, then of similar total size in square metres and an equivalent quality to the MCG;
- Car parking – approximately 150 dedicated car spaces.

3.4. Within the context of the Rosny Parklands site, Council staff included several guidelines into MGS's brief to ensure alignment with the goals of the City Heart project, being:

- The concept must be **well integrated into the parklands** and the surrounding precincts. This means the facility needs to sit harmoniously in the landscape and be physically well-connected through to Rosny Park town centre and bus mall, with good active transport links to public transport and other locations within the City Heart area,

- The **secondary playing field can be accessed and used by the community** in addition to AFL and AFLW teams, and that its design may be less formal and provide a visual transition space between the formal sports infrastructure of the High Performance Centre and the informal open spaces that will characterise the parklands. The second playing field could potentially be used for occasional performances and community gatherings,
- The **High Performance Centre is to be located to the north-east of the site**, leaving sufficient space for the planned expansion of the Rosny Farm cultural precinct and the Art Play Park proposed in the draft City Heart Plan, and
- **Traffic and parking impacts to be considered holistically** in light of other City Heart goals and a separate planning assessment may be required to test the car parking numbers.

4. SITE CONSTRAINTS AND CHALLENGES - ROSNY PARKLANDS

- 4.1.** Despite the economic and social benefits that an AFL High Performance Centre at Rosny would be expected to deliver, there are issues and constraints associated with developing the site for this purpose that are important for Council to consider.
- 4.2.** Consultants MGS Architects undertook a high-level design exercise to overlay the AFL/State requirements for the High Performance Centre (building and two ovals) and the Council's guidelines at the Rosny Parklands site.
- 4.3.** MGS have been responsible for developing the City Heart plan and are therefore familiar with Rosny Parklands and the surrounding precincts. MGS, Council staff and DSG representatives collaborated to test options, with the aim of finding a viable option to test with community.

- 4.4.** Through the process, site constraints and issues requiring further investigation and mitigation of impacts have been identified, including:

Planning

- Not all uses associated with the High Performance Centre comply with the current zoning (Recreational) and a planning scheme amendment would be required to accommodate the proposed uses.

Civil and Geotechnical

- Achieving flat ovals on the sloping site will require significant earthworks, battering and retaining walls, leading to challenges with levels and access, engineering, and cost.
- Geotechnical advice is needed to determine the substrate and its suitability for the extensive cut and fill required.
- Excavation near existing infrastructure (including the Tasman Highway, TasWater and TasNetworks easements) needs to be carefully considered.

Hydrology

- Infrastructure in the flood zone of the Kangaroo Bay Rivulet is likely to exacerbate existing flooding issues upstream and downstream, and therefore any proposed encroachment into the flood zone must be carefully considered, designed and managed.

Environmental

- A significant number of important mature trees would need to be removed for the development of ovals and batters shown in the high-level preliminary designs, impacting bird habitat and food source, shade, and visual amenity.
- There would be considerations around threatened flora species impacts and lighting pollution which may also affect wildlife and habitats.

Landscape and Universal Access

- Disturbance areas will require extensive planting to remediate the impacts of earthworks.
- The network of paths needs to be at 1:20 gradient to ensure universal accessibility, and these pathways need to link up to access all parts of the parklands, preserving overall pedestrian connectivity around and through the site.

Placemaking and Public Realm Amenity

- As the oval/s would be perched at a higher level in the park above the Rivulet corridor, visibility and sightlines to and from the rivulet level (main pedestrian pathway through the site) would be reduced. This means that the activity on the oval/s may not be easily visible to people walking through the site along the Rivulet corridor. The Rivulet corridor may therefore lack passive surveillance or “eyes on the space” contributing to reduced feelings of safety.
- Reduction of mature trees and creation of retaining walls may increase traffic noise from the Tasman Highway across the parklands and toward the Rosny CBD area, particularly in the area of the Council Chambers and Tasmanian Government offices.
- Visual impact and bulk of development needs consideration, from key arrival points to the parklands such as the Gordons Hill Road entry and Council Chambers.

Connectivity and Parking

- Impacts on traffic and parking will be very important to understand and mitigate.
- As proposed in the City Heart Plan an accessible pedestrian connection from the High Performance Centre, across the Rivulet to Rosny CBD and Bus Mall will be needed to realise the community access, economic and activation benefits.

- The extent of ovals and impacted areas/batters will make it difficult to achieve the planned Tasman Highway Cycleway and a shared trail along the Rivulet as proposed in the City Heart Plan.

The combination of these site constraints and issues has resulted in no acceptable option being identified that satisfies all the AFL High Performance Centre requirements within the project's probable budget, and Council's reasonable expectations for accessible and sustainable outcomes.

- 4.5.** It is possible that a hybrid approach could work, where a single oval and High Performance Centre building are located on the main Parklands site, and a secondary shared oval is located elsewhere nearby. However, this has not yet been tested by the consultants.

5. ALIGNMENT WITH CITY HEART PLAN

- 5.1.** At its meeting of 24 February 2020, Council approved development of a long-term and large-scale urban planning project to establish a future vision for the Rosny Park activity centre and surrounds (The City Heart Project).
- 5.2.** Community engagement (albeit disrupted by COVID-19) was undertaken by consultants Timmins Ray throughout 2020. An outcomes report was provided to the Council at its meeting of 14 December 2020 and published on the Council's website. The high-level goal of the City Heart Project is to strengthen the sense of place within the Rosny Park central business district (CBD), to foster a vibrant and liveable primary activity centre for Clarence. The former Rosny Park Golf Course land was later added to the City Heart Project Area.
- 5.3.** The *Clarence City Council Strategic Plan 2021-2031* was released in May 2021, identifying the City Heart Project as a Major Strategic Project.

- 5.4.** The scope for the City Heart Plan was developed in early 2022, followed by an Expression of Interest process which was undertaken in July 2022, and at its meeting of 5 September 2022, Council accepted the quotation from MGS Architects as the lead consultants of a multi-disciplinary team to develop the plan.
- 5.5.** A detailed brief was provided to the consultants in September 2022 with broad community engagement and expert analysis undertaken from October to December 2022, leading to the publishing of the *Analysis and Key Directions Report* in December 2022.
- 5.6.** Concept proposals for the City Heart plan were developed throughout January-March 2023 and the *City Heart Concepts Report* was issued in March 2023 for an extensive consultation involving community and key stakeholders over a seven-week period from March-May 2023.
- 5.7.** The engagement on the Concepts Report informed the **Draft City Heart Plan (Attachment 3)** however the draft is yet to be formally considered by the Council for adoption.
- 5.8.** A final round of community consultation will occur in relation to the City Heart Plan, ahead of Council considering the plan for adoption. However, this step has been paused, to provide the community with an opportunity to have a say on the AFL High Performance Centre in line with the State's project timeframes, and specifically the potential for locating it within Rosny Parklands, which if it went ahead would require substantial changes to the City Heart Plan.
- 5.9.** Six principles were developed to guide the City Heart Plan, all achieving high levels of community support during engagement. They are:
- 1 **A living city heart** on palawa country celebrates and connects to our unique sense of place and culture.
 - 2 **A resilient and sustainable city heart** that values and connects with our natural environment.

- 3 **A vibrant city heart** that nurtures our economy, opportunities, and experiences.
- 4 **An inclusive city heart** that offers diverse housing choices and affordable housing.
- 5 **A liveable city heart** that provides great places and spaces designed for the whole community to enjoy.
- 6 **A connected city heart** that links people and destinations with a network of high-quality streets, promenades, and pathways for all transport modes.

5.10. A High Performance Centre within the Rosny Parklands, if it were well designed, and able to be sustainably integrated into the site, with all issues and constraints managed or mitigated, has the potential to deliver strongly on Outcome 3: *“A vibrant city heart that nurtures our economy, opportunities, and experiences.”*

5.11. However, initial design investigations have not been able to demonstrate sustainable integration of the requirements of the High Performance Centre (as set out in section 3) into the Parklands, meaning that such a project would struggle to achieve Outcome 2: *“A resilient and sustainable City Heart where we value and connect with our natural environment”* - particularly the following Strategies:

“2.2 *Retain established trees and incorporate regenerative design outcomes for the Kangaroo Bay Rivulet, Rosny Parklands, and other open spaces to promote urban biodiversity, and*

2.5 *Integration of best practice design in flood prone areas.”*

Vision for Rosny Parklands in the City Heart Plan

5.12. Prior to the High Performance Centre being a consideration, Rosny Parklands was recognised in the **draft City Heart Plan (Attachment 3)** as a unique opportunity to retain a substantial green public open space within the heart of a growing city while activating the area to ensure greater levels of community use, enjoyment, and benefit.

The community has consistently told us that retaining and enhancing the green spaces in the City Heart for everyone to enjoy, for the long term, is a high priority. Therefore, the balance of community access versus private AFL/AFLW use, will be pivotal to a successful outcome.

5.13. The City Heart vision for the Rosny Parklands precinct is to offer “diverse outdoor spaces that evoke the spirit of Clarence - embracing our cultural origins and revelling in the contemporary expression of a place where everyone can participate in a healthy and active life, celebrating the arts and the outdoors within an ecologically diverse setting and gathering together as one community with a shared future”.

5.14. The Rosny Parklands concept within the Draft City Heart Plan offers a series of different activity areas across an expansive and connected parkland network. The landscape architects took a “design with nature” approach, responding to the existing slope and levels, rivulet and drainage lines, trees and grasslands to create different areas of focus- including:

- Active Park (at Charles Hand Reserve) a place to enjoy the energy that comes from active participation in a vibrant, safe and playful outdoor setting.
- Art and Play Park which strengthens Rosny Farm as a culture hub – a gathering place that supports education, arts and play of all kinds and inspires creativity, stimulates intellectual curiosity, and fosters critical thinking. An age and abilities inclusive, art-themed playful space welcoming the child in everyone.
- Civic Park that provides atmospheric spaces where community members can celebrate community life together – a “living room for the city”.
- Surrounding these areas would be the Wild Park with Kangaroo Bay Rivulet as a celebrated feature connecting the various spaces in a green corridor with accessible pathways, areas for Aboriginal cultural expression and an ecological approach to design.

- Warrane Parklands continues urban green space into the Inner and Outer Warrane residential neighbourhoods that are likely to experience greater infill development and residential densification in the coming decades, with a particular focus on water sensitive urban design, street trees and flood mitigation measures.

5.15. While an AFL High Performance Centre may appear to fit within the area of the former Rosny Golf Course site, extensive cut and fill would be required to accommodate two ovals and the building with high-level concepts indicating approximately 31% of the Parklands being utilised for this development, and the vision for Rosny Parklands in the draft City Heart Plan could not be fully realised.

5.16. If one oval rather than two, in the Parklands, could be acceptable to the State Government, potentially halving the site coverage, as well as high levels of community access through and around the site were provided, a welcoming and inclusive parkland experience for Clarence residents could be achieved more harmoniously, while leveraging the economic and social benefits of having the AFL/AFLW teams based here in Clarence.

City Heart Vision for Civic and Employment Precinct

5.17. The Civic and Employment precinct within the **draft City Heart Plan** includes the Council Chambers, Rosny Library, Bellerive Police Station, Service Tasmania, government offices at 130 Gordons Hill Road and CR Barnard Drive.

5.18. The concept imagines a civic and community hub, connected to Rosny Parklands by an accessible pedestrian bridge across Kangaroo Bay Rivulet, with increased space for services and employment uses to grow, connected by welcoming outdoor places and landscapes for workers and visitors.

5.19. A redevelopment of the Council Chambers building is currently in early design and planning stages.

- 5.20.** A High Performance Centre could complement the vision for the Civic and Employment Precinct, provided it is well designed and integrated into the site and includes a high-quality pedestrian link across the rivulet to connect the Rosny Parklands with the Civic and Employment Precinct (as detailed in the City Heart Plan).

6. ENGAGEMENT APPROACH IN SUMMARY

- 6.1.** The proposed consultation and engagement plan (**Attachment 4**) outlines the detailed engagement approach.
- 6.2.** The purpose of the proposed community consultation is to provide Council with a strong understanding of how the Clarence community feels about having an AFL High Performance Centre located (a) in Clarence generally, (b) fully located at the Rosny Parklands specifically, (c) at dual sites located at Rosny Parkland and Charles Hand Park, and (d) at other sites within the City that may be identified as suitable for the purpose.
- 6.3.** Consultation methods will include a survey and face-to-face methods.
- 6.4.** The consultation program will reach out to those who previously engaged in the City Heart engagement stages, as well as seek to achieve high participation by the whole Clarence community through social media (paid and unpaid) and community pop-up sessions.
- 6.5.** A consistent scale will be used within the survey for appropriate comparison with previous City Heart parklands engagement results.
- 6.6.** The engagement period will be for a minimum of four weeks (excluding school holidays) in line with Council's practice.

- 6.7.** While the Rosny Parklands presents clear locational advantages to the AFL/AFLW teams, a High Performance Centre is a more intensive and challenging use of the site than was previously envisaged, and elite sports use is a significant departure from what has been previously shared and developed with the community. For this reason, it is critical that the proposal be subject to community consultation in order to identify the level of community support for the project (social licence) and to identify any critical issues (beyond those identified in this report) that the Clarence community has concern about.

7. EXTERNAL IMPACTS

- 7.1.** If a suitable site is identified, there is significant potential for Clarence to play a role in the establishment of an AFL/AFLW team in Tasmania.
- 7.2.** Significant stakeholder engagement work has been undertaken through the City Heart Plan project to ensure that the project is widely known about and that impacts are positive, but the AFL High Performance Centre has not been part of these conversations.
- 7.3.** If a High Performance Centre was to be developed in the Parklands, trade-offs would need to be accepted such as a reduction in the amount of public open space available for passive recreation, and a downscaling of the City Heart parklands vision, in order to leverage the positive impacts such as employment growth and tourism, into the future.
- 7.4.** Community and stakeholder engagement, transparent communications, further technical investigations, and good governance principles will be important in the selection and development phase, should Council decide to proceed.

8. RISK AND LEGAL IMPLICATIONS

There are no particular risk or legal implications associated with engaging the community on this matter.

9. FINANCIAL IMPLICATIONS

- 9.1.** There are no significant financial impacts beyond the cost of preliminary investigations, which are largely met by the Department of State Growth.
- 9.2.** The proposed consultation would be undertaken using existing resources.

10. CONCLUSION

- 10.1.** It will be difficult for any site in Clarence to satisfy all site and development criteria associated with a High Performance Centre, that is, a suitably large, flat site near Hobart's CBD, connected to urban amenity, and not already utilised by sporting clubs or schools.
- 10.2.** Noting the comment above, there will need to be open communication between all relevant parties (Council, DSG, AFL) to find workable compromises on one or more aspects of the site selection criteria if the project is to proceed in Clarence.
- 10.3.** Rosny Parklands offers many advantages for a future High Performance Centre, both in terms of the amenity it would afford Tasmanian AFL/AFLW players, coaching and club staff, and importantly, it would generate employment, business and other economic benefits within the heart of Clarence.
- 10.4.** Options have been tested on the north-east section of the Rosny Parklands, given the locational suitability of the site, the potential for benefit to the community and the State's interest in the site, however achieving all the AFL High Performance Centre's requirements on that site is not practicable, due to the significant site constraints.
- 10.5.** Community engagement will provide Council with a good understanding of the community's appetite and interest in Clarence being the home of the Tasmanian AFL/AFLW team and its High Performance Centre.

- 10.6.** Pending the outcomes of the engagement, a Rosny Parklands hybrid option could be investigated (as noted in the Recommendation), with one oval on former Rosny Golf Course site which would be a fenced, primary oval for exclusive AFL/AFLW team use, and the 9,000m² building facility, and associated car parking located under the building. A second oval would need to be identified nearby for shared community and AFL/AFLW use, such as a new oval on Charles Hand Park.

- Attachments:
1. Presentation to Council Workshop on the AFL High Performance Centre [DSG] (19)
 2. AFL High Performance Centre Site Assessment Process [DSG] (16)
 3. Draft City Heart Plan (88)
 4. Proposed Engagement and Consultation Plan (4)

Ian Nelson

CHIEF EXECUTIVE OFFICER

AFL Tasmania High Performance Centre



Presentation to Clarence City Council Workshop

By Department of State Growth

September 2023

Welcome and Agenda

- Ben Moloney, High Performance Centre Project Director – Department of State Growth
 - Project Background
 - Vision for the High Performance Centre
 - Connection To City Heart
 - Community use and benefit
 - Site assessment and selection process
 - Next steps

Background

- On 3 May 2023, the Tasmanian Government signed the AFL Licence Agreement to establish a Tasmanian AFL and AFLW Club.
- A High Performance Centre will support the Tasmanian AFL Licence and underpin future player attraction and retention.
- The Tasmanian Government will determine the location of the facility at a location in close proximity to Hobart central business district and will use best endeavours to achieve practical completion of the facility on or before 31 December 2025.
- Up to \$60 million has been allocated by the State to deliver the project, with a further \$10 million to be contributed by the AFL.



Background



The Tasmanian AFL High Performance Centre will:

- Be a facility that enables the recruitment and retention of high-quality players, coaches, support staff and administrators.
- Include external and internal training facilities as well as office spaces and facilities for football and administrative staff.
- Provide become the home to the Tasmania's AFL, AFLW, VFL and VFLW teams and when required, will become the Southern Hub for underage pathway development.
- Be a community asset and a vital link between the Tasmanian AFL club and the community in which it is built.

In addition to the delivery of the high performance outcomes, it is imperative that the facility has the ability to further develop and foster community partnerships and programs.

The vision for the facility is to provide the players (men and women), coaches and administrators with every opportunity to be successful, both on and off the field. In addition, the facility is anticipated to be accessed for community use purposes and for use by Tasmanian elite athletes.



Community use

The ability to further develop community partnerships and programs is a crucial element for the vision of the facility.

- The facility will create opportunities to provide for multiuse sports training and development that is accessible to the community and other Tasmanian sportspersons, whilst meeting the specific requirements of the AFL club.
- The new facility will be planned and designed to ensure that “elite” and “community use” can be achieved in harmony.
- The functions and extent of community use will be evaluated during the project’s planning phase, with significant input from the municipality that the centre is to be located in.



Community benefits and use

- Majority of the AFL Club training facilities enable public access to the main training ground outside of Club use.
- Generally, facilities with two training grounds incorporate public access and shared use on the secondary field.
- Majority of Club's have facilities that are accessible to the public.
- Community programming (both in the AFL and NRL) is largely targeted towards local communities that sit within the same suburban and local government area of the Clubs.
- Can include café, gym, community programs, oval access outside club allocated use

Example – Brisbane Lions Springfield Training Facility



Credit: Populous

Best practice community programs



CARLTON
RESPECTS

Carlton Football Club

Carlton Respects is the Club's flagship community initiative promoting gender equality for the prevention of violence against women. Established in 2016, the initiative was developed in response to the alarming statistics around family and domestic violence in Australia.

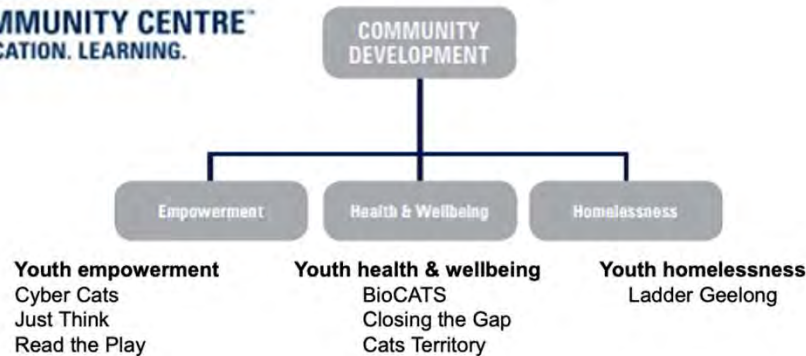
The initiative consists of a Schools Program, Awareness-Raising Campaign and Workplace Charter, all with the aim of promoting gender equality for the prevention of violence against women.

School's program: Developed in 2016, the Schools Program educates Australian Students aged 8-14 about respect and equality as a tool for primary prevention of violence against women. 2022 has seen the first full year of the Carlton Respects Digital Learning Platform which has allowed for rapid growth and uptake of the program across the State and Country.

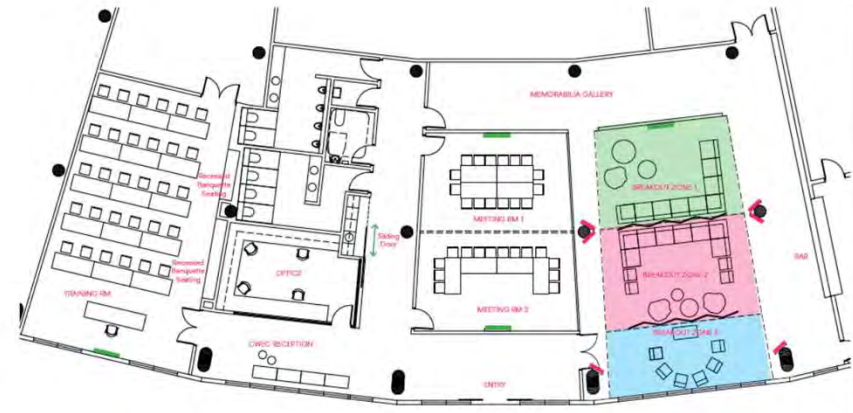
Without dedicated onsite facilities, this program has been delivered on campus and via digital means.



DEAKIN CATS COMMUNITY CENTRE™
COMMUNITY. EDUCATION. LEARNING.



Best practice community programs



The Facilities

- The Training Lab is a flexible space designed to seat 36 in class room style however can be configured in a number of ways as the furniture is on wheels.
- The Meeting Room space can be configured in a number of ways including theatre style, u-shape and board room accommodating up to 80 people in theatre style.
- Large multi-purpose area that can be used as three break out areas to fit approximately 18 people in each section or theatre style to seat 100 for presentations or 150 in for a stand-up event.
- Bi-fold doors between the meeting room and multi-purpose area allow for a large expanded space.

Best practice community programs

THE HUDDLE SNAPSHOT

SINCE INCEPTION IN 2010, THE HUDDLE HAS:



ENGAGED WITH OVER
120,000
young people



FROM OVER
160 cultures
ACROSS AUSTRALIA



1,000 volunteers
HAVE CONTRIBUTED

20,500 hours
OF THEIR TIME

sharing their skills and
knowledge to empower
young people

500 sportspersons
have participated in programs
from various sports



DELIVERED
PROGRAMS IN



250 schools
ACROSS AUSTRALIA



\$23m raised
AND INVESTED BACK
INTO COMMUNITY

350 employment outcomes

for young people through a mix of
local partnerships (since 2018)



11,000+ social
media followers

Today, The Huddle offers



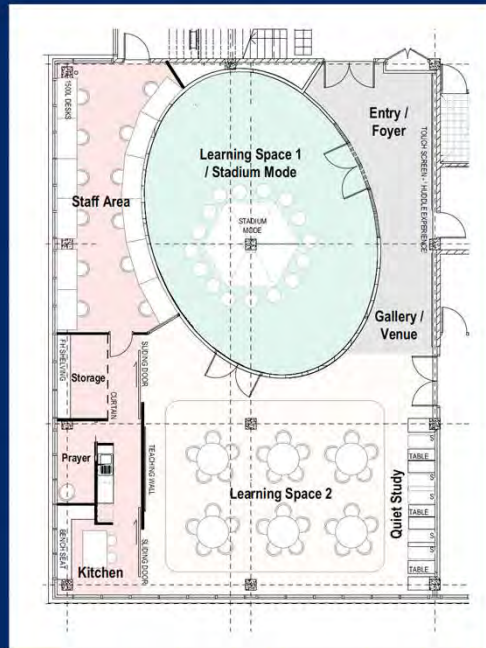
15 programs
delivered in schools,
communities and online

North Melbourne Football Club

Born out of the role and value of sport and the influence the North Melbourne Football Club plays in connecting people across different ages, genders, and cultures; The Huddle was established in 2010 as the community arm of the Club in partnership with the Scanlon Foundation and the Australian Multicultural Foundation.

It was established to engage, support, and empower young people to build on their strengths, increase their ability to participate in society and contribute to more socially inclusive communities.

Best practice community programs



• The redevelopment of the North Melbourne Football Club (NMFC) and The Huddle facilities will deliver a range of immediate benefits, including:

Community
changerooms

Multipurpose
community room

Multi-media room

Newly designed
Huddle facilities

Office accommodation

Addresses inequalities
in female change
rooms and shortage of
community amenities

Provides safe, inclusive
and welcoming spaces
for mothers, their
children and families
connected with The
Huddle programs

Creates shared spaces
for professionals and
young people to work
together to strengthen
education, leadership,
communication and
employment

Delivers world-class
learning outcomes
for The Huddle's
programs, schools,
Universities and
communities

Addresses the lack of
adequate workspace
for a rapidly growing
Huddle and NMFC
workforce, including
volunteers



NORTH MELBOURNE HUB: Connecting Communities through the power of sport

The Programs are delivered from a purpose built community facility that forms part of the Club's High Performance and Administration facility.

Best practice community programs



“Game Day”

Taking place in a brand new, state-of-the-art facility, The Game Day facility and program has been designed so that are required to think critically and work collaboratively to achieve Science, Technology, Engineering, and Mathematics challenges, such as building a pendulum to kick a ball through the goals.

The students explore concepts such as artificial and emotional intelligence, grit, and mindset as well as identify their own characteristics that will support students ‘to win’ throughout life.

Rosny Parklands site



- ✓ Sufficient space for building, main field and car parking.
- ✓ 2nd full sized match field possible on the site
- ✓ Proximity to CBD, new Stadium and likely marque player residential areas
- ✓ Space available to explore community aspects / partnerships.
- ✓ Potential catalyst for Council's City Heart project vision
- ✓ Connection to local shops, services, public transport and housing
- ✓ Not displacing/relocating existing sporting club use
- ✗ Extent of site works required to create level playing fields
- ✗ Need to provide good active transport routes through the site
- ✗ Risk of excessive expectations that the State make substantial complementary/ancillary investments

Site Selection and Assessment Process

- Selecting a site for the High Performance Centre is about more than the technical requirements of the land on which the centre would be built.
- The State is seeking to partner with a local government that can see the benefits that such a facility could bring to their municipality.
- The State has developed a site assessment and selection process that will allow participating councils to have a fair opportunity to develop and understand suitable options for sites that may meet the needs of the AFL/AFLW club.
- Participating councils invite to prepare a written proposal by the end of November 2023 that includes:
 - Identification of site
 - Confirmation that council supports in principle, the use of the site for the High Performance Centre
 - Identification of appropriate impacts mitigations
 - Proposed general terms and conditions that council would apply for the use of the site for the High Performance Centre.

Milestone or Activity	Timeframe
Initial identification and technical appraisal of sites.	June 2022
Confidential discussions between State and Greater Hobart councils.	Aug/Sept 2023
Development of options with councils including public consultation.	Oct/Nov 2023
Comparative assessment of sites supported 'in principle' by councils.	December 2023
Negotiate. Finalise and formalise an agreement for the preferred site.	Jan/Feb/March 2024

Potential benefits for Clarence municipality



Community utilisation of the facility

Home town of the AFL and AFLW teams

Construction investment and jobs

Long-term local employment opportunities

Elite sporting facilities within the community can offer numerous benefits to individuals and the broader community. Here are some of the key advantages:

- Community pride and identity.
- Social Interaction and Networking.
- Healthy Lifestyle Promotion.
- Community Partnerships.
- Legacy and Long-term Impact.

Well-established sporting facilities can leave a lasting legacy for the community, providing benefits for future generations. They can become part of the community's heritage and continue to support athletic development and well-being for years to come.

Connection to a vibrant City Heart

The High Performance Centre will be the heart and soul of Tasmania's football club.

Around 150 players, coaches, support staff and administrators will use the site every day, with significant flow on impacts to the local community.

Players will live in the local area, visit shops, cafes and restaurants

Benefit of Rosny Parklands site is the connection to the Rosny shopping precinct, Bellerive Beach, Blundstone arena, ferry terminal

Live and work in the community

Catalyst for further investment

Council's initial questions, thoughts and feedback?



Contact details



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AFL High Performance Centre

Site Assessment Process

26 September 2023



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I Synopsis

The Tasmanian Government recognises that selecting a site for the AFL High Performance Centre is about more than the technical merits of the land on which Centre would be located; it is also about the surrounding community and the level of support such a facility would have at any chosen location. As such, we are seeking to undertake a site selection process that includes working collaboratively with councils to explore site options.

The project proposes \$60 million to \$70 million of capital investment in the chosen municipality, offers long term employment opportunities for many people at the facility, and many other potential benefits for the local community.

The High Performance Centre will become the home to the Tasmania's AFL, AFLW, VFL and VFLW teams and when required, will become the Southern Hub for underage pathway development.

The vision for the facility is to provide the players (men and women), coaches and administrators with every opportunity to be successful, both on and off the field.

In addition to the delivery of the high performance outcomes, it is imperative that the facility has the ability to further develop and foster community partnerships and programs.

The indicative timeframes for the Site Assessment Process are as follows:

Milestone or Activity	Timeframe
Confidential discussions between the State and Greater Hobart councils	August/September 2023
Development of options with Council(s) including public consultation	October/November 2023
Comparative assessment of sites supported 'in principle' by councils	December 2023
Negotiate, finalise and formalise an agreement for the preferred site	January/February/March 2024

2 Introduction

2.1 Project Background

On 3 May 2023, the Tasmanian Government signed a Club Funding and Development Agreement with the Australian Football League (AFL) for the establishment of a Tasmanian-based AFL and AFLW Club.

A High Performance Centre has been identified as a key requirement to support the Tasmanian AFL licence and to underpin the attraction and retention of high-quality players, coaches, support staff and administrators.

The establishment of a high performance centre forms part of the Tasmanian Government's agreement with the AFL and the AFL Licence Agreement states that the Tasmanian Government will:

- Determine the location of the facility at a location in close proximity to Hobart central business district; and
- Use best endeavours to achieve practical completion of the facility on or before 31 December 2025.

On 27 June 2023, the Tasmanian Government allocated up to \$60 million for the delivery of the project, with a further \$10 million to be contributed by the AFL.

The Tasmanian Government has worked closely with the AFL Taskforce to undertake an options analysis of potential sites in the Greater Hobart area and to develop a preliminary masterplan and cost estimates. This work undertaken in mid-2022, informed the decision on a licence for a Tasmanian AFL team.

2.2 Expected Outcomes

The expected outcomes from the successful delivery of a High Performance Centre are:

1. A facility that enables the recruitment and retention of high-quality players, coaches, support staff and administrators.
2. Includes external and internal training facilities as well as office spaces and facilities for football and administrative staff.
3. Becomes the home to Tasmania's AFL, AFLW, VFL and VFLW teams and when required, will become the Southern Hub for underage pathway development.

In addition to the delivery of the high performance outcomes, it is imperative that the facility has the ability to further develop and foster community partnerships and programs.

2.3 Vision for the High Performance Centre

The vision for the facility is to provide the players (men and women), coaches and administrators with every opportunity to be successful, both on and off the field. In addition, the facility is anticipated to be accessed for community use purposes.

2.3.1 Community Use

The ability to further develop community partnerships and programs is a crucial element of the vision for the facility.

The new facility will create opportunities to provide for multiuse sports training and development that is accessible to the community and other Tasmanian sportspersons, whilst meeting the specific requirements of the AFL club.

The new facility will be planned and designed in a manner to ensure that “elite” use and “community use” can be achieved in harmony.

The functions and extent of community use will be evaluated during the project’s planning phase.

2.4 Site Requirements

From a technical perspective, the AFL Licence Agreement outlines a range of requirements for a site. This together with the State’s understanding of the AFLs requirements, indicates that a site must be chosen that:

- is in close proximity to Hobart central business district
- can accommodate a 9,000 m² facility (in a footprint of approximately 7,000 m²) as well as a full sized MCG sized oval (162 m x 139 m or approximately 22,000 m²)
- provides access to an additional grassed training area (preferably an oval) adjacent or close by to the main oval
- provides parking for approximately 150 cars
- allows for a north/south orientation of the main field
- should not be unduly influenced by weather (e.g. wind).

2.4.1 Functional Relationships

- The proximity of the Training and Administration Facility to the full sized, MCG dimension fenced oval is of high importance, and proximity of the Training and Administration Facility to the secondary oval / grassed training area is desirable but of lesser importance.
- The location of the car park (some if not all) in proximity to the Training and Administration Facility is also important.

2.5 Site Selection Objectives

The objectives of the site selection process are:

- To secure a site to build the High Performance Centre that meets the technical requirements of the AFL Licence Agreement.
- To secure a site that has broad support from the Council / Local Government in which the site is located.
- To secure a site that minimises the disbenefits to the community (noting that any site chosen is likely to have ‘impacts’ but these may be able to be offset).
- To secure a site that maximises the benefits to the local community that a national scale and quality facility will bring.
- To select a preferred site by December 2023 and secure this site by March 2024.

2.6 Site Selection Principles

A site has not yet been selected for the High Performance Centre. While prior studies were undertaken to examine potential locations, this was done confidentially and without extensive consultation due to the commercial nature of discussions taking place at the time.

The Tasmanian Government recognises that selecting a site is about more than the technical merits of the land on which the High Performance Centre would be located; it is also about the surrounding community and the level of support such a facility would have at any chosen location.

As such, the State is seeking to undertake a site selection process that includes working collaboratively with councils to explore site options.

The site selection principles to be adopted are as follows:

- Voluntary – Councils are given the opportunity to opt-in to the process by providing their support to explore options within their municipality.
- Flexible – the site selection process needs to be able to adapt to the feedback from local communities and their representatives.
- Commercial – the site selection process needs to be considerate of the commercial aspects of the facility and thus while transparency will be aimed for, in some cases this will not be possible.
- Equal – the State wishes to give councils equal opportunity to put forward sites within their municipality to host the High Performance Centre. Each site will have its own advantages and disadvantages to be considered. The State recognises that the development of the High Performance Centre in any municipality would be a significant boost to that municipality's economy.

3 Site Selection Strategy

The State is seeking to partner with a local government council that wants to be the home of the Tasmanian AFL/AFLW Club.

In addition to the benefits offered broadly to the communities throughout Tasmania, the High Performance Centre will offer substantial benefits for the local community that it is located in.

The site selection strategy is broken into five elements:

1. **Initial Technical Appraisal of Sites** – a process was undertaken, with minimal stakeholder engagement, to explore potential technically viable sites across the greater Hobart area with 13 locations explored as part of this assessment. This process was not deemed to be site selection; rather it provided an indication of opportunities for siting of the facility and enabled identification of sites considered to have a high potential of meeting the needs of the Tasmanian AFL/AFLW Club.
2. **Council Approach** – each of the councils within the greater Hobart area (Glenorchy, Hobart, Clarence, and Kingborough) were approached by the State to determine their interest in exploring options for sites to host the High Performance Centre in their municipality. The sites considered to have a high potential of meeting the needs of the Tasmanian AFL/AFLW Club were identified to these councils but the invitation was left for councils to identify any alternative sites. In advance of these meetings, Brighton Council had already proactively approached the State regarding a potential site within its municipality.
3. **Development of Options** – for any council that indicated interest in exploring site options, the State would work with that council to detail up the option for further engagement and to explore the benefits and disbenefits the option(s) present. It is anticipated that this phase would include public consultation lead by council to determine the level of community support for the option being considered. Final 'in principle' support would need to be provided by council for any option that was to be taken forward to the next phase.
4. **Comparative Assessment** – a comparative assessment against a set of assessment criteria would be undertaken to identify the preferred site for the High Performance Centre.
5. **Finalise Site Selection** – confirming and formalising the selection to enable the progression of the development on that site.

This strategy has been developed to enable the State to work collaboratively with councils to explore options and is informed by the Principles outlined in Section 2.6.

The following sections provide more detail on each of these steps.

3.1 Initial Technical Appraisal of Sites

In the period leading to June 2022, the State (through its consultants) completed a process to identify and assess potential sites based on their technical merit.

Thirteen sites were identified across the Greater Hobart area and these sites were assessed and a short-list of sites that had high potential of meeting the needs of the Tasmanian AFL/AFLW Club were identified. No selection of a preferred site was undertaken at this time.

3.2 Council Approach

Confidential discussions between the State and Greater Hobart councils commenced in mid-August 2023.

The State is committed to working with a council to explore options for a site if a council identifies a site that:

1. the project considers has a high potential of meeting the needs of the Tasmanian AFL/AFLW Club; and
2. may be acceptable to the community if associated impacts are carefully considered and managed.

Discussions have been held with Greater Hobart councils (Glenorchy, Hobart, Clarence, Kingborough) and with Brighton Council to date.

The discussions have outlined to each council any sites within their municipality considered to have a high potential of meeting the needs of the Tasmanian AFL/AFLW Club, but these discussions left the option for councils to present alternative options for consideration.

The intention at this stage is for each council to consider their views on potential sites, discuss associated impacts and potential mitigations and consider what the site's stakeholders and local community opinions may be, without the time, expense and potential community angst that may result from undertaking formal engagement at this early stage.

3.3 Development of Options

If an option is identified that shows promise of good outcomes for the community and AFL, the council may choose to develop and explore that option further in consultation with any impacted stakeholders and the broader community, and with the support of the Department of State Growth's High Performance Centre project team.

If a council requires the assistance of an external consultant to develop a concept plan for the site and/or concept plan for mitigation of an impact (e.g. relocation of an existing use), then the State may be prepared to fund associated consultancy costs, reasonably incurred by the council. Alternatively, the State may be prepared to procure a suitable consultant to perform the services with direction from both the project and council representatives. However, in both cases, the scope and cost of such services would need to be discussed and agreed in advance of the services being procured and delivered.

Through this process, options to manage and mitigate community impacts or compromises may be developed, along with opportunities to maximise the benefits resulting from the proposed site becoming the new home of the Tasmanian AFL/AFLW Club.

With a sound understanding of benefits and disbenefits of the option(s) explored, the council may reach a final determination as to whether it wishes to support 'in-principle' the use of the proposed site, and would be in an informed position to identify to the State the general terms and conditions that would apply to the use of the site.

3.4 Comparative Assessment

For each of the options that have 'in-principle' support from an associated council, a comparative assessment will be undertaken by the State, in consultation with AFL and the Tasmanian ALF/AFLW Club.

This assessment will be undertaken against a set of assessment criteria. A set of indicative criteria are included in Section 4.

This will allow for the selection of the optimum option that meets the needs of the AFL, the AFL/AFLW Club and the community and takes into consideration the actions required to manage impacts, deliver the desired benefits and comply with the applicable terms and conditions.

3.5 Finalise Site Selection

Once a preferred site is identified through the prior process, the State will then seek to negotiate, finalise and formalise the agreement regarding the preferred option with the council.

4 Indicative Assessment Criteria

The State's assessment criteria may include:

Criteria	FINANCIAL AND COMMERCIAL
1	Base Capital Cost
2	Funding Opportunities
3	Ongoing Costs
4	Commercial Revenue Streams and Sponsorship Opportunities
Criteria	CONNECTION TO STAKEHOLDERS
5	Proximity to Key infrastructure
6	Potential Community Partnerships
7	Access to supporter and membership base
8	Access to Transport Infrastructure
Criteria	ACCESS & FACILITIES
9	Priority access to state-of-the-art training facilities
10	Priority access to two training field(s) and potential impact on existing tenants
11	Quality of facilities located within the precinct
12	Potential to grow
13	Ability to locate all facilities (buildings and two fields) at one site
14	Ability to accommodate for pathway programs
Criteria	GOVERNMENT & PLANNING
15	Level of Landholder Support
16	Planning Process
Criteria	RETENTION & RECRUITMENT
17	Player Retention and Recruitment
18	Staff Retention and Recruitment
Criteria	SITE DETAILS
19	Land Tenure - Buildings
20	Site Features
21	Strategic Implications

5 Site Selection Timeframe

The table, below outlines the indicative timeframes leading to an agreement for a site being in place.

Milestone or Activity	Timeframe
Initial identification and technical appraisal of sites	June 2022
Confidential discussions between the State and Greater Hobart councils	August/September 2023
Development of options with Council(s) including public consultation	October/November 2023
Comparative assessment of sites supported 'in principle' by councils	December 2023
Negotiate, finalise and formalise an agreement for the preferred site	January/February/March 2024

6 Assessment Team

The State will undertake the assessment against the assessment criteria indicated in Section 4 with input from its consultant team, in particular:

- Planning – Ireneinc Planning
- Cost – WT Partnerships
- Specialist Sports Facilities – Waypoint

Recommendations for endorsement will be made to the project's Steering Committee, which contains representation from the AFL and the Tasmanian AFL/AFLW Club.

If endorsed by the Steering Committee, the Chair of the Steering Committee will refer the recommendation to the Tasmanian Minister for Stadia and Events for approval.

7 Consultation

It is acknowledged that councils will need to carefully plan and implement appropriate stakeholder and community consultation when identifying and evaluating potential sites, including assessing potential impacts and strategies for their management. While the State's project team will support councils in this regard, it is anticipated that councils will lead the site-specific consultation as councils are likely to have the more direct relationship with potentially impacted stakeholders and local community.

7.1 Confidentiality

The State and its project team will maintain appropriate confidentiality when dealing with information acquired by them in the performance of their duties when working with councils regarding sites and options.

For example, the State does not intend to identify the councils participating in this process, unless and until a council publicly identifies itself as participating in the process. Furthermore, the State does not intend to identify the potential site(s) that a council may be evaluating, unless and until a council publicly identifies the site(s) it is evaluating.

Information is confidential if it is gained while carrying out the associated duties, and:

- (i) it is marked "Cabinet in Confidence", "Commercial in Confidence", "confidential" or "strictly confidential" or conveyed with a statement by the person conveying it, that it is secret or conveyed in confidence; or
- (ii) is known to be confidential; or
- (iii) if the State might reasonably consider that the disclosure of the information to, or the use of the information by or in the interests of, any person other than itself or council (whether a particular person or other persons generally) would affect or prejudice the State's or council's commercial or financial interests.

The State and its project team are not obliged to keep confidential information:

- (iv) which is in the public domain through no default of the State and its project team (e.g. information that a council chooses to release as part of its community consultation); or
- (v) the disclosure of which is:
 - (A) required by Law;
 - (B) necessary or advisable to get a consent, authorisation, approval or licence from an Authority;
 - (C) necessary or advisable to disclose the confidential provision to a taxation or fiscal authority;
 - (D) necessary to disclose a confidential provision in answer to a question asked of a Minister in State or Federal Parliament or otherwise to comply with a State or Federal Minister's obligations to Parliament;
 - (E) given to a court in the course of proceedings to which the State or council is a party;
 - (F) given to a person for the purpose of obtaining professional advice, subject to that person being bound by confidentiality undertakings no less favourable than those stated above; or
 - (G) the State and council otherwise agree in writing can be disclosed.

8 Site Identification and Proposal Submission

While the State initially identified sites as part of prior work and sites that had high potential of meeting the needs of the Tasmanian AFL/AFLW Club were identified councils when meeting with them, the opportunity has been left open to individual councils to come forward with land that they think would be suitable to host the High Performance Centre.

As such, the identification of land rests with the councils themselves, as the landowners. To that end, a process to receive proposals from a council is required, which is outlined, below.

8.1 Receipt of proposals from councils

The site identification process is not intended to be a design competition.

Some sites will, by their nature, offer different benefits and disbenefits than other sites. The State will exercise reasonable endeavours to ensure that all participating councils have a fair opportunity to develop and understand suitable option(s) for proposed sites in cases where the site proposed has a high potential of meeting the needs of the Tasmanian AFL/AFLW Club.

However, it is important to note that the project proposes \$60 million to \$70 million of capital investment in the chosen municipality, offers long term employment opportunities for many people at the facility, and many other potential benefits for the local community.

Therefore, a council may choose to exercise its discretion to improve the attractiveness of its proposal to increase its likelihood of securing the High Performance Centre within its municipality, by:

1. accepting greater responsibility for implementing actions to manage associated impacts; and
2. offering terms and conditions that are more favourable than it would otherwise require.

Therefore, to protect the commercial interests of participating councils and to ensure the process is fair in this regard, written proposals are required to be submitted via email to highperformancecentre@stategrowth.tas.gov.au by 2.00pm on Thursday, 30 November 2023 (unless the State advises and extended date in writing to all participating councils) containing the following:

1. identification of the site
2. confirmation that the council supports 'in-principle' the use of the site for the purposes of the construction, operation and maintenance of the proposed AFL High Performance Centre, including its potential ancillary uses (e.g. community programs, sports merchandise shop etc.)
3. identified actions the council considers appropriate to mitigate impacts associated with the use of the site for the above purposes, and identification of the party proposed to be responsible for the cost and/or implementation of these action
4. proposed general terms and conditions that council would apply to the use of the site for the above purposes.

8.2 Consideration of expressions of interest from other landowners

While it is not anticipated that other landowners will be interested in offering properties under terms and conditions more favourable than may be offered by councils keen to become the home of the new AFL/AFLW Club, this possibility is not excluded.

The State will be receptive to expressions of interest from other landowners seeking to provide an offer as follows.

- Legal access to and use of property (e.g. by lease or sale) offer by the owner of that property.
- For the purposes of the construction, operation and maintenance of the proposed AFL High Performance Centre, including its potential ancillary uses (e.g. community programs, sports merchandise shop).
- For a period of time no less than 27 years from approximately December 2024.
- With the property offered to be more than five hectares in size and within 20 kilometres by road from the Hobart central business district.

However, the State shall be under no obligation to consider or respond to any or all expressions of interest received.

In the event that the State receives a proposal that the State assesses, at its sole discretion, as warranting further consideration, the State shall inform the participating councils of this unanticipated eventuality and reserves the right to alter the process described in this document to enable fair and equitable consideration of this proposal.



Phone: 1800 030 688

Email: highperformancecentre@stategrowth.tas.gov.au

Prepared by MGS Architects
2 October 2023

DRAFT

Preliminary DRAFT Clarence City Heart Plan



**CITY
HEART**

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Mosaic Lab | SGS Economics & Planning |
Movement and Place Consulting | Leigh Woolley |
Ireneinc Planning & Urban Design | WT Partnership



Clarence... a brighter place



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Economics
SGS Economics & Planning

Cost Management
WT Partnership

Document

Draft Clarence City Heart Plan

Date	Version	Authorised by
16.06.2023	Draft v1	VM
10.08.2023	Draft v2	VM
02.10.2023	Draft v2a	VM

Clarence City Council acknowledge the Tasmanian Aboriginal People as the traditional custodians of the land and pay respect to Elders past and present, and recognise their continuing spiritual connection to the land.

MGS Architects and the project team respectfully acknowledge the first nations and Traditional Custodians of the land on which we are located. We recognise their continuing connection to land, waters and culture. We pay our respects to their Elders past and present and extend this respect to all Aboriginal and Torres Strait Islander people.

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Clarence City Council have identified the need to develop a vision and strategy for the Clarence City Heart. This report, the Preliminary City Heart Plan, follows previous work developing key directions and concepts, and brings them together as Outcomes and Strategies to create a cohesive vision for the future of the City Heart.

The ideas in the Plan reflect what we have heard from the community and key stakeholders as well as building on our understanding of the context to make it a welcoming and vibrant place to work, live and play.

The City Heart Plan will place Clarence at the vanguard of a worldwide movement to build more liveable cities – cities that embrace and express their unique cultural, social and landscape values in an authentic manner. Liveable cities are stages for daily life accommodating a range of activities that add value to the quality of residents' lifestyles. Active, liveable cities also benefit local economies. People's positive image of a city centre as safe, sustainable, beautiful and vibrant correlates with a willingness to come, stay longer and spend more.

When they do, everyone benefits. While there are challenges for achieving these outcomes in City Heart, the Plan aims to embrace the diversity of the community and create the pathway for a sustainable and inclusive 'heart' to grow and flourish.

Next steps

To finalise the City Heart Plan, MGS Architects and consultant team will develop an action plan to provide guidance on a range of next steps for the City Heart including further strategic planning to support the outcomes, identifying and building partnerships for delivery of concepts and in what ways council can effect change through advocacy. Together these actions will enable a linked-up process creating a pathway to realising the City Heart vision.





Executive Summary

Key drivers of the City Heart Plan

DRAFT

The City Heart Plan is shaped by several specific challenges and influences, which form the strategic 'key drivers' of the plan. Identifying and understanding these key drivers is essential for underpinning analysis of the place, its context and future opportunities. The following drivers represent critical issues that demand a strategic and layered response to establish the right conditions for success.



There is an imperative to protect and enhance urban biodiversity, build sustainable, climate-resilient places and infrastructure, and engage in planning that aims for sustainable outcomes.



The global challenge of climate change



There is a need to create opportunities for safe and meaningful connections to historic and contemporary Tasmanian Aboriginal culture and practices.

Engagement and expression of Tasmanian Aboriginal culture and heritage



Clarence's housing stock is dominated by larger homes (93% of dwellings). Diversifying and creating more medium density units and apartments in the City Heart will create more housing choice and help alleviate the affordability crisis, supporting our diverse and growing population.



+2,200
Demand for future medium/high density dwellings by 2041



The projected future growth in retail, health and government service jobs will require more floor space that should be designed to maximise connections and placemaking, making the City Heart a desirable and competitive workplace destination.

+1,040
Additional jobs by 2041



Creating an inclusive place that reflects the diversity, heritage and culture of the community.

Celebrate heritage and history



Digital by default, there is a need for digital infrastructure to be embedded in future planning and development to support the digital economy and workforce.

Digital by default



Improving safety and sense of place is critical to building a night time economy and making attractive destinations for visitors and community to enjoy.

Improving safety and sense of place



Improving connections between primary open spaces and other areas limit their use and value to the community.

Improve connections between primary open spaces and other areas



Ensuring Rosny Parklands can be enjoyed by everyone, including all people of ages and abilities, requires us to understand the values of the community today, while planning for future needs.

18ha of parkland in the City Heart area



Consider parking solutions that will improve access, benefit the attractiveness of the place, and enhance public transport and pedestrian experiences through a precinct-based transport, parking, and place approach. With 40% of land in Rosny Park CBD currently used for at grade car parking, there is a significant opportunity to free up valuable land for community benefits.

Better parking solutions

The Vision for the City Heart Precincts and outcomes

DRAFT

The City Heart is made up of interconnected precincts, each with its own vision and character. The plan aims to leverage existing strengths and strategic opportunities in each precinct to meet the needs and aspirations of the people who live, work, and visit the City Heart. Six outcomes guide the plan and address the evolving needs of the Clarence community.



Outcome 1
A **living** City Heart on palawa country that recognises and celebrates Country and connects us to our unique sense of place, history, and culture



Outcome 2
A **resilient** and **sustainable** City Heart that values and connects with our natural environment



Outcome 3
A **vibrant** City Heart that nurtures our economy, arts and culture, opportunities, and experiences



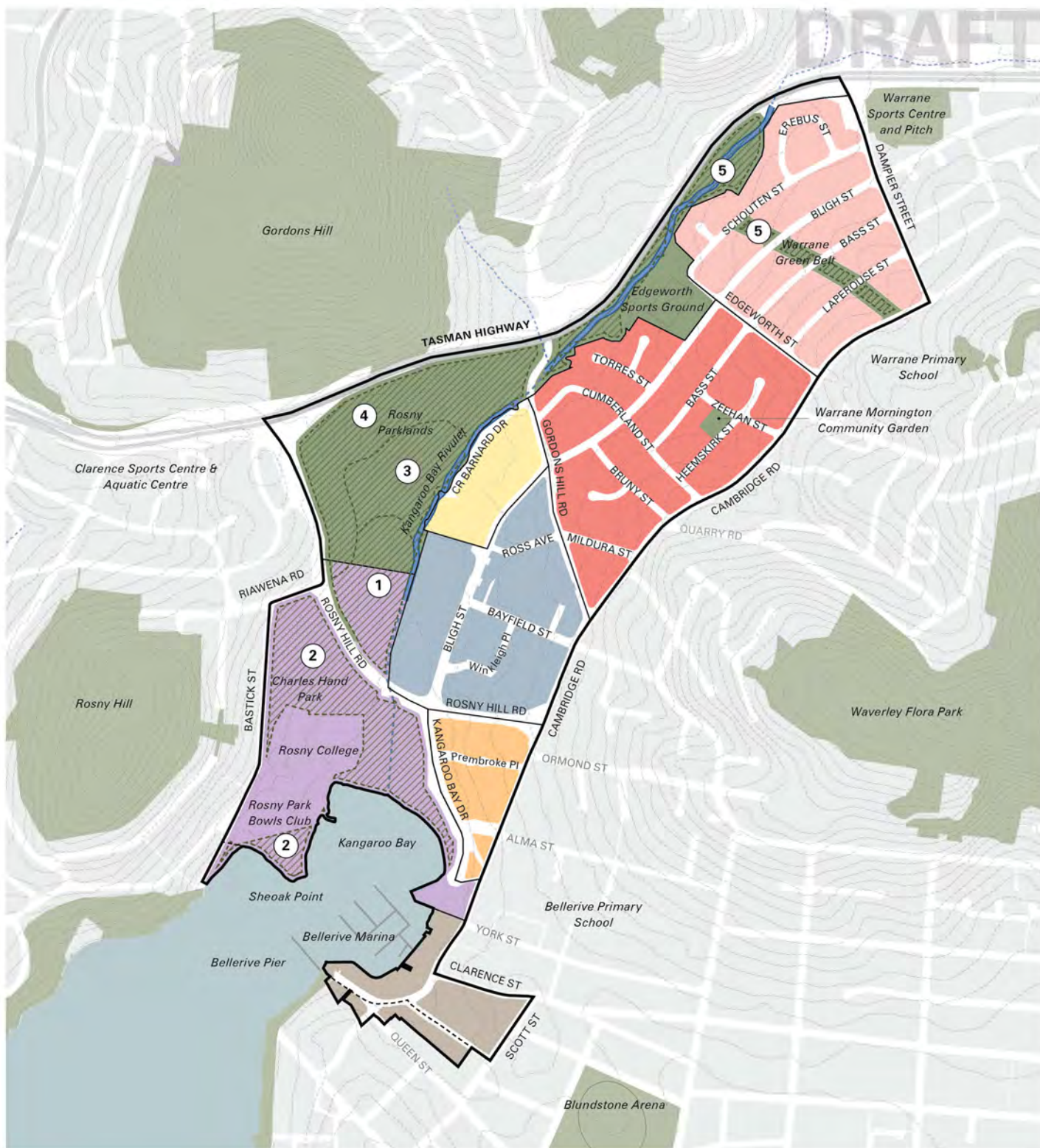
Outcome 4
An **inclusive** City Heart that offers diverse housing choices and affordable housing



Outcome 5
A **liveable** City Heart that provides great places and spaces designed for the whole community to enjoy



Outcome 6
A **connected** City Heart that links people and destinations with a network of high-quality streets, promenades, and pathways for all transport modes



Precincts

Legend

- Revised Study Area
- Old Study Area
- Precinct Boundary
- Open space and landscape
- Kangaroo Bay Rivulet (open)
- Kangaroo Bay Rivulet (closed)

City Heart Precincts

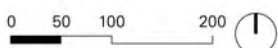
- Rosny Parklands*
- Civic, Community & Employment
- Arts, Culture & Leisure
- Town Centre
- Kangaroo Bay
- Bellerive Village
- Inner Warrane**
- Outer Warrane

Parks of Rosny Parklands

- ① Arts & Play Park
- ② Active Park
- ③ Civic Park
- ④ Wild Park
- ⑤ Warrane Parklands

*The Rosny Parklands Precinct overlaps areas of the Arts, Culture & Leisure Precinct where there is a park.

**Inner Warrane includes a small area of Bellerive.



Executive Summary

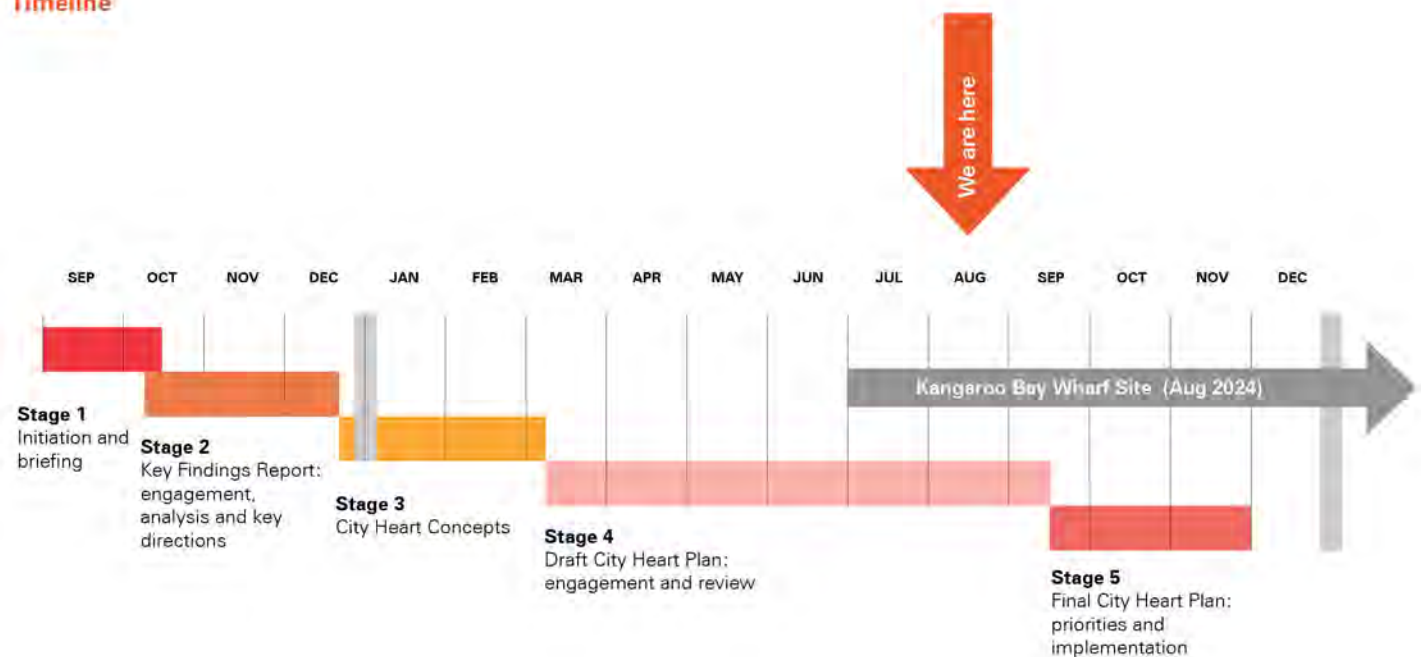
Next steps

Feedback from previous engagements with the community are captured in callout boxes throughout the City Heart Plan.

The next stage of the project focuses on consultation and mapping out an approach to implementation and priorities.

To find out more and provide feedback go to yoursay.ccc.tas.gov.au/cityheart

Timeline



Introduction

1

The City Heart

Today

The palawa (Tasmanian Aboriginal people) are the traditional and original owners of lutruwita (Tasmania) on which the City Heart is located. The City Heart area (see Map 2) includes the land surrounding Kangaroo Bay and stretches from the head of the bay to the north following Kangaroo Bay Rivulet. It encompasses Rosny Park, Clarence's principal activity centre, the south-west portion of the residential suburb of Warrane, Bellerive Village and the master-planned Kangaroo Bay precinct and significant swathes of public open space including the Rosny Park land historically used for golf.

Key places of the City Heart include:

- A** Rosny Park principal activity centre (CBD)
- B** Rosny Parklands
- C** Rosny Farm and Barn
- D** Kangaroo Bay Precinct
- E** Bellerive Village
- F** Charles Hand Park
- G** Clarence City Council Chambers and Lawns
- H** Sheoak Point
- I** Warrane Potential Urban Renewal Area
- J** Kangaroo Bay Rivulet
- K** Rosny Park Bus Mall



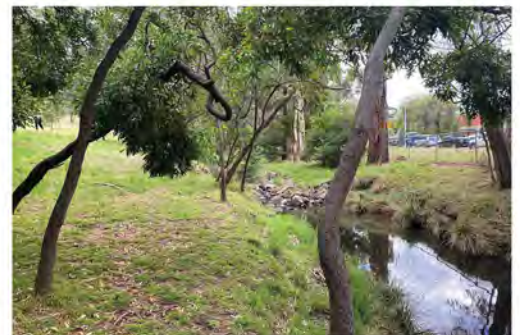
View of Rosny Park CBD, Rosny Parkland towards Rosny Hill and Kangaroo Bay



Kangaroo Bay boardwalk



Rosny Farm and Barn



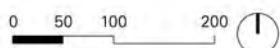
Kangaroo Bay Rivulet (source: Inspiring Place)



The City Heart

Key places

- | | | |
|-------------------------------------|---|---|
| A Rosny Park Activity Centre | 01 Eastlands | 01 Clarence Cricket Club |
| B Rosny Parklands | 02 Rosny Library | 02 Rosny Skate Park |
| C Rosny Farm & Barn | 03 Bellerive Yacht Club | 03 Rosny Park Tennis Club |
| D Kangaroo Bay Precinct | 04 Bellerive Marina | 04 Rosny Park Bowls Club |
| E Bellerive Village | 05 Bellerive Community Arts Centre | 05 Edgeworth Sports Ground |
| F Charles Hand Park | 06 Bellerive Police Station | 06 Warrior Park and Warrane Sports Centre |
| G Council Chambers & Lawns | 07 Warrane Mornington Neighbourhood Centre | 07 Clarence Sports Centre & Aquatic Centre |
| H Sheoak Point | 01 Rosny College | 08 Blundstone Arena |
| I Warrane | 02 Corpus Christi Catholic School | 09 Warrane Green Belt |
| J Kangaroo Bay Rivulet | 03 Bellerive Primary School | 10 Warrane Mornington Community Garden |
| K Bus mall | 04 Warrane Primary School | 06 Derwent Ferry / Bellerive Pier |
| | 05 Rose Bay High School | |
| | 06 The Cottage School | |



Key Drivers of the City Heart Plan

A number of specific challenges and influences are emerging as key drivers for the City


Heart Plan. Together with the analysis of place and context, these key drivers underpin the questions and outcomes that are central to the City Heart Plan. The key drivers are critical issues that require a layered, strategic and long term response to create the right conditions for success:

- 


The global challenge of climate change and the need to protect and enhance urban biodiversity, and build sustainable, climate resilient places and infrastructure, required planning for sustainable outcomes.
- 


Improving safety and sense of place is critical to building a night time economy and making attractive destinations for visitors and community to enjoy.
- 

Engagement and expression of Tasmanian Aboriginal culture and heritage is a way to create opportunities for safe and meaningful connections to historic and contemporary Tasmanian Aboriginal culture and practices.
- 


Improving connections between primary open spaces and other areas limit their use and value to the community.
- 

Diversifying and creating more medium density units and apartments in the City Heart will create more housing choice and help alleviate the affordability crisis, supporting our diverse and growing population.
- 

Ensuring Rosny Parklands can be enjoyed by everyone, including all people of ages and abilities, requires us to understand the values of the community today, while planning for future needs.
- 

The projected future growth in retail, health and government service jobs will require more floor space that should be designed to maximise connections and placemaking, making the City Heart a desirable and competitive workplace destination.
- 

Consider parking solutions that will improve access, benefit the attractiveness of the place, and enhance public transport and pedestrian experiences through a precinct-based transport, parking, and place approach.
- 

Celebrate heritage and history by creating an inclusive place that reflects the diversity, heritage and culture of the community.
- 

Digital by default, there is a need for digital infrastructure to be embedded in future planning and development to support the digital economy and workforce.

Planning for sustainable outcomes



The Vision for the City Heart

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The City Heart is on palawa Country, framed by the parklands and water of Kangaroo Bay, and with views to kunanyi beyond. It is the social, economic, and cultural centre of Clarence. It is a thriving and vibrant village where we live and work; learn and play; access shops and services; connect with nature and community. The City Heart is a diverse place, where we are all welcome to create and celebrate culture; build local prosperity and resilience.

lege

y Farm
al Precinct

Civic and Community Hub

Rosny Parklands

Kangaroo Bay Rivulet

Tasman Highway

2

ARTISTS IMPRESSION

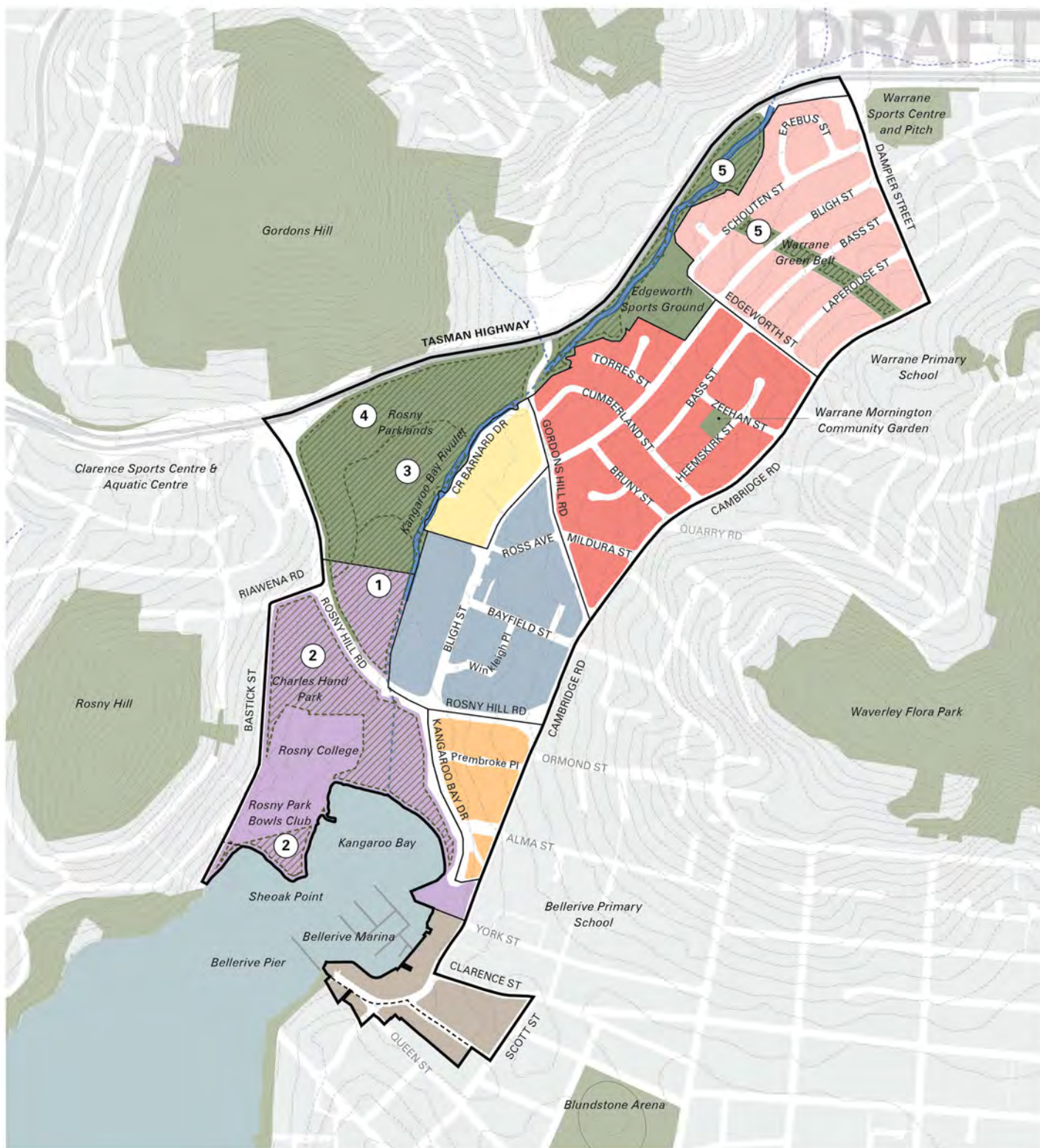
The City Heart is made up of interconnected precincts, each with its own vision and character. The plan aims to leverage existing strengths and strategic opportunities in each precinct to meet the needs and aspirations of the people who live, work, and visit the City Heart. Six outcomes guide the plan and address the evolving needs of the Clarence community.

City Heart Precincts

Civic, Community & Employment	<i>The civic and community centre of the City Heart — a revitalized hub for civic and government services and employment, nestled alongside Kangaroo Bay Rivulet, connecting the Town Centre with Rosny Parklands.</i>	Local and State government services, offices, community uses
Arts, Culture & Leisure	<i>A regionally significant arts and cultural destination at Rosny Parklands and Kangaroo Bay — a place to celebrate the City Heart's rich history, showcases contemporary arts and music, and supports a thriving night-time economy, providing opportunities for leisure and active living.</i>	Arts and cultural, hospitality, events, education, sports and recreation and medium density residential (up to 3-4 storeys)
Town Centre	<i>The centre of the City Heart – a vibrant mixed-use destination offering retail, hospitality, entertainment, health, services, and employment. It supports a thriving night-time economy and provides opportunities for urban living.</i>	Hospitality, retail and mixed use, local government services, health and high density residential above ground floor (up to 6 storeys*)
Kangaroo Bay	<i>A waterfront village destination — a great place to live and visit for recreation and events alongside a tree-lined bayside promenade with views across the bay and harbour to kunanyi and Central Hobart.</i>	Medium and high density residential and mixed use (up to 4 storeys), retail and mixed use
Bellerive Village	<i>A welcoming gateway to Clarence Heart and the Bellerive Community — offering an intimate main street and waterfront promenade shopping, dining, entertainment and living experience.</i>	Retail and mixed use, hospitality.
Inner Warrane	<i>A high-quality and resilient residential neighbourhood — an affordable place to live offering diverse choices of housing, meeting local needs, close to Town Centre and convenient connections to Rosny Parklands.</i>	Low to medium density residential up to 3 storeys, noting that 4 storeys** may be achieved on larger consolidated sites.
Outer Warrane	<i>A green neighbourhood — an affordable and diverse place to live close to education and Warrane Greenbelt.</i>	Low to medium density residential up to 2 storeys.
Rosny Parklands	<i>Rosny Parklands will offer diverse outdoor spaces that evoke the spirit of Clarence - embracing our cultural origins and revelling in the contemporary expression of a place where everyone can participate in a healthy and active life, celebrating the arts and the outdoors within an ecologically diverse setting and gathering together as one community with a shared future.</i>	

* The proposed heights for Town Centre is consistent with the Clarence Planning Scheme (20 metres).

** The proposed heights for Inner Warrane would require re-zoning to Inner Residential (9.5 metres) with 4 storeys subject to satisfaction of performance criteria.



Precincts

Legend

- Revised Study Area
- Old Study Area
- Precinct Boundary
- Open space and landscape
- Kangaroo Bay Rivulet (open)
- Kangaroo Bay Rivulet (closed)

City Heart Precincts

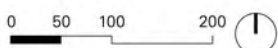
- Rosny Parklands*
- Civic, Community & Employment
- Arts, Culture & Leisure
- Town Centre
- Kangaroo Bay
- Bellerive Village
- Inner Warrane**
- Outer Warrane

Parks of Rosny Parklands

- ① Arts & Play Park
- ② Active Park
- ③ Civic Park
- ④ Wild Park
- ⑤ Warrane Parklands

*The Rosny Parklands Precinct overlaps areas of the Arts, Culture & Leisure Precinct where there is a park.

**Inner Warrane includes a small area of Bellerive.



The City Heart Plan consists of six outcomes that work together to realise the vision for the City Heart. These outcomes encompass the diverse and interconnected opportunities present throughout the City Heart area, forming a clearly expressed set of outcomes to guide the planning process and address the evolving needs of the Clarence community. Each outcome is supported by strategies that outline how each outcome will be achieved.



Outcome 1
A **living** City Heart on palawa country that recognises and celebrates Country and connects us to our unique sense of place, history, and culture



Outcome 2
A **resilient** and **sustainable** City Heart that values and connects with our natural environment



Outcome 3
A **vibrant** City Heart that nurtures our economy, arts and culture, opportunities, and experiences



Outcome 4
An **inclusive** City Heart that offers diverse housing choices and affordable housing

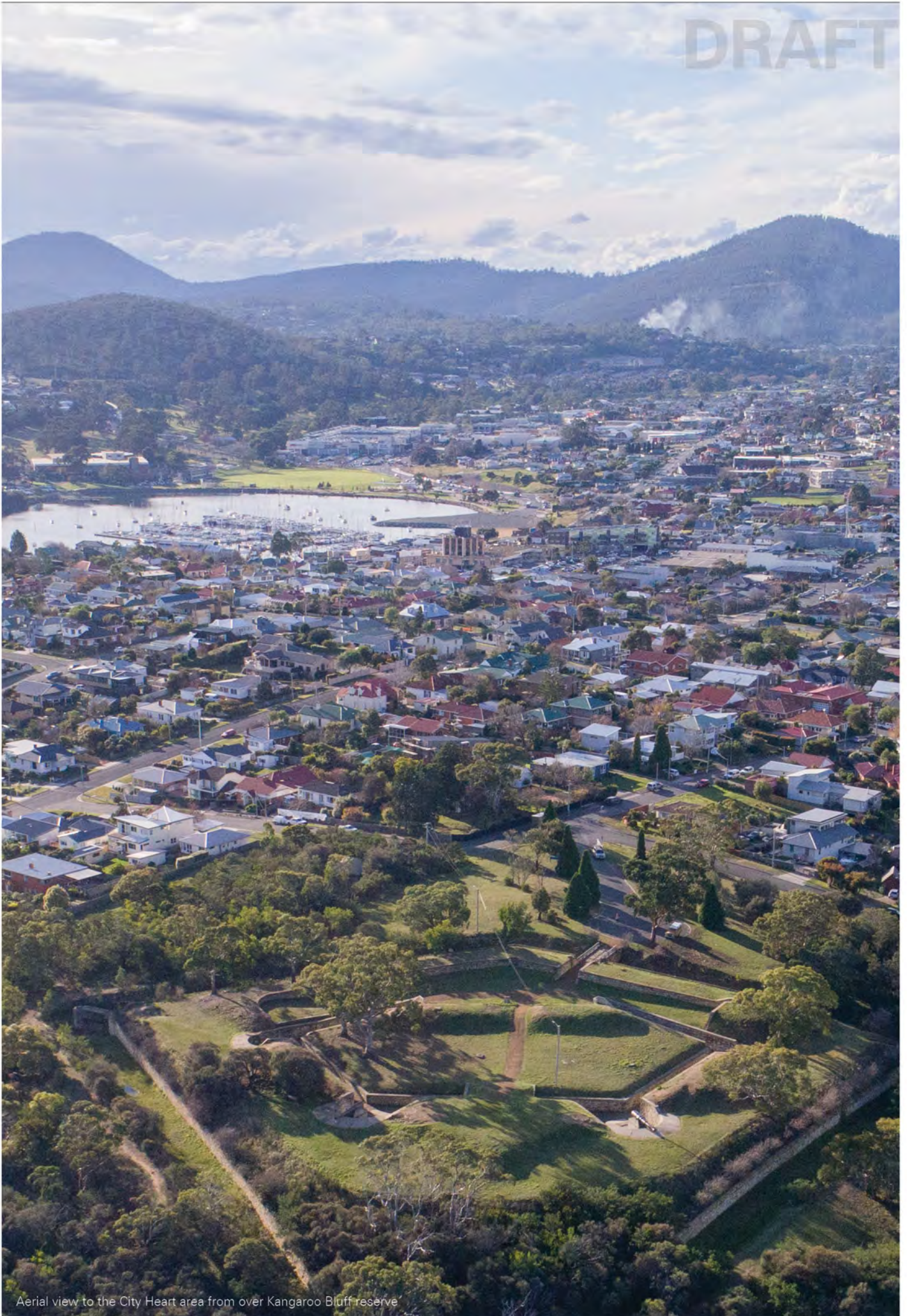


Outcome 5
A **liveable** City Heart that provides great places and spaces designed for the whole community to enjoy



Outcome 6
A **connected** City Heart that links people and destinations with a network of high-quality streets, promenades, and pathways for all transport modes

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Aerial view to the City Heart area from over Kangaroo Bluff reserve



A **living** City Heart on palawa country that recognises and celebrates Country and connects to our unique sense of place, history, and culture

These strategies aim to foster meaningful engagement with the Tasmanian Aboriginal community in the City Heart area. By establishing culturally appropriate protocols for consultation, increasing and recognising Tasmanian Aboriginal cultural and natural values, and encouraging sustainable resource management, these strategies seek to protect and promote Tasmanian Aboriginal heritage and cultural values. Additionally, it aims to create culturally safe gathering spaces for sharing Tasmanian Aboriginal values and history, and promote the dual-naming of culturally significant locations.

Strategies

- 1.1** Engage with the Tasmanian Aboriginal community to define and formalise a culturally appropriate protocol for guiding Tasmanian Aboriginal community consultation to implement recommendations of the City Heart Plan.
- 1.2** Increase Tasmanian Aboriginal cultural and natural values in the City Heart.
- 1.3** Create future projects that support palawa/Tasmanian Aboriginal people to explore, regenerate, create and sustainably harvest cultural and natural resources within the City Heart.
- 1.4** Develop appropriate conservation and protection programs for registered Tasmanian Aboriginal cultural heritage sites and cultural values within the City Heart.
- 1.5** Create culturally safe gathering spaces that contain a mix of contemporary and traditional cultural elements, where Tasmanian Aboriginal values and history can be shared.
- 1.6** Increase the palawa kani (Aboriginal language) use in the City Heart area with a particular focus on dual-naming or renaming of significant cultural spaces.

Why is this proposed?

Community consultation with the Tasmanian Aboriginal people, conducted as part of this project, identified three key barriers to engaging Tasmanian Aboriginal people in 'Country.' During the consultation, the Tasmanian Aboriginal people referred to the City Heart area as 'Country.' These barriers include:

- 1** Clarence currently does not celebrate or understand the cultural elements of country.
- 2** Clarence does not give recognition to the full (Tasmanian Aboriginal) history of the area.
- 3** Clarence does not provide culturally safe environments for Tasmanian Aboriginal people to gather and engage in the general community.

Addressing these barriers requires an understanding of the Tasmanian Aboriginal definitions of 'Country,' 'Recognition of Country,' and the characteristics of a 'Culturally Safe Space.' Tasmanian Aboriginal consultant Sharnie Read has used the information gathered during the consultation process to outline these three areas of interest.

Incorporating these definitions of Tasmanian Aboriginal cultural elements will guide the development of future projects in the City Heart. By integrating these elements into project planning, it will be possible to engage the palawa community, increase Tasmanian Aboriginal involvement in the creation of culturally appropriate and beneficial outcomes, and showcase an understanding and acknowledgment of the significance of Palawa history. Moreover, these efforts will contribute to the creation of culturally safe spaces and help the wider community understand and appreciate the complete history of the area.

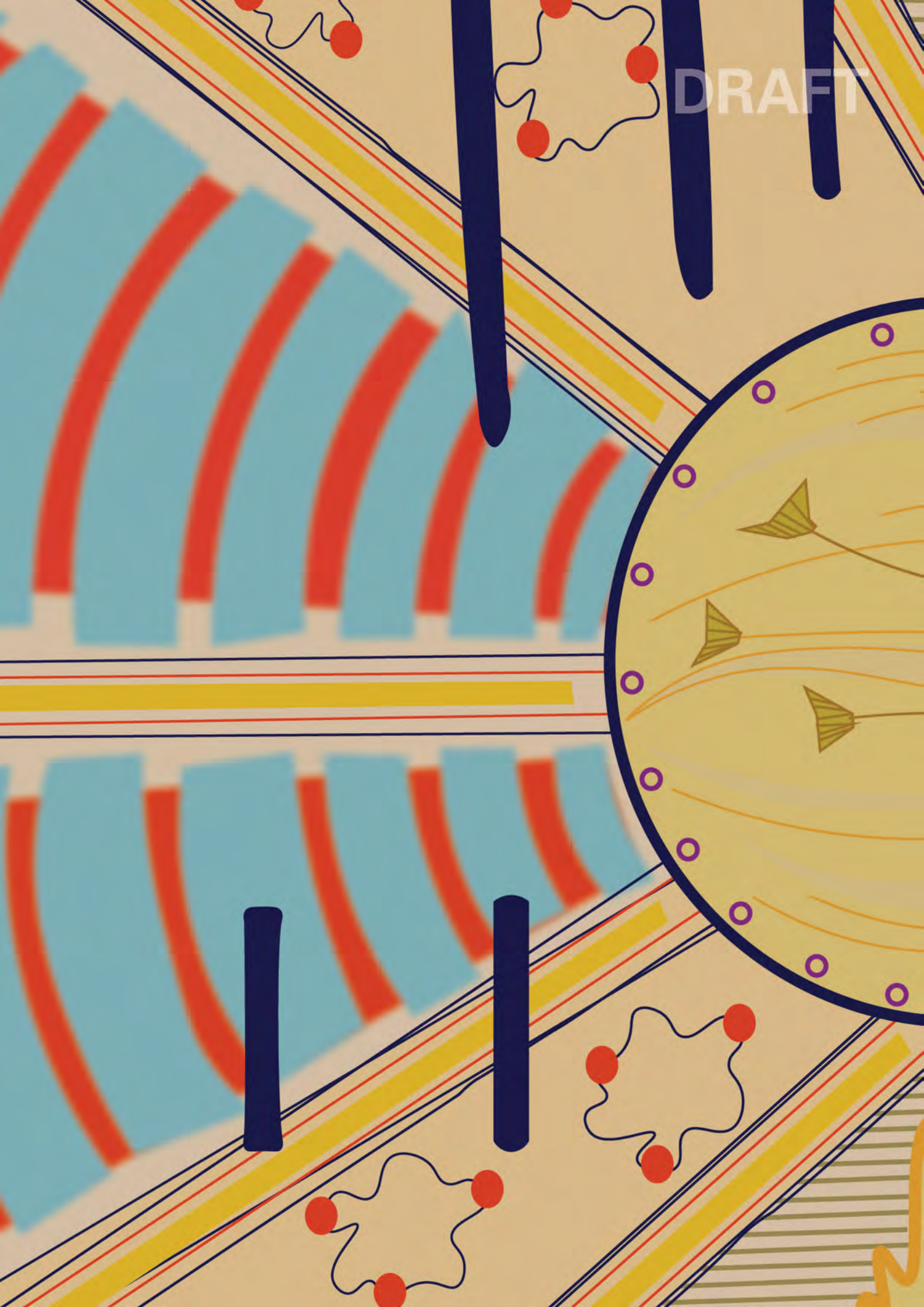


A living City Heart

- City Heart Study Area
- Open space and landscape
- Kangaroo Bay Rivulet (open)
- Kangaroo Bay Rivulet (closed)
- Bus mall
- Ferry
- Proposed
- Culturally safe gathering space
- Public art opportunity for expression and promotion of Aboriginal heritage and cultural values
- Cultivated ecology and increase native vegetation
- Kangaroo Bay Rivulet - increase native vegetation

Opportunities for expression of Tasmanian Aboriginal heritage and cultural values, and culturally safe spaces are identified throughout the City Heart Plan.

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For the consultation process the City Heart project area was referenced by Tasmanian Aboriginal people as *country*.

Country is a term used by palawa (Tasmanian Aboriginal people) that refers to the land, sea, and sky environments as a complete connective space. The term *country* includes the natural and built environment, plants, animals, water, and people.

For the purpose of engaging Tasmanian Aboriginal people in the project area we need to understand how to support the recognition of country from a Tasmanian Aboriginal perspective. The following notes describe elements of the cultural landscape of the project area and how we can use these elements in a culturally respectful manner.

Artwork (left)
"milaythina rayakana"
(country song) 2008,
Sharnie Read



For the purpose of engaging Tasmanian Aboriginal people in the project area we need to understand how to support the recognition of country. To better understand what this means from a Tasmanian Aboriginal perspective, I have provided a breakdown of the elements of the cultural landscape of the project area and how we can use these elements in a culturally respectful manner.

Elements of the cultural landscape are not limited to physical heritage sites as defined by the Aboriginal Relics Act 1975. However, Tasmanian Aboriginal heritage sites are important aspects of the cultural landscape. These heritage sites are used by palawa to gain insight, understanding and for educating others about traditional life in lutruwita (Tasmania). Tasmanian Aboriginal heritage sites provide insight to the traditional resources and form powerfully connective spaces where palawa people feel deeply connected to their ancestors through the physical aspects of the heritage that has been left.

For this reason, cultural landscapes containing the physical material/heritage are important and need to be protected and cared for. Tasmanian Aboriginal community members should be engaged in the ongoing management of all aspects of these important spaces.

Recognition of country must also provide avenues for highlighting other cultural elements of the space. Not all these elements will be traditional, or nature based. Some spaces will be defined by the contemporary community as gathering spaces. These are spaces where the palawa community feel culturally safe and welcome to gather, to carry out cultural activities and spaces where they can continue cultural practices. They may be enhanced by natural resources but can also be spaces that just facilitate the gathering of community members for the purpose of connecting with each other. For this reason, many open park spaces, areas skirting rivulets or natural waterways and coastal areas are defined by palawa as gathering spaces. Locations within contemporary built or urban scapes that either connect with or house elements of natural resources are favoured by palawa for gathering and are seen as culturally safe spaces.

Aerial shot looking north across Derwent valley and project area

TASMANIAN ABORIGINAL (PALAWA) DEFINITION OF COUNTRY:

Country is a term used by palawa (Tasmanian Aboriginal people) that refers to the land, sea, and sky environments as a complete connective space. The term *country* includes the natural and built environment, plants, animals, water, and people. The environment is considered as one with all elements connecting to a shared living rhythm, this connectivity is where Tasmanian Aboriginal people see their place in the living landscape, they are one of the elements within the rhythm of the country, not separate from it.

The term *country* is a reference to the connective space that they live within and provides an ongoing deep connection to Tasmanian Aboriginal spirituality and cultural identity, reinforcing the concept that Tasmanian Aboriginal people belong to country.

Recognition of *country* and recognition of the cultural elements of country is seen by palawa as supporting the spiritual connection they hold with the land. One way to pay respect to country can be through acknowledgment of Tasmanian Aboriginal peoples ongoing custodianship of the land and the deep history they have for caring for their country.

CULTURALLY SAFE SPACES AND ENVIRONMENTS:

For palawa Tasmanian Aboriginal people, culturally safe spaces are developed by creating an environment where Tasmanian Aboriginal people feel valued and respected.

Where Tasmanian Aboriginal people are not challenged regarding their cultural identity, where culture is recognised, celebrated, and supported.

A space where cultural knowledge can be shared and cultural practices are encouraged, supported, and protected.

A space where Tasmanian Aboriginal cultural and intellectual rights are acknowledged and adhered to in a way that provides understanding and respect for everyone regarding Tasmanian Aboriginal culture.

Culturally safe spaces provide Tasmanian Aboriginal people room to gather and interact with others in an authentic and culturally appropriate way.

Culturally safe spaces typically provide access to cultural resources and natural materials to facilitate ongoing cultural practices.

To address the outcome of community consultation where a barrier was identified as a lack of understanding of the cultural elements of country, a breakdown of cultural elements from a Tasmanian Aboriginal perspective can be defined as: family, shelter, food, water and fire.

PALAWA KANI: LANGUAGE PROTOCOLS

Further to support the concept of increasing the use of Palawa Kani Tasmanian Aboriginal language in the project area, there is a formal process for obtaining advice, and permission from the Tasmanian Aboriginal Centre and subsequently the palawa – Tasmanian Aboriginal people for the use of their language.

A process has been developed to provide the palawa community with an avenue to guide culturally appropriate use and expression of our language. Following this process also ensures a fully informed and accurate interpretation of the language/Palawa Kani words.

The Tasmanian Aboriginal Centre's Language unit workers are skilled educators of the language. The *Palawa Kani Unit* they have the capacity to guide the process to ensure accurate interpretation, appropriate use of the language as well as assisting in the formal process of approval/permission from the community.

The project area has a great many opportunities for dual or renaming of geographical spaces. The Kangaroo Bay Rivulet being an obvious cultural and natural feature of space, where dual or re-naming could be achieved.

tara layna.....kangaroo bay rivulet
tara (tah rah).....male kangaroo
layna (lie nah).....fresh water

CULTURAL ELEMENTS OF COUNTRY:



FAMILY

Referring to all elements of community from contemporary community use of a space today through to Tasmanian Aboriginal community history of an area. This would include the nature of the tribal group of that land. How many Tasmanian Aboriginal family groups (Bands) lived in this area, how was the project area used during traditional life and how has that now changed. Family and language is also important to recognise that we had many languages in lutruwita during tribal time but now use one language.

Tasmanian Aboriginal identity is formed through strong family networks and recognition of Tasmanian Aboriginal identity in contemporary society is strongly associated with the use of one language – Palawa Kani.

The dual naming of places such as kunanyi – Mount Wellington provides Tasmanian Aboriginal people with recognition of belonging. The use of Palawa Kani can assist in the sharing of cultural knowledge and a greater understanding of Tasmanian Aboriginal people in lutruwita – Tasmania.

Tribes in lutruwita formed relationships with other tribes and our traditional practise would see the woman migrate from one tribe into another to start families. This helped to establish strong ties with bordering tribal groups. These relationships were maintained and respected and can provide insight into how families lived. This information can help to define community/family use of a landscape and the tribal boundaries.

How the palawa community/families use the space today is defined as minimal. Palawa today feel there is a lack of cultural spaces, and this is a barrier for families and therefore it is not a space they feel supported to engage in. Most Tasmanian Aboriginal families will look to access other spaces outside of the project area to engage in community/family or cultural activities.

SHELTER

This element is multifunctional it can reflect a wider landscape value where natural shelter is provided for both tribal and contemporary cultural purposes.

Shelter can include hut depressions, cave sites and overhangs or cliffs in the landscape where Tasmanian Aboriginal people gathered for protection from the elements such as wind, rain, sun.

Many of these cultural spaces are located within the Meehan Rangers. This range is seen as a significant aspect of the cultural landscape associated and connected to the project area.

The other function of *shelter* in a contemporary environment refers to the creating of a culturally safe space. Culturally safe spaces shelter and protect Tasmanian Aboriginal cultural identity and facilitate the continuation of culture in many ways. These provide a physical space in which the sharing of knowledge can occur.

In a more contemporary aspect shelter can also be defined simply as the place you live at, your home. The lack of housing is considered a shared problem for all community groups. The lack of available and affordable housing is recognised as a need both within the project area and in the broader context of lutruwita.

FOOD

Understanding what cultural food and cultural resources are in a space or landscape is important to palawa people. These resources facilitate cultural practices and can assist in making landscapes healthy and engaging spaces for both Tasmanian Aboriginal and non-Aboriginal community groups. For this project we need to understand there are two defined spaces where cultural food is gathered. The breakdown of the two areas is inland resources and coastal resources. These areas are defined separately as they are managed differently by the seasons of harvesting, regeneration and resting.

The project areas importantly provide access to saltwater country and coastal landscapes. Kangaroo Bay sits on the banks of the Derwent River.

The area is known by palawa as timtumili minanya – (teem tu mee lee) (mee nah nyah) Tasmanian Aboriginal cultural sites such as the living spaces and shell mounds at Sheoak Point provide us with an understanding of the food resources gathered from the river system over thousands of generations. Shell middens on the banks of the river are estimated to represent five to six thousand years of occupation history and therefore form important lenses into the food resources that were harvested here. These spaces also create opportunities for palawa to be supported to develop culturally appropriate conservation and protection programs.

The inland resources vary depending on location and environment these environments include but are not limited to grass lands, woodlands, wetlands, animals, bird life, insects, and bush tucker. The food resources found in these environments are managed through understanding different cultural management approaches including practices such as cultural burning. Unfortunately, the changed landscape within the project area has impacted on a great majority of the inland resources and these environments.

Understanding what changes have occurred in the landscape and how they may have impacted on the cultural resources can help with regeneration of these resources. Providing support and assisting in creating culturally rich spaces within the project area will increase palawa participation in this space and can provide cross cultural knowledge sharing and networking.

WATER

Water is an important aspect of Tasmanian Aboriginal culture. Fresh water is seen as life both for Tasmanian Aboriginal people and for the land, it is essential for health and wellbeing as people and for healthy country. Healthy country can only be possible with healthy waterways. The provision of many of our cultural resources including plant, animal and bird resources are directly associated with our fresh waterways. Tasmanian Aboriginal gathering spaces are almost always in close proximity to fresh water. For this reason, the Kangaroo Bay Rivulet located within the project area is seen as a significant aspect of the cultural landscape. The Kangaroo Bay Rivulet presents many opportunities to acknowledge, celebrate and created a greater understanding of the cultural elements of the landscape. The rivulet is seen as a prime location for the rehabilitation of the cultural resources and for the creating of a culturally appropriate gathering space.

The project area is located within the seams of a traditional hunting ground. The rivulet being an important part of the cultural hunting landscape.

The rivulet provided living spaces and manufacturing spaces. This rivulet was used as a space to sit and make tools and for the creation of cultural materials such as manufacturing of animal skins for clothing and blankets, spears, waddies and stone blades, all basic requirements for hunting and processing of food.

The project areas also importantly provide access to saltwater country and the coastal landscapes. Kangaroo Bay sits on the banks of the Derwent River known by palawa as timtumili minanya – (teem tu mee lee) (mee nah nyah) Tasmanian Aboriginal cultural sites such as the living spaces and shell mounds at she-oak point provide us with an understanding of the food resources gathered from the river system over thousands of generations. Shell middens on the banks of the river are estimated to represent five to six thousand years of occupation history and therefore form important lenses into the food resources that were harvested here.

FIRE

Palawa have always used fire in all aspects of community life. Fire is an important aspect of ceremonies and is an avenue for the continuation of culture.

The use of fire in contemporary community is seen as a direct link to our ancestors and our ancient cultural practices. Spiritual expression, engagement and inclusion is achieved using fire.

The creation of hunting grounds, maintaining healthy country and the regeneration of bush tucker and other cultural resources are all reliant on fire. Cultural burning is no longer achievable in this project area due to the urbanisation of the landscape however it is still an important aspect of Tasmanian Aboriginal cultural values and of the history of the area. Tasmanian Aboriginal people see fire as an essential element of our culture and therefore it cannot be left out when consideration is given to the creation of culturally rich landscapes. It is essential for many cultural practices and for Tasmanian Aboriginal ceremony.

The Vision for the City Heart

Outcome 2 — A resilient and sustainable City Heart

8 in 10 told us that they thought the vision for resilience and sustainability was met.



A **resilient** and **sustainable** City Heart that values and connects with our natural environment

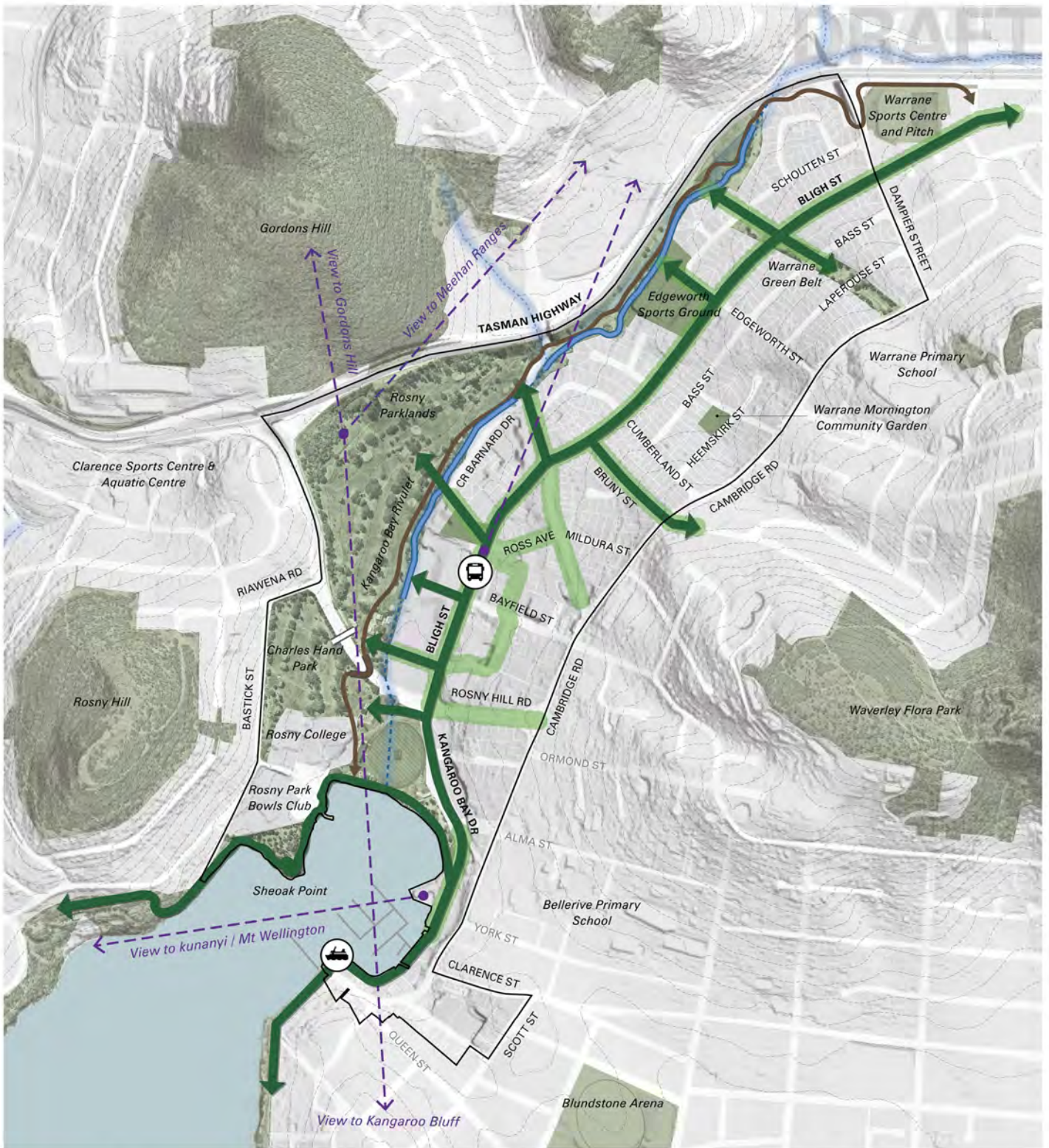
These strategies aim to create a resilient and sustainable City Heart by nurturing a deeper connection to the natural environment, embedding principles of sustainability and resilience into the design of the built environment and public spaces, while preserving established trees and incorporating regenerative design outcomes for open spaces like Kangaroo Bay Rivulet and Rosny Parklands to build a climate resilient community.

Why is this proposed?

The global challenge of climate change and the need to protect and enhance urban biodiversity, and build sustainable, climate resilient places and infrastructure, required planning for sustainable outcomes.

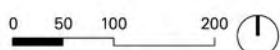
Strategies

- 2.1** Embed principles of sustainability (ESD), resilience and regeneration into the design of the built environment and public spaces.
- 2.2** Retain established trees and incorporate regenerative design outcomes for the Kangaroo Bay Rivulet, Rosny Parklands and other open spaces to promote urban biodiversity.
- 2.3** Increase green infrastructure in the City Heart by enhancing street canopy tree cover, encouraging the implementation of green roofs, walls, and facades on existing and new buildings, and incorporating water-sensitive urban design (WSUD) in the built environment and public spaces.
- 2.4** Integrate urban food production opportunities throughout the City Heart to promote community building, health, and wellbeing, and placemaking.
- 2.5** Integration of best practice design in flood prone areas



A resilient & sustainable City Heart

- City Heart Study Area
- Open space and landscape
- Kangaroo Bay Rivulet (open)
- Kangaroo Bay Rivulet (closed)
- B Bus mall
- F Ferry
- Bligh Corridor & connections to natural environments
- Kangaroo Bay Rivulet track
- ↔ Views to the landscape landmarks
- Indicative focus areas for street canopy planting



The Vision for the City Heart

Outcome 3 — A vibrant City Heart

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A **vibrant** City Heart that nurtures our economy, opportunities and experiences

These strategies aim to create a vibrant City Heart by identifying strategic opportunities for job intensification and diversification. They also focus on promoting and supporting arts and culture, capturing the evolving retail landscape, fostering a thriving night-time and visitor economy, and embedding digital infrastructure to support a flourishing community.

Strategies

- 3.1** Identify strategic opportunities for the intensification and diversification of employment, retail and hospitality, arts, events and cultural activities, health, education and wellbeing within the City Heart to foster a flourishing community.
- 3.2** Consolidate and expand the civic (including the Council Chambers site) and government services precinct, creating a vibrant Civic, Community, and Employment Precinct that caters to the needs of the community and attracts potential major head offices or government departments to relocate to the City Heart.
- 3.3** Capitalise on existing arts, culture, and entertainment activities to attract economic activity and promote a thriving night-time economy.
- 3.4** Encourage the redevelopment of the retail offer in the City Heart, focussing on a mix of uses, small businesses and street activation.
- 3.5** Leverage the existing strengths and opportunities to establish vibrant streets and well-connected public spaces that support work places and City Heart destinations by providing areas to meet, collaborate, and relax.
- 3.6** Embed digital infrastructure by default in future planning and development to improve connectivity and support innovation in enterprise, business, and government services.

Why is this proposed?

There is projected growth of **1,040 jobs** by 2041 in Bellerive-Rosny SA2, with significant growth in commercial, retail, hospitality, and institutional sectors, particularly healthcare. This indicates a potential demand for employment floorspace to increase by **32,600m²** by 2041. To effectively accommodate this growth, strategic opportunities for job intensification and diversification must be identified, ensuring a comprehensive response to the projected demand. Eastlands shopping centre serves as a key drawcard. However, the retail sector in Australia is changing, with retail accounting for a smaller share of the overall economy. This underscores the importance of capturing the evolution of retail and capitalising on the City Heart's art, cultural, and entertainment activities, promoting diversified and experiential-based retail. Enhancing the public realm is crucial to establish the City Heart as a desirable and competitive workplace destination. Together, increased activity within vibrant streets and spaces will support the growth of a night-time economy.

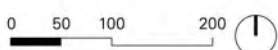


The plan seeks to support the creation of a night-time economy in the City Heart area | Bar Wa Izakaya, Hobart



A vibrant City Heart

- | | |
|-------------------------------|--|
| City Heart Study Area | Eastlands (major retail / strategic opportunity for transformation) |
| Open space and landscape | Retail mixed-use (encourage fine-grain and activated street frontages with residential development above ground level) |
| Kangaroo Bay Rivulet (open) | Residential mixed-use (support retail and hospitality uses at ground level to Kangaroo Bay Drive) |
| Kangaroo Bay Rivulet (closed) | Civic, community and employment (consolidate and expand) |
| Bus mall | Preferred night-time economy locations |
| Ferry | |
| School | |





An **inclusive** City Heart that offers diverse housing choices and affordable housing

These strategies aim to support an inclusive City Heart by facilitating a wide range of housing choices, including affordable and key worker housing, while creating a high-quality living environment with convenient access to services and a welcoming sense of place. They also emphasize the importance of well-designed infill housing in the right locations to cater to a diverse community.

Strategies

- 4.1** Support well-designed medium and high density housing in the right locations in the City Heart.
- 4.2** Support medium density infill development in Inner Warrane that sensitively responds to the existing low-rise character.
- 4.3** Ensure residential development in the Town Centre does not crowd out employment or space for arts and culture, particularly at ground floor by locating residential development at upper floors and provide affordable options to attract artists.
- 4.4** Partner with government to attract reinvestment in affordable housing pathways and additional supply of affordable and key worker housing for rent in alignment with the Tasmanian Housing Strategy.
- 4.4** Ensure new developments deliver high quality outcomes with embedded ESD principles and delivery of community benefits such as improved open space linkages and interfaces, community or active uses along street frontages and minimising impact of parking at ground level.

Why is this proposed?

The future demand for housing in Clarence is influenced by demographic changes, affordability, and the availability of housing in suitable locations, particularly for key workers. Currently, there is an undersupply of diverse and affordable housing options in the City Heart, which does not align with the community's immediate and future needs. According to SGS Economics modelling, there is a projected need for up to **2,200** additional infill units by 2041. It is crucial to support increased housing density in the right locations within the City Heart, such as near existing transport networks and the Town Centre, and on larger sites where off-site impacts can be managed effectively. There is sufficient capacity in the City Heart to deliver more housing, the key will be to deliver a wide variety of dense, attractive housing, combined with other initiatives to encourage liveability. This will need to be supported by a continued improvements to the built environment, deliver a mix of jobs and retail/leisure opportunities to the City Heart, and continue to engage with developers through the delivery of housing as part of mixed-use sites.



* Source: Clarence City Heart – City Heart Plan: Economic analysis, December 2022 by SGS Economics & Planning



A inclusive City Heart

City Heart Study Area

Open space and landscape

Kangaroo Bay Rivulet (open)

Kangaroo Bay Rivulet (closed)

Bus mall

Ferry

Infill housing opportunity areas

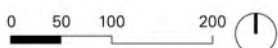
Low to medium density
(up to 2 storeys)

Medium density
(up to 3 storeys* lot consolidation encouraged)

Medium and high density residential
(up to 4 storeys)

High density mixed-use with residential above ground floor
(up to 6 storeys)

**4 storeys may be achievable on large consolidated sites,
subject to satisfaction of performance criteria.*





A **liveable** City Heart that provides great places and spaces designed for the whole community to enjoy.

These strategies aim to create a liveable City Heart by transforming Rosny Parklands, Charles Hand Park, Warrane Greenbelt, and Sheoak Point into a well-connected network of spaces along the rivulet. This will be complemented by a safe and enriched public realm with vibrant street life and a unique sense of place identity. The strategies also seek to embed design excellence and universal design principles in placemaking to ensure accessibility for people of all ages and abilities in future places and spaces.

Strategies

- 4.1** Transforming Rosny Parklands into a safe and valued linear network of wonderful programmed and natural open spaces is the goal. This network would link the hills and rivulet to Kangaroo Bay, enriching and connecting the two hubs of Rosny Park and Bellerive. The revitalisation of each hub would create high-quality day and night time places and spaces.
- 4.2** Create a vibrant, active and safe public realm with a unique sense of place and identity for the City Heart through placemaking, improved wayfinding and capitalising on parkland views. Encouraging active uses at ground level along new and upgraded streetscapes and public spaces
- 4.3** Embed design excellence and universal design principles to ensure that places and buildings are well-designed and accessible to all people, regardless of age, disability, background, and other factors.
- 4.4** Locate new facilities and parkland places to leverage existing infrastructure and nearby amenities and to improve safety and sense of activity day and night.
- 4.5** Identify opportunities for creating an inclusive place that reflects the diversity, heritage and culture of the community

Why is this proposed?

Rosny Parklands, spanning 18 hectares, surpasses the size of the Royal Tasmanian Botanical Gardens (~14ha). However, despite its size, this expansive open space is currently difficult to access and offers a limited range of uses. There is a significant opportunity to transform the parklands into a collection of well-designed and accessible parks that cater to a wider range of community uses and activities. Similarly, the Kangaroo Bay rivulet, with its fragmented trail network and limited bridge access, hinders the community's ability to access and enjoy the parklands from the Town Centre. By reimagining the parklands and rivulet as a connected blue and green corridor, complete with accessible trails and new bridges, the availability of diverse places and spaces for the community to enjoy would be greatly expanded.



Rosny Parklands

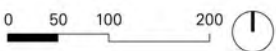


Royal Tasmanian Botanical Gardens



A liveable City Heart

- City Heart Study Area
- Open space and landscape
- Kangaroo Bay Rivulet (open)
- Kangaroo Bay Rivulet (closed)
- Bus Bus mall
- Ferry Ferry
- Rosny Parklands to Kangaroo Bay
- Kangaroo Bay Rivulet track
- Bligh Street Corridor and connections to parklands and improved public realm
- Public realm improvement focus areas
- Heart Rosny Park / Bellerive (activity hub)
- Views to the parklands





A **connected** City Heart that links people and destinations with a network of high quality streets, promenades and pathways for all transport modes.

These strategies aim to transform Bligh Street, Kangaroo Bay Drive, and Bellerive Esplanade into a green active transport corridor, upgrade and complete the Kangaroo Bay Rivulet track, better manage transport and parking, and establish a robust network of pedestrian and cycle paths throughout the City Heart. Upgrades to the streetscapes and improve public transport to improve safety, walkability, accessibility and amenity for all.

Strategies

- 6.1 Connect Bligh Street, Kangaroo Bay Drive and Bellerive Esplanade as a high-quality green active transport corridor.
- 6.2 Upgrade and complete the Kangaroo Bay Rivulet Track as a high quality shared active transport corridor (cycle/pedestrian).
- 6.3 Manage transport and aggregate car parking to support active and public transport, more convenient access to destinations, optimise parking utilisation and maximise land efficiency.
- 6.4 Increase the connectivity and accessibility of the City Heart's pedestrian network through new and upgraded accessible pathways, including bridges along the rivulet, and improved crossings.
- 6.5 Create a continuous and robust network of bicycle, e-bike and e-scooter lanes, including new and upgraded cycleways, and increase the provision of micro-mobility parking and storage facilities at key destinations.
- 6.6 Upgrade streetscapes through placemaking to improve the amenity, accessibility and overall quality of experience for all.
- 6.7 Improve the public transport network to make it more attractive, accessible and convenient for all users and provide direct connections between multiple transport modes.

Why is this proposed?

The City Heart is the primary transport interchange within Clarence, offering bus services to Central Hobart and other activity centres in Greater Hobart, as well as a direct ferry service to Brooke Street Pier (15 minutes). However, the heart faces clear challenges related to walkability, connectivity, safety, and amenity for pedestrians and users of bicycles, e-bikes, and e-scooters of all ages and abilities due to large areas dedicated to vehicles and car parking, as well as missing linkages. The configuration of movement, access, and car parking will need to be rethought for the City Heart to leverage its role as a transport interchange, cater to residents, attract workers and visitors, and improve connections between key destinations within the City Heart. Key workers will require multiple forms of transportation, including frequent bus connections to Hobart and other centres, better access to the ferry, improved management of vehicles and parking, and enhanced bicycle lanes and streets for active transport, walkability and accessibility to key attractions such as the parklands and cultural facilities.



7.6ha/18.8ha
= approx.

40%

of land* used for off street parking

* Land within Rosny Park Specific Area Plan excluding roads and public reserves.
This does not include on-street parking.

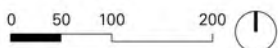


A connected City Heart

- City Heart Study Area
- Open space and landscape
- Kangaroo Bay Rivulet (open)
- Kangaroo Bay Rivulet (closed)
- Bus mall
- Ferry

- Existing Clarence Foreshore Trail
- Existing Off-road cycle route
- Existing On-road cycle route
- Existing Shared trail – upgrade
- Existing Pedestrian – upgrade
- Existing Crossing – upgrade
- Existing Bus mall to be upgraded
- Existing Bridge

- Proposed* Future off-road cycle
- Proposed* Future on-road cycle
- Proposed**
- Proposed Bligh Street Corridor
- Proposed Separated cycle route
- Proposed Kangaroo Bay Rivulet track (upgrade/complete)
- Proposed Potential links to Rivulet
- Proposed Proposed pedestrian link
- Proposed Proposed Parkland trails
- Proposed Proposed bridge
- P Proposed Council owned car park (aggregation opportunity)



*Current proposals and strategies

Concepts

3

The City Heart offers numerous strategic opportunities and encompasses both council-owned sites or other key sites owned by key City Heart stakeholders. The City Heart Plan presents concepts on how these sites can act as catalysts for change, working towards achieving the Vision for the City Heart.

Council owned sites

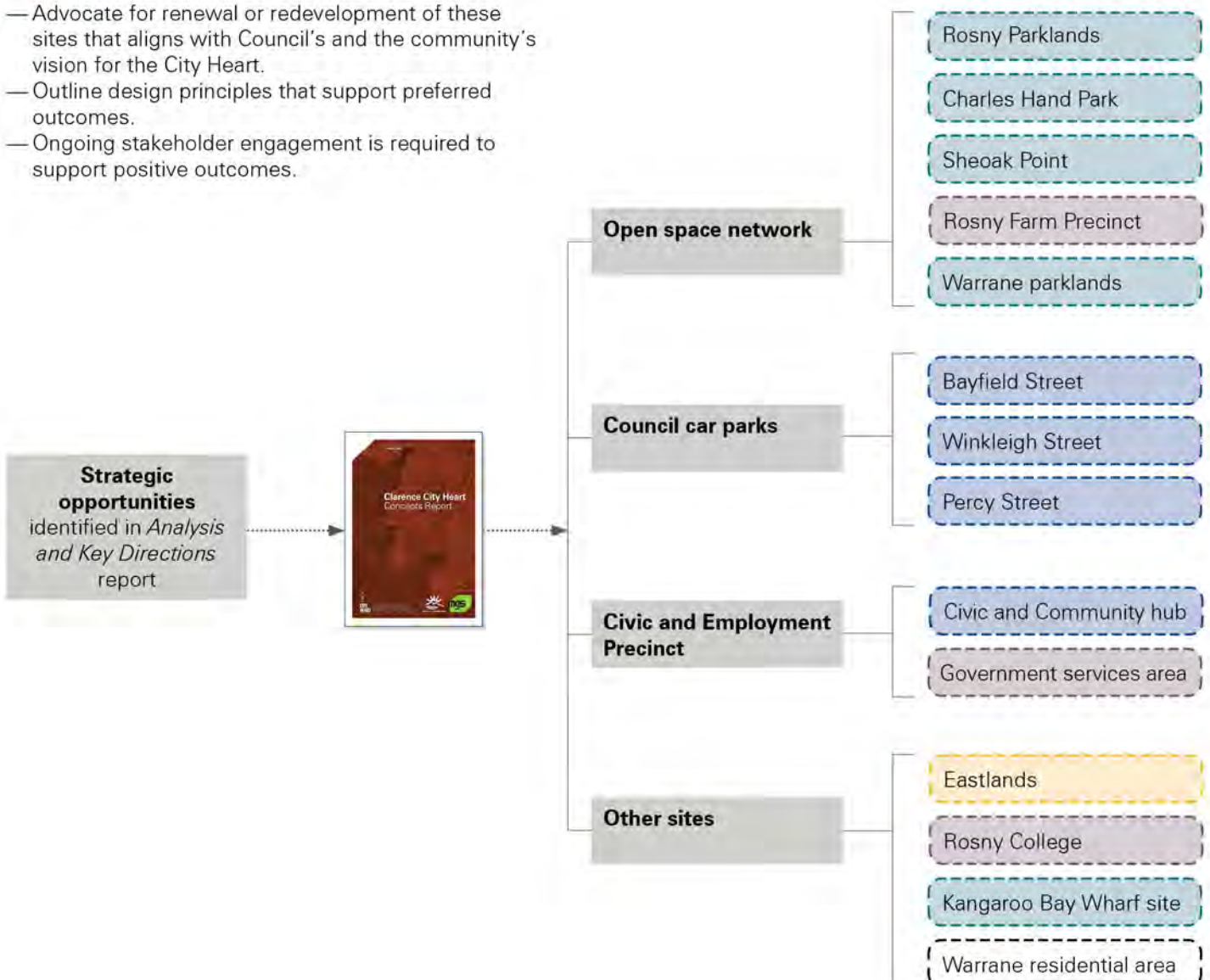
- Council has a strong influence on the use and development outcomes on these sites.
- Demonstrate leadership through an integrated program of development.
- Set an example of best-practices in design quality Environmental Sustainable Development (ESD), and universal design for accessibility
- Use Council projects as benchmarks and catalysts for wider positive change.

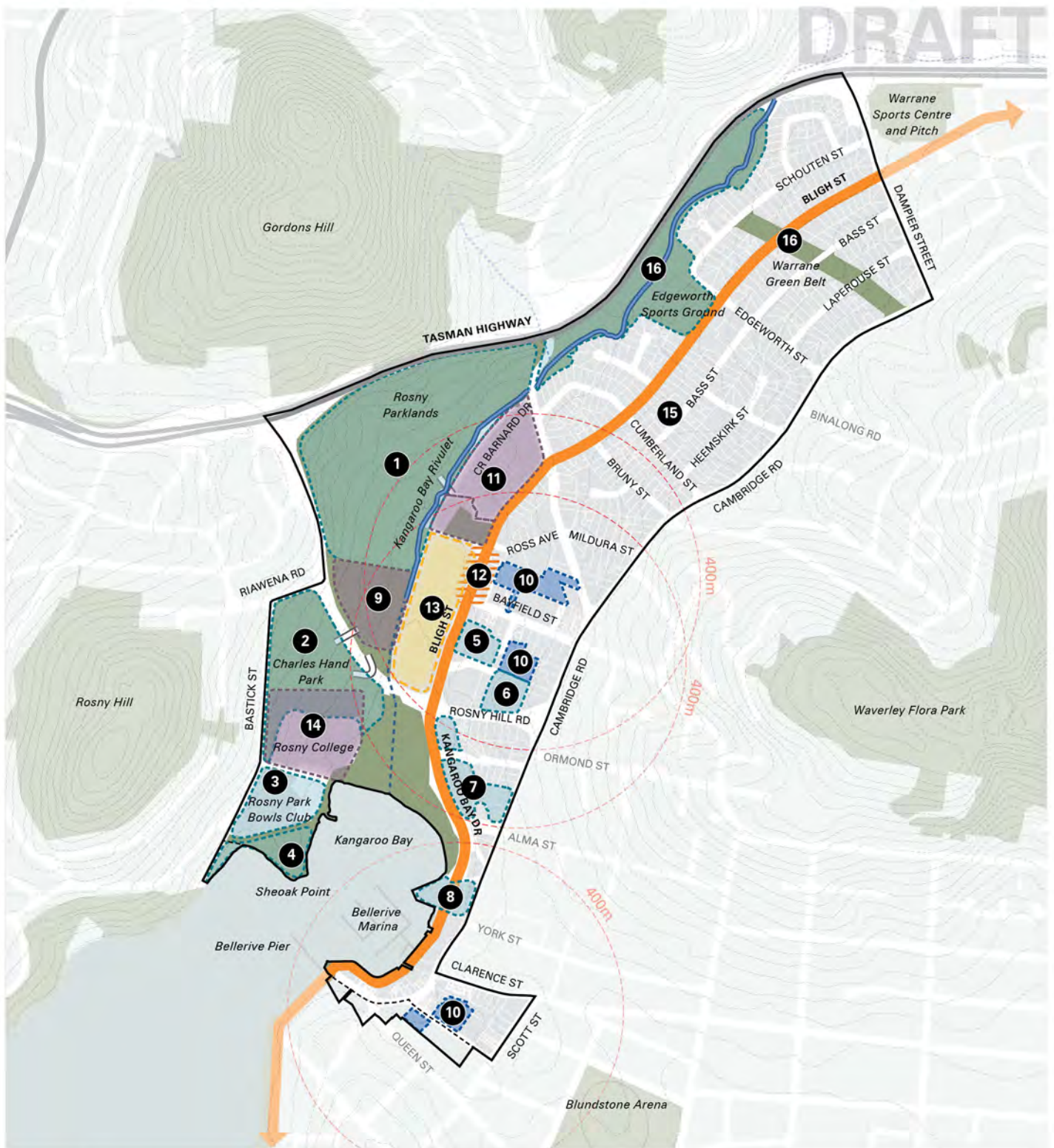
Other sites

- Advocate for renewal or redevelopment of these sites that aligns with Council's and the community's vision for the City Heart.
- Outline design principles that support preferred outcomes.
- Ongoing stakeholder engagement is required to support positive outcomes.

It should noted that there are other strategic opportunities identified that were not included in the plan as concepts, these include the following:

- Rosny Park Bowls Club
- Vicinity/Village site
- TMAG Rosny site
- Kangaroo Bay Boulevard
- Kangaroo Bay Wharf site: over the next 12 months council will be continuing with further work to determine the future use of this site.





Strategic opportunities

- City Heart Study Area
- Previous extent of Study Area
- Open space and landscape
- Proposed*
- Strategic site
- Strategic site – Retail/Commercial
- Strategic site – Civic/institutional
- Council-owned site

- 1** Rosny Parklands
- 2** Charles Hand Park
- 3** Rosny Park Bowls Club
- 4** Sheoak point
- 5** Vicinity/village site
- 6** TMAG
- 7** Kangaroo Bay Boulevard
- 8** Kangaroo Bay Wharf site
- 9** Rosny Farm Precinct

- 10** Council landholdings
- 11** Civic and Employment Precinct
- 12** Bligh St and Bus Mall
- 13** Vicinity Centres - Eastlands
- 14** Rosny College
- 15** Warrane residential area
- 16** Warrane parklands

0 100 200 400m



Note: Walking catchments show a 400m radius (5 mins walk) centred on the Council car park sites.

Open Space Network

Open space is an essential component of a liveable city, critical to human health and an essential infrastructure element mitigating climate change, alleviating floods and providing ecosystem services. It is not an empty space waiting to be filled.

A city's green spaces are where people come together to share experiences, celebrate culture, and create bonds that build a shared sense of place imbued with meanings that are unique to that place. In the City Heart, the green spaces can express Clarence's place in the world, becoming central to the community's idea of what it means to live in Clarence.

Through this lens, the following principles for the City Heart's open space network have emerged.



1 Establish a 'Green City Heart' grounded in ecological and social principles



2 Celebrate the living cultural landscape of lutrawita / Tasmania



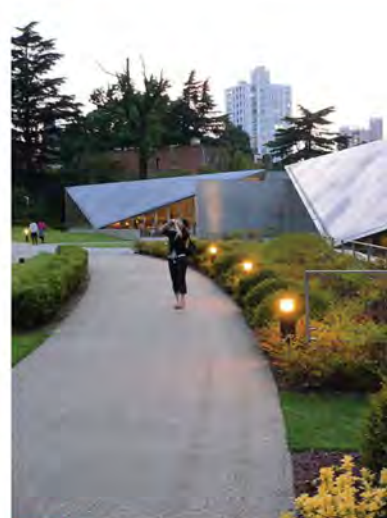
3 Foster a safe, walkable City Heart that inspires and delights



4 Create a playful landscape to improve the health of all



5 Put the arts and culture centre stage in the city's daily life



6 Embed accessibility and universal design principles

Open space network

Parklands of the City Heart

7 in 10 say they would use the Parklands more often if the proposed concepts are implemented

"The City Heart will offer diverse outdoor spaces that evoke the spirit of Clarence - embracing our cultural origins and revelling in the contemporary expression of a place where everyone can participate in a healthy and active life, celebrating the arts and the outdoors within an ecologically diverse setting and gathering together as one community with a shared future."

— The Parkland Vision





Open space network

Active Park

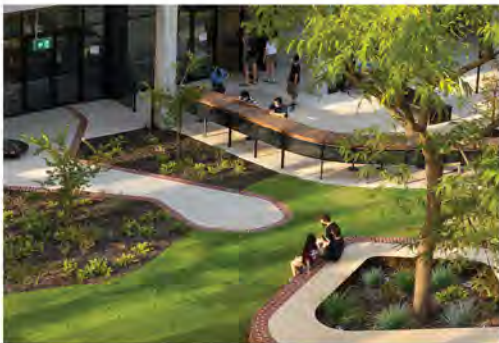
Outcomes



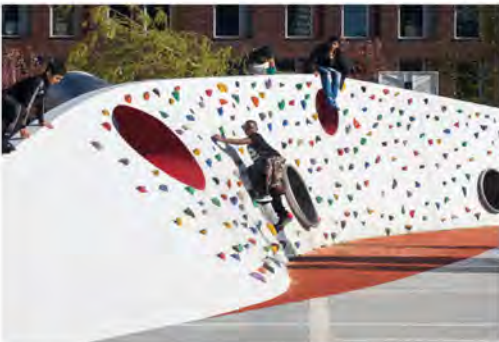
A place to enjoy the energy that comes from active participation in a vibrant, safe and playful outdoor setting.



Precedents



Curtin University
Exchange Innovation
Precinct — Realm



Into The Wild playscape,
The Hague — Openfabric
+ Dmau



Clayton Road Linear Park,
Melbourne — Glascott
Landscape & Civil

Key

- ① Potential for future development of Rosny College to orient the school towards the Parklands. Explore opportunities to open up into the landscape.
- ② Potential Rosny College semi-private open space. Opportunities for hospitality and arts enterprise.
- ③ Flexible courts for tennis and pickleball.
- ④ Expanded hard surfaced games spaces — basketball, four square, netball, table tennis, climbing and bouldering walls.
- ⑤ Existing skate park. Potential development between park and College to create private, secure space.
- ⑥ Express the rivulet through the use of water as a feature.
- ⑦ New shared track from Rosny College to Rosny Hill Road.
- ⑧ New pedestrian bridge linking to the Art Park. 1:14 gradient, ensure there is adequate clearance for heavy vehicles.
- ⑨ Medium density housing for key workers with parking under overlooking the park and providing passive surveillance to parklands.
- ⑩ Green space for imaginative passive recreation. Potential concert space.
- ⑪ Art totem sculptures to frame entry to City Heart.
- ⑫ Sheoak Point yarning and colearning space, a place to gather and reflect on the living cultural Tasmanian Aboriginal landscape.
- ⑬ Upgrade intersection to improve pedestrian connection from the activity centre to Rosny College
- ⑭ Strengthen circulation routes directing movement away from college and cricket club.

Indicative section



- Proposed built form
- Future development site



Open space network

Art and Play Park

Outcomes



A gathering point that supports education, arts and play of all kinds and inspires creativity, stimulates intellectual curiosity, fosters critical thinking. An age and abilities inclusive space welcoming “the child in everyone” to develop their physical, cognitive, sensory and social skills through play in the fresh air and sunlight of the outdoors.



Precedents



Rockingham Foreshore,
Perth | PLACE Laboratory



21_21 Design Sight art
museum, Tokyo — Tadao
Ando & Issey Miyake



Awen Gathering Place,
Winnipeg | Brook McIlroy

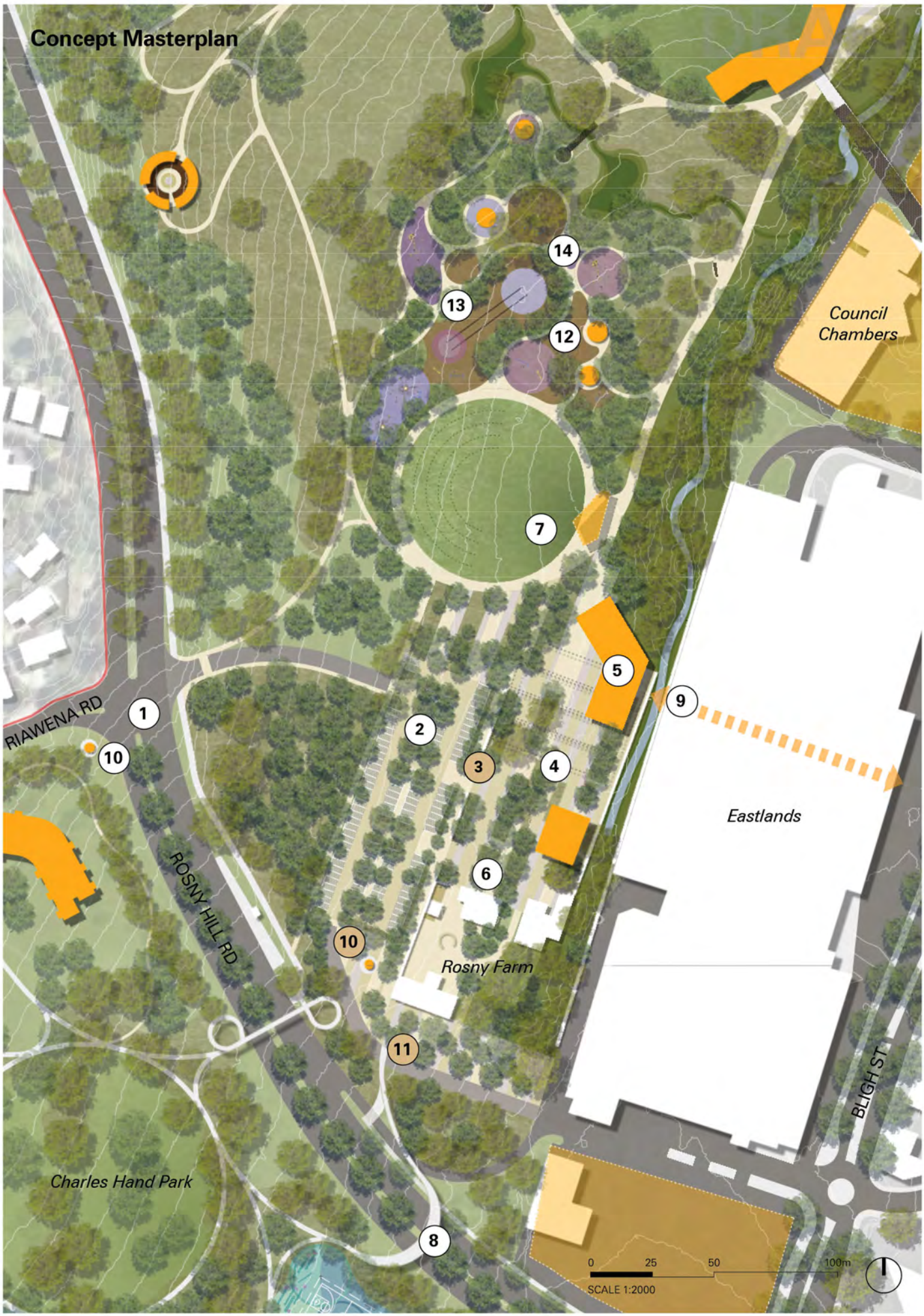


Kingston Park,
Kingborough — Playstreet

Key

- 1 Signalised intersection to enable safe pedestrian access to the Rosny Parklands and vehicle access to the precinct.
- 2 Car parking with accessible entry and drop off area and space for bus drop off. Scale to be determined. Balance need for the provision of this supporting infrastructure with protecting important views and sight lines and other valued characteristics of the City Heart.
- 3 Extension of walled courtyard with new entry (“the gateway to Tasmanian culture”).
- 4 Flexible hard surfaced events space for markets, festivals, pop ups, performances, etc. Water sensitive, low environment impact design prioritised.
- 5 New rated contemporary gallery space with sales outlet, cafe.
- 6 Workshops, administration, meeting rooms, public programs.
- 7 Multi-purpose festival space with stage area. Permanent electricity and water. Permanent stage facilities to be considered. Accessible amenities including picnic, barbecue and toilets to support group gathering, bookings, etc.
- 8 New pedestrian bridge to Art and Play Park.
- 9 Investigate east-west connection through the site to Rosny Parklands
- 10 Art totem sculptures to frame entry to City Heart.
- 11 Forecourt with water themed art sculpture.
- 12 Art-full ‘all abilities playground’ supporting range of physical and cognitive abilities including seating, shade and water play. Play area to include artwork installations, sculptures, light installations and murals.
- 13 Play areas of varying sizes for a range of ages and abilities with dual flying fox, seating, water play and shade.
- 14 1:20 path network surrounding play area for bike riding and scooters. The track can be used as a loop or as an adventure into the parklands.

- Proposed built form
- Future development site
- Potential pedestrian link



Open space network

Wild Park

Outcomes



A space for all living things of the city to thrive — a cultivated ecology where people can spontaneously revel in the power of nature and the joy and health of being in a biodiverse landscape, filled with beauty and delight.



Precedents



Snowy Mountains —
Michael Bates



Western Sydney
Parklands —
McGregor Coxall



The Adelaide Botanic
Garden First Creek
Wetland— TCL



Sabina outdoor yoga
platform — Marco
Antonini Architects

Key

- 1 Chain of ponds with seating/gathering spaces scattered alongside.
- 2 Story, living culture seating spaces in the shade of the rewilded landscape.
- 3 Rewilded dry eucalypt forest and woodland to highway edge and changing existing weedscape to improve biodiversity.
- 4 Cultivated ecologies using existing trees as a starting point introduce a range of habitat pockets (dry, wet, rocky with native vegetation) to support a diversity of urban adapted native wildlife... bees, skinks, butterflies, small mammals.
- 5 Connector path into Wild Park from highway, linking site to Gordon's Hill nature path network, Rose Bay High School and broader commuter network. Install help point and safety information.
- 6 Covered pavilions or seating with views through parklands.
- 7 Seek collaboration and colearning opportunities with the University and Royal Botanic Gardens.

- Proposed built form
- Future development site
- Potential pedestrian link

Concept Masterplan

"Wild Park! A rewilded biodiverse landscape with pathways. This fits with Tasmania's brand and provides an alternative to the Botanical Gardens. I can imagine more experimental theatre companies wanting to stage plays here."



Open space network

Civic Park

Outcomes

5

A living room for the city — a place for families, community bodies, cultural groups and visitors of all backgrounds and ages to gather and celebrate civic life in a park atmosphere.



Precedents



Keast Park Community Pavilion Seaford, Victoria
— JCB



Curtin Yarning Circle, Perth — UDLA



Mitcham Library & Hawthorn Reserve — ASPECT Studios



Banks of the Saone, France — IN SITU
Paysages & urbanisme

Key

1

New civic and community hub for access to government services and jobs, community facilities and resources

2

Lawn play and performance space. Accommodates around 12,000-15,000 people. When not in performance mode the lawn offers invaluable and flexible open space for un-programmed use such as kickabout space, kite flying, yoga, boot camp, birthday parties, social gatherings etc.

3

Story and living culture seating spaces — seating adjacent to the Rivulet in the shade of the trees close to native vegetation to support cultural practices (food, medicine, weaving).

4

Chain of ponds (<1mL to avoid being a 'dam') with seating/gathering spaces scattered alongside.

5

Bridging element as part of the Civic and Community hub redevelopment to connect administrative precinct and retail precincts of the City Heart.

6

Small pavilions for small group gathering.

7

Rivulet bank planting using native vegetation. Expansion of Eucalyptus ovata (black gum) through revegetation techniques. Rewild context to support healthy rivulet.

8

Civic and Community hub renewal.

9

Benches and seating to relax and enjoy views over the parklands.

10

Infrastructure to support 'pop-up' events and stages

NOTE: Civic Park is not publicly accessible by road, service vehicle access only.

Propose built form

Future development site



Open space network

Warrane Parklands

Outcomes



Warrane, palawa kani for 'blue sky'. The park reimagines its open spaces as a string of green pearls filled with opportunities to gather and play that connect and support the good life of this diverse suburb with its easy access to services, level ground for all ages, supportive network of friends and families and the blue skies above.



Precedents



Felixstow Reserve —
ASPECT Studios and
Oxygen



Sunvale Community Park
Sunshine, Victoria



North Lakes, Queensland

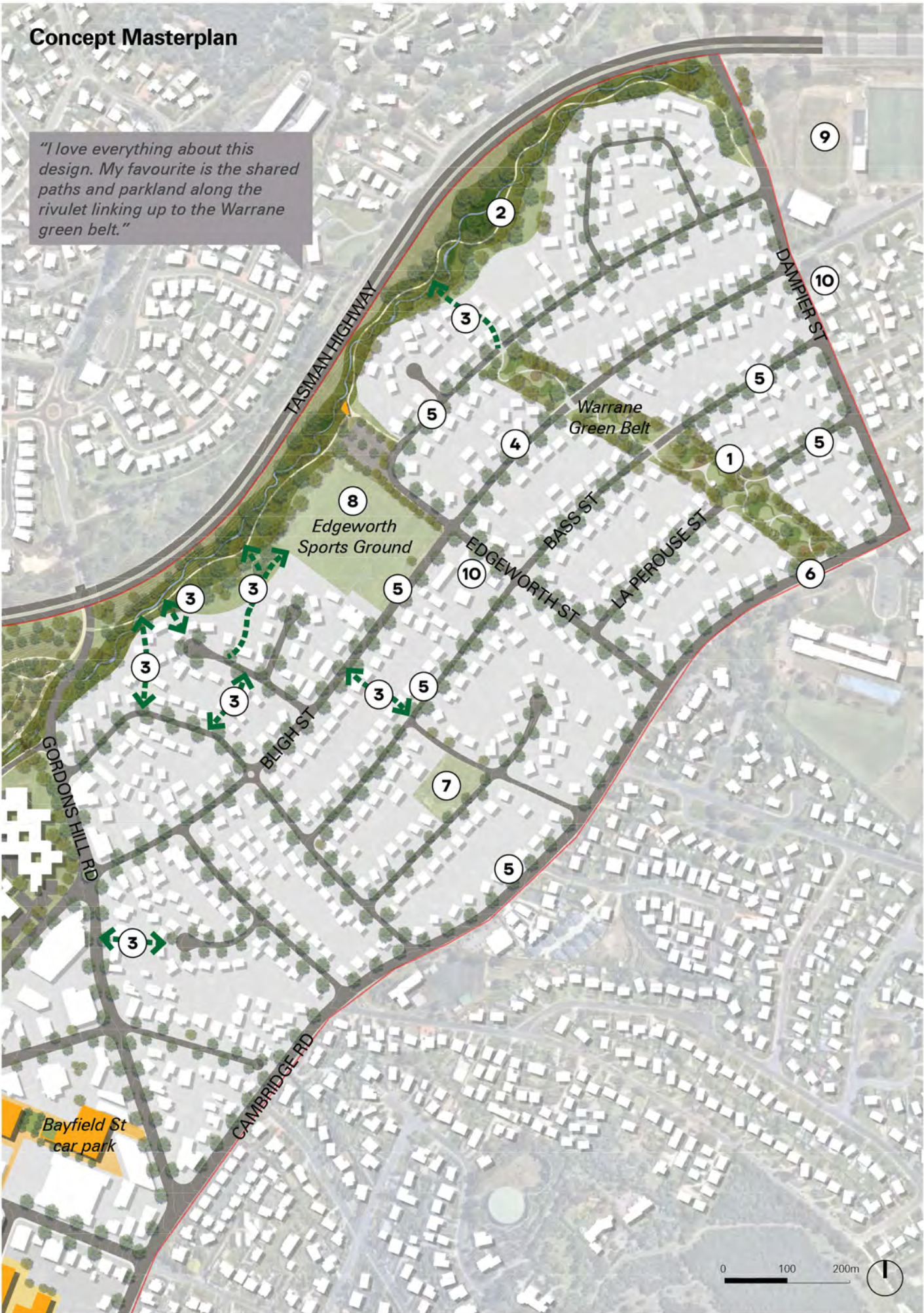
Key

- 1 The Warrane Green Belt play corridor with improved facilities for play, picnic and community gathering centred on water quality and flood management treatments including grassed swales, infiltration trenches and bio-retention basins. Path connecting play corridor to Warrane Primary School. Close La Perouse St at park to consolidate two park spaces. Slow traffic on Bass and Bligh Street to enable safer transition between upper and lower park spaces
- 2 Kangaroo Bay Rivulet treatments including bio-retention basins and riparian vegetation
- 3 Explore options to create pedestrian linkages as properties are redeveloped
- 4 Bligh Street improvements to include improved footpaths and cycle infrastructure with raingardens to assist with stormwater management
- 5 Network of 'absorbent' streets with street trees, kerbside rain gardens
- 6 Upgrade underpass to create a more light and colour-filled environment to enhance safety and increase use
- 7 Retain existing community garden
- 8 Prepare master plan for Edgeworth Sports Ground to maximise the community benefits of the facilities there
- 9 Prepare master plan for Warrior Park to maximise the community benefits of the facilities there
- 10 Create a master plan for the shopping precincts to make it more pedestrian friendly with seating, street trees, improved pavements and raised all-abilities parking spaces
- 11 Support neighbourhood greening projects.

Propose built form
 Future development site

Concept Masterplan

"I love everything about this design. My favourite is the shared paths and parkland along the rivulet linking up to the Warrane green belt."



Mixed use development opportunities on Council car parks

*7 in 10 agree with the
design principles for
rethinking car parking
in the City Heart.*

Mixed use development opportunities on Council car parks

Design principles

DRAFT

Rethinking parking in the City Heart presents Council with an opportunity to provide car parking while also unlocking space for other developments that will support a diverse, vibrant and inclusive activity centre.

- 1 Consolidate parking to unlock the ground level for active and higher value uses
- 2 Encourages pedestrian 'ant trail' from car parks to nearby destinations in the City Heart to support economic activity
- 3 Integrated residential, hospitality or community spaces to increase activity day and night, improving safety
- 4 No loss of publicly accessible parking spaces within the City Heart
- 5 Design proposed built form to comply within the existing planning height controls

Case studies

Dawson Street Car Park Sunshine, Victoria



- Constructed to align with civic centre redevelopment
- Provides free 2-hour parking and reduced rate parking to the precinct including to council site
- Strategically located to attract employment anchors, support retail activity by increased pedestrian movement, and attract public transport investment
- Reduced retail and hospitality vacancies on Hampshire Road
- 10 mins walk to Sunshine train station, 5 mins to council offices

Eunerva Ave & Bogong Ave Glen Waverley, Victoria



- Council strategy to consolidate parking and unlock sites throughout activity centre for higher uses including council and private developments
- Incorporate highly visible entries and other uses at ground floor (Eunerva Ave: community health service)
- Car parking contribution scheme, 'cash-in-lieu'
- Increased pedestrian activity, renewal of fine grain retail street
- 6 storey, 300+ spaces Eunerva Ave and 3 storey, 1000+ car parking spaces, Bogong Ave
- 5 mins walk to Glen Waverley train station

Elgin St Car Park Carlton, Victoria



- Combines City of Melbourne car park with 2/3 storey apartment and town houses
- Fine grain retail and heritage context
- Reduced parking rates to support retail activity
- 2 storey, 200 car parking spaces
- 2 mins walk to tram services

Mixed use development opportunities on Council car parks

Bayfield Street

Outcomes

3 6

A centrally located car park hub with a new mixed use community destination and an urban plaza with adjoining retail and commercial spaces.



Precedents



Dawson Street Car park,
Sunshine — MGS Architects



Healeys Lane,
Melbourne

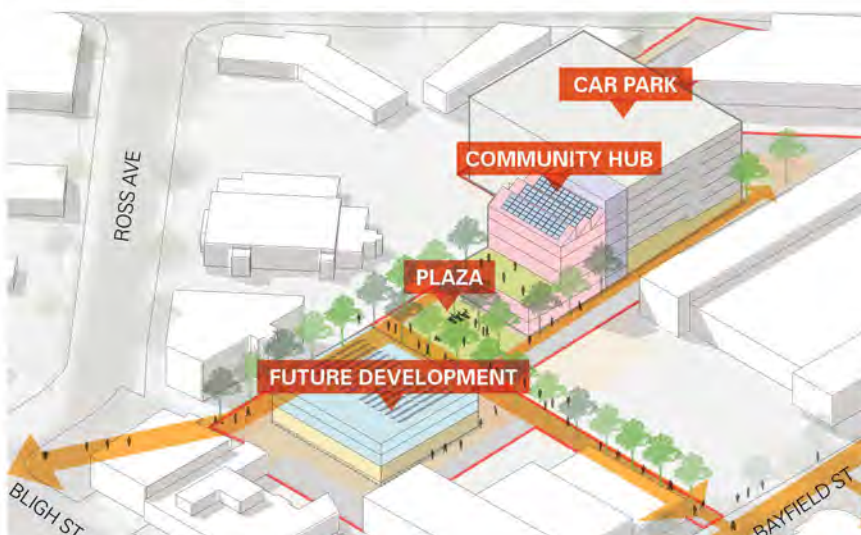
Key moves

- 1 Consolidate parking to the east of the site
- 2 New community destination and employment space at the centre
- 3 Create new open space and upgrade link to civic centre
- 4 Increase activity at street level and create opportunities for natural surveillance
- 5 Create new development opportunity
- 6 Reinforce existing linkage to Bayfield Street

Key outcomes

- Reinforce existing pedestrian linkages to Bayfield and Bligh Streets
- Frees up space for commercial/office space development which is needed in the City Heart
- Creates more activity and value on an underutilised city block
- Increasing safety and creating a better town centre experience.
- Designing the car park for greatest efficiency
- Intensifies a low density urban block

Indicative massing



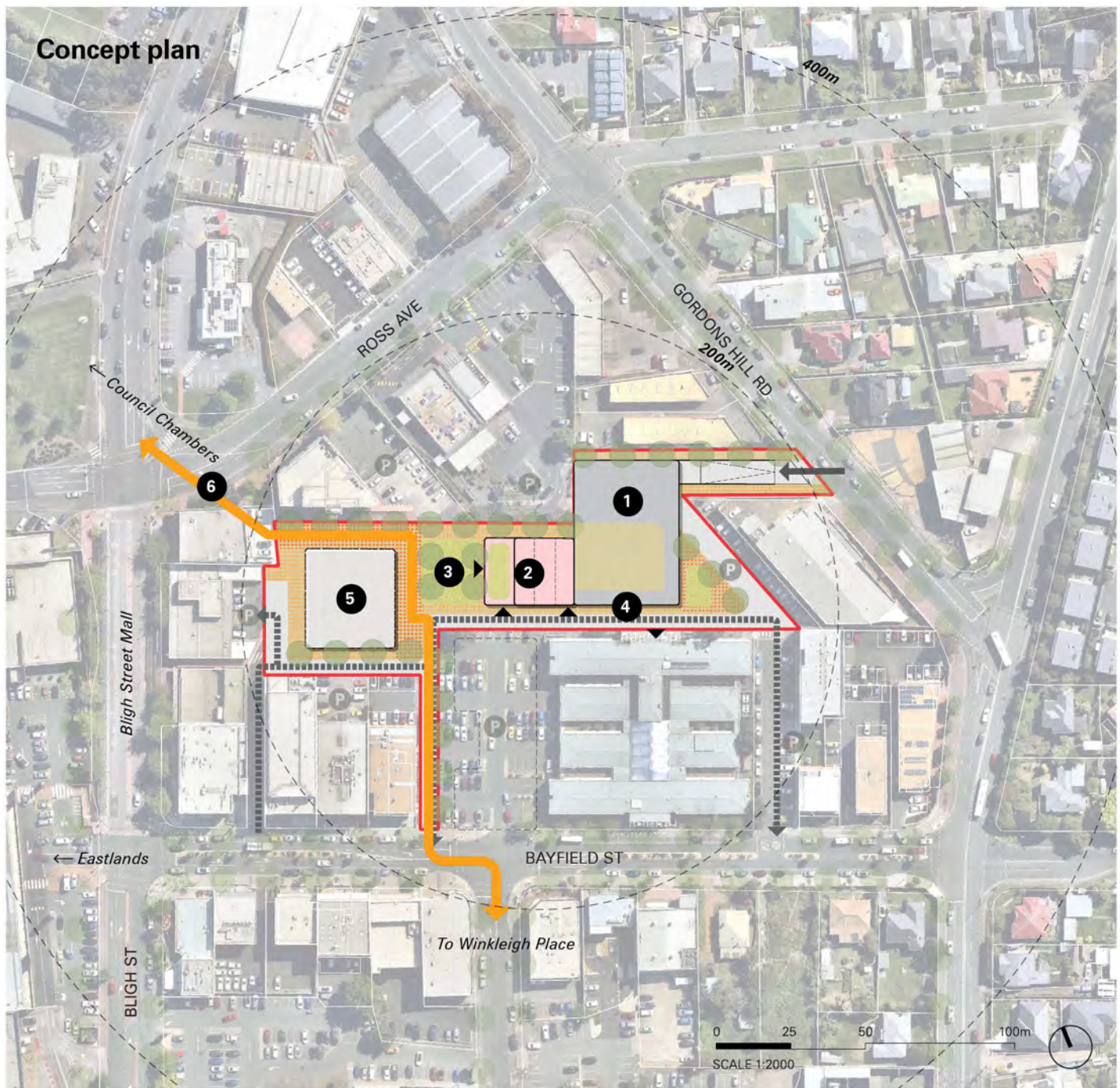
Indicative yield

East

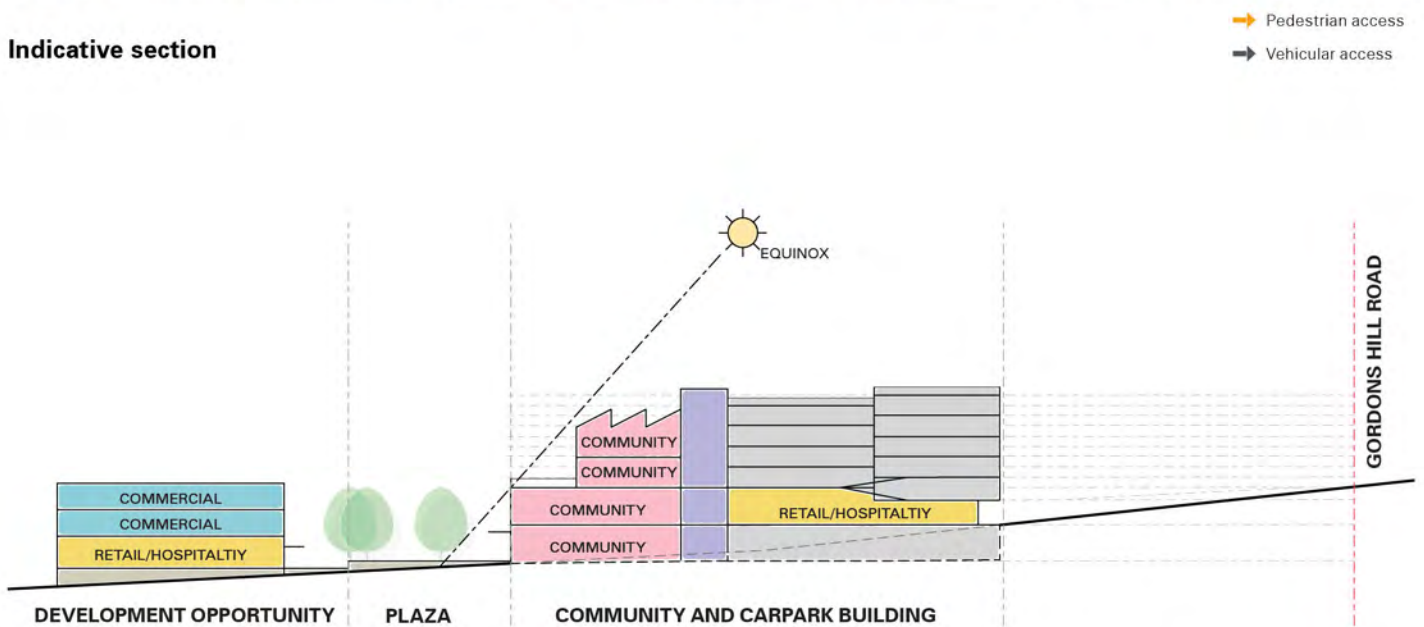
- Car parking, 230-280 spaces (replacing approx. 200) including rooftop and/or basement
- Community, approx 2000m² GFA, including roof terrace
- Retail/hospitality, approx 800m² GFA

West (development opportunity)

- Commercial/retail/hospitality, approx 3000m² GFA



Indicative section



DRAFT



DRAFT



Mixed use development opportunities on Council car parks

Winkleigh Place

Outcomes

3 4 6

Creating a new residential community with great access to services, jobs and transport integrated with a new public car parking hub. Integrate active uses such as communal spaces or retail to enliven the streetscape.



Precedents



Nightingale 1, Melbourne — Breathe Architecture



Guild Coworking Space, Southbank

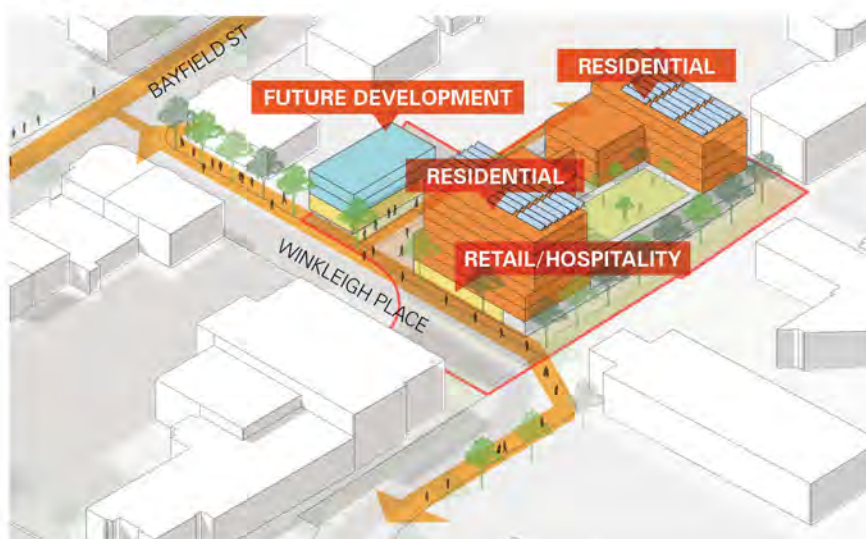
Key moves

- 1 Provide separate entries and reserved car and bicycle parking for residents
- 2 Include community, retail or other uses at street level to create an active frontage (or similar)
- 3 Shared pedestrian and vehicular lane along the northern boundary
- 4 Setback from Winkleigh Place to provide space for canopy trees and pedestrian link to Bayfield Street
- 5 Create new development opportunity
- 6 Retain existing accesses to properties on Cambridge Road which access from Winkleigh Place

Key outcomes

- Increase safety with 'eyes on the street' and creating more housing choice within the City Heart
- Increase pedestrian activity within the urban block and between Cambridge Road and Bligh Street

Indicative massing



Indicative yield

Residential

- Approx 80-90 dwellings
- Typical breakdown of apartment size*
 - 70% 1 bed
 - 20% 2 bed
 - 10% 3 bed

Car park

- 180-190 spaces (replacing approx. 152)
- Car parking rate for 0.6 per dwelling*, no of additional spaces required 50-55

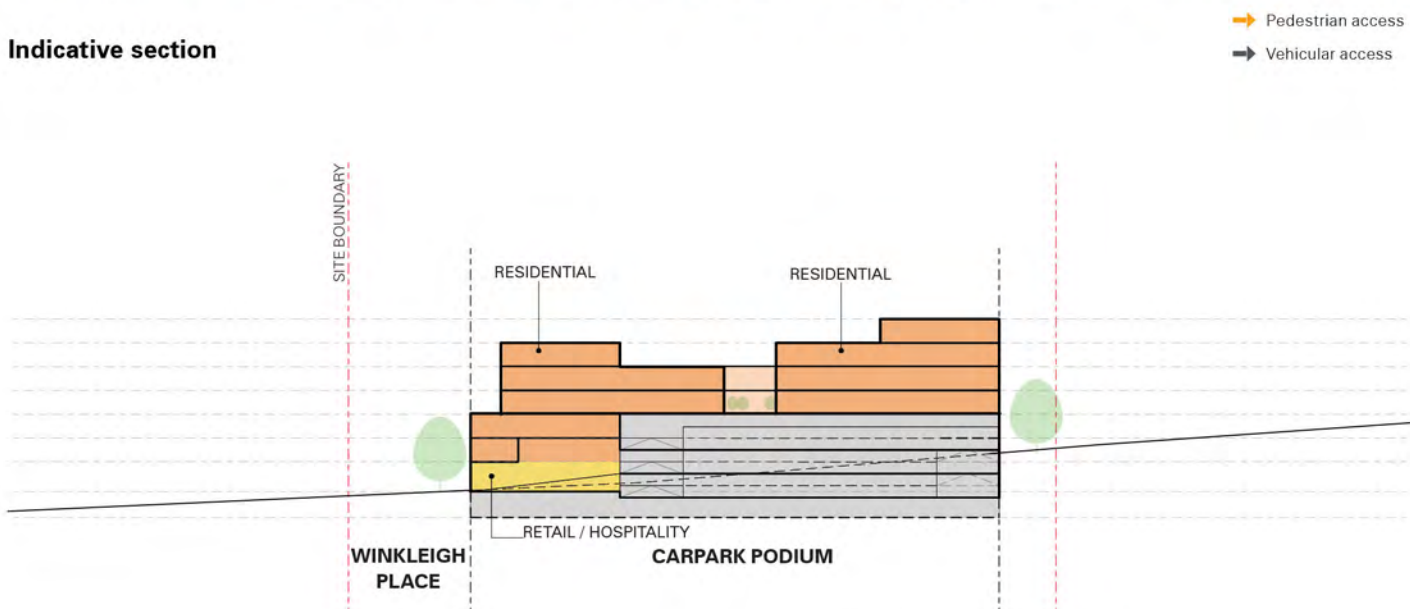
North (development opportunity)

- Commercial/retail/hospitality, approx 1000m² GFA

**Based on recent Homes Victoria projects, but parking ratios will be determined according to the local context*



Indicative section



Mixed use development opportunities on Council car parks

Percy Street

Outcomes

3 6

Supporting the Bellerive Village as a destination for local retail and dining with a unique water front experience and views.



Precedents



Harrow Street Car park and Community Centre, Melbourne — MGS Architects



Kyme Place Rooming House, Melbourne — MGS Architects

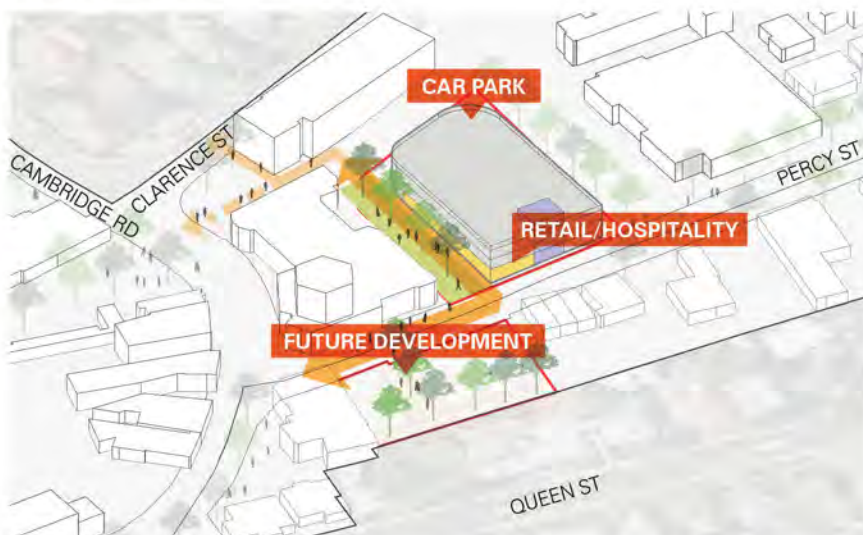
Key moves

- 1 Consolidate the two council-owned car parks at Bellerive Quay and Percy Street
- 2 Locate ground floor with retail, hospitality and end-of-trip uses to increase activation.
- 3 Shared zone including other placemaking elements such as landscape, seating to western edge.
- 4 Unlocks Percy Street south car park for mixed use development.
- 5 Supports future uses on Kangaroo Bay Wharf site, within a 5 -10 minute walk.

Key outcomes

- Opportunities to enhance the fine-grain character of the village through potential new pedestrian links and public realm spaces.

Indicative massing



Indicative yield

Percy St north (adjacent to Bellerive Quay)

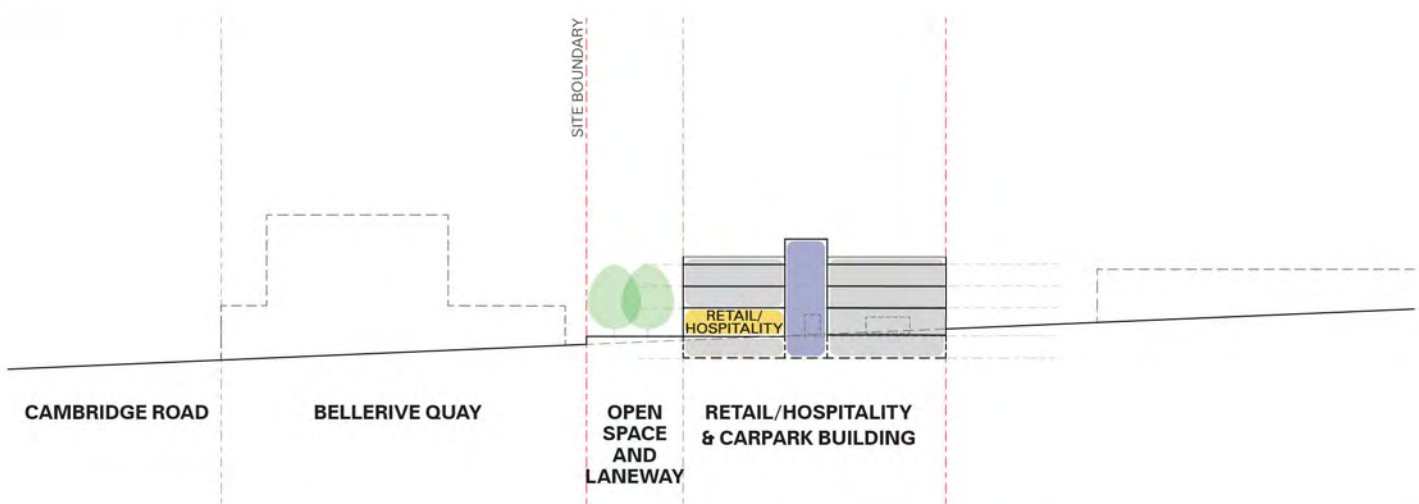
- Car parking, 150-180 spaces (replacing 154)
- Ground floor retail, 660m² GFA
- Setback to western boundary for new open space
- 3 levels plus rooftop, basement optional
- Retain existing accesses to properties

Car park

- Mixed use development, approx 117m² GFA
- Potential use as a visitor accommodation



Indicative section





Civic, Community and Employment Precinct

Civic, Community and Employment Precinct Design Principles

Outcomes



A Civic and Community Hub that celebrates and embraces the rivulet and parklands, increasing space for services and jobs and connected by welcoming outdoor places and landscapes for workers and visitors.



- 1** Expand and re-imagine the Civic precinct as a welcoming place for community and hub for government services and jobs, community facilities and resources invested with civic pride, and an expression of community participation and democracy
- 2** A series of open spaces including plazas connected along a green spine to support community events and safe, accessible movement between destinations.
- 3** Orientate the Civic and Community hub and potential future library towards the rivulet and new promenade spaces
- 4** Provide an attractive and accessible bridge over the rivulet connecting the Civic and Employment Precinct to Rosny Parklands
- 5** Create a new cultural space in the parklands that celebrates Tasmanian Aboriginal culture and events for the whole community
- 6** Expand the Clarence City Council office accommodation and new 'town hall' auditorium with flexible spaces for the community to meet and gather.

Case studies

Brimbank City Council, Community & Civic Centre Sunshine, Victoria



- Adjacent to Sunshine Market Place (Vicinity)
- Consolidation of community and civic uses
- Integrated library, exhibition spaces and local business hub
- 10 mins walk to Sunshine train station, 5 min walk to council car park

Dandenong Municipal Building and Civic Square Dandenong, Victoria

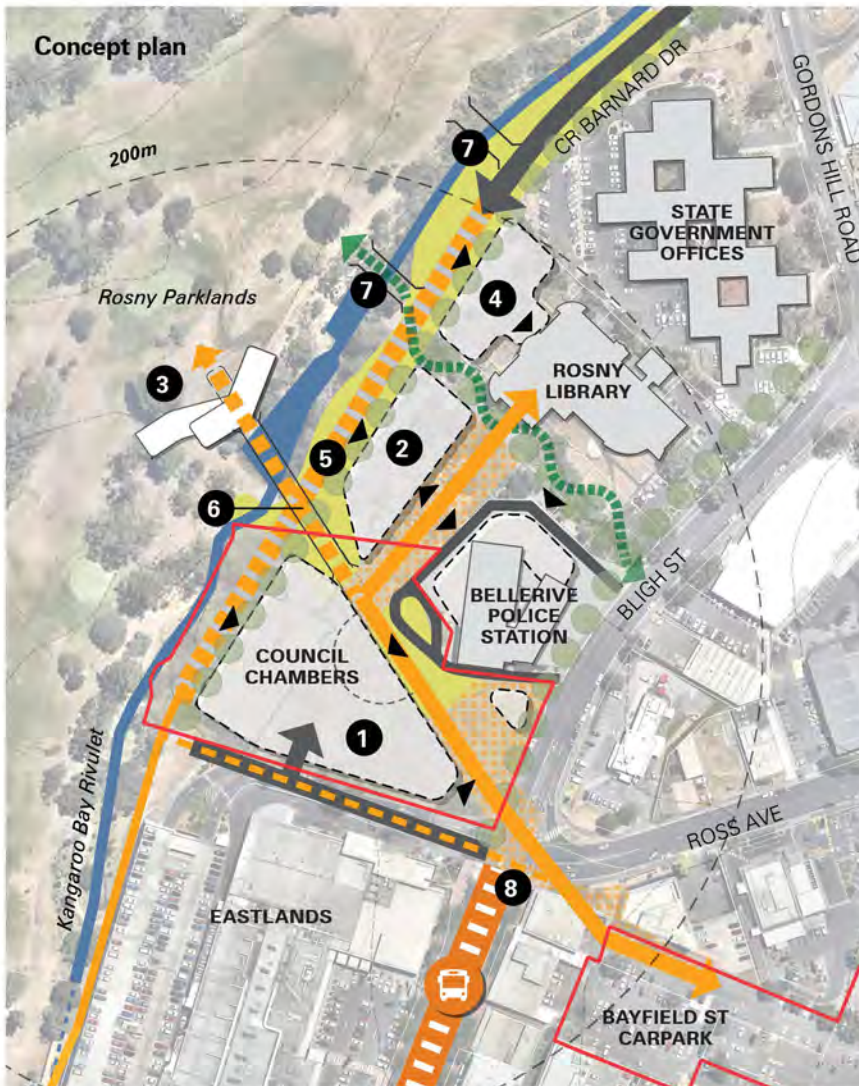


- Council offices, library, meetings rooms, retail and cafe
- 3,500m² of open space with digital screen to support community events
- Key project within Revitalising Central Dandenong \$290m state funded program
- 5 mins walk to Dandenong train station and retail core

GovHub Ballarat Ballarat, Victoria



- Employment space for 1000 public and private sector jobs
- Sustainable design (mass timber, CLT and GLT)
- Funded by the Victorian State government
- 220 car spaces and end-of-trip facilities
- 5 mins walk to regional train services and retail centre



Key moves

Council Chambers site – expand office accommodation and locate new ‘town hall’ auditorium - a flexible space for community to gather. Consider possible co-location of state government services.

- 1 Council Chambers site – expand office accommodation and locate new ‘town hall’ auditorium - a flexible space for community to gather. Consider possible co-location of state government services.
- 2 ‘Possible future’ library, local business hub and gallery
- 3 Tasmanian Aboriginal culture and parklands pavilion
- 4 ‘Possible future’ new government services building
- 5 Create a pedestrian boulevard with slow vehicle movement
- 6 Accessible bridge to parklands
- 7 Options for rivulet crossing points
- 8 Upgrade intersection to improve safety for pedestrians

Legend

- City Heart Study Area
- Open space and landscape
- Council site boundary
- Existing building
- Bus interchange

Proposed

- Plaza
- Open space
- Development footprint
- Building footprint
- Building entry

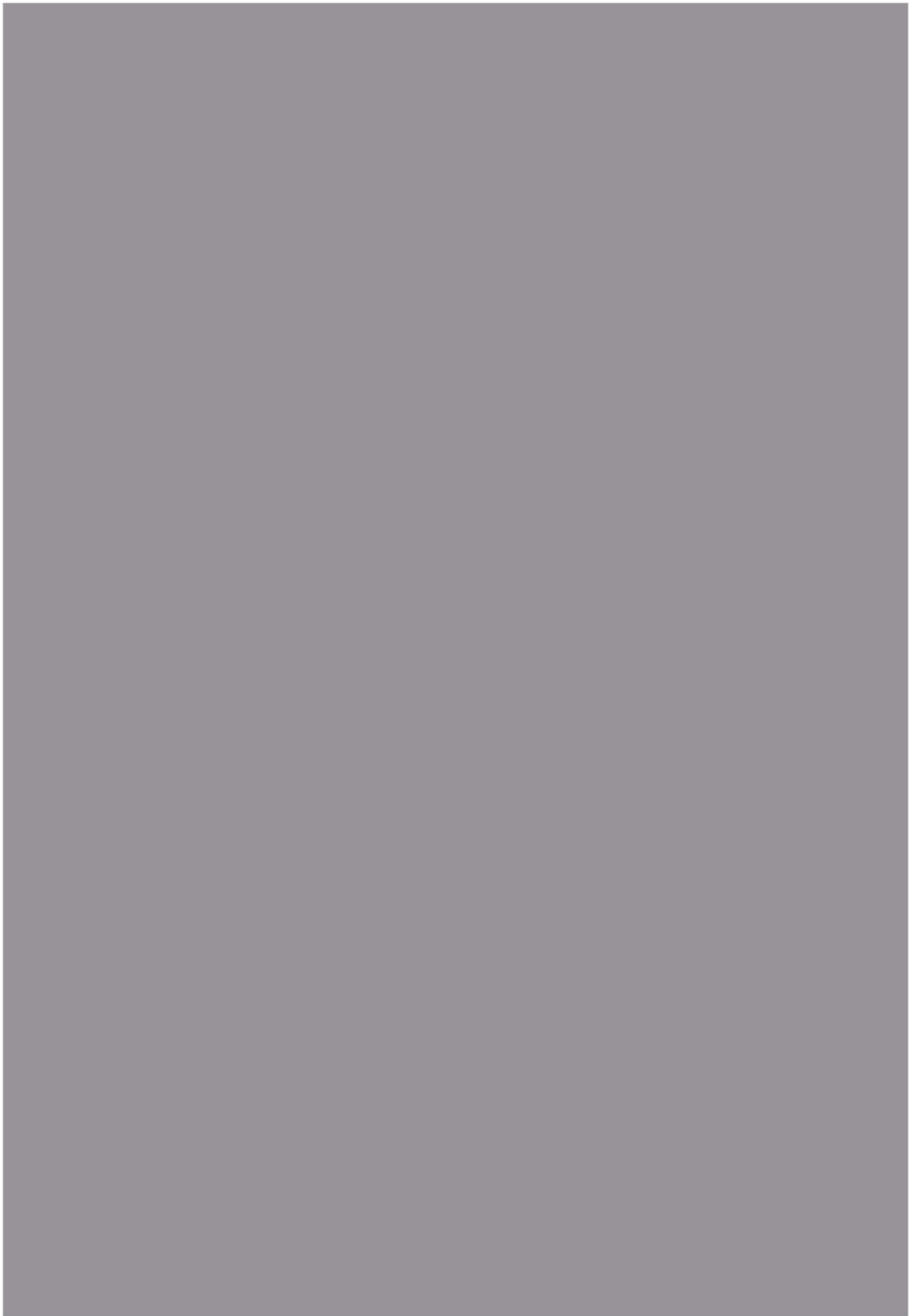
- Vehicular access
- Pedestrian access
- Potential pedestrian access
- Shared street
- Green spine

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Bligh Street corridor

Bligh Street corridor

Design Principles

Outcomes

5 6

The City Heart's green spine will connect Bellerive to Warrane, supporting active transport along it and with landscaping and infrastructure to create a streetscape to enjoy.



- 1 Revitalise Bligh Street with high quality avenue tree planting, generous footpaths and protected cycle lanes for its full extent from outer Warrane to Kangaroo Bay
- 2 Improve connection between public transport and City Heart destinations and the regional cycle network
- 3 Encourage active transport travel and Bligh Street's evolution into a consistently high quality Main Street pedestrian experience with linkages to key precinct destinations
- 4 Support growing residential and worker populations to move safely between destinations
- 5 Provide accessible, safe and enjoyable pedestrian movement linking the precincts in the centre at all times of day and night
- 6 Upgrades to Bligh Street bus mall to improve amenity and safety, contributing to the character of the street

Case studies

Malop Street Green Spine

Geelong, Victoria



- Extensive streetscape renewal to support growing residential and worker populations with new street trees, rain gardens, paving and street furniture.
- Bicycle lanes (each side)
- 1.4km long / 30m wide

Bourke Street Cycleway

Sydney, New South Wales



- Dedicated buffered 2-way cycleway to encourage active transport travel
- Extensive buffer planting, new street trees, rain gardens, permeable paving and integration with existing public spaces.
- 3.4km long / approx. 20m wide

8 out of 10 support the design principles for the Bligh Street corridor

Concept plan



Precedents



Shared bike and pedestrian path, Linear Park Reserve, Melbourne



Raised pedestrian 'wombat' crossing



Two-way cycle lane, San Francisco, USA

1 Warrane

Linking residents to the activity centre, rivulet corridor, Green Belt and beyond.

- 1 Introduce shared paths along Warrane Greenbelt to connect to Kangaroo Bay rivulet and other open spaces, and upgrade crossing to prioritise active transport users safety and experience (e.g. wombat crossing)
- 2 Create safe cycle lanes (investigate options)
- 3 Upgrade Gordons Hill Road intersection to prioritise active transport users safety and experience
- 4 Include water-sensitive urban design (WSUD) and greening - canopy trees, bioswales
- 5 Provide amenity along the corridor including nodes for planting, seating, bike parking and crossings
- 6 Investigate options for on street parking (e.g. one side of street only, between trees, limited locations)

2 Rosny Park

An active streetscape that connects people with transport choices.

- 1 Manage and separate different travel modes to minimise conflicts through this section, particularly the bus mall
- 2 Investigate strategic removal of on street parking if necessary to facilitate other uses
- 3 Create safe cycle lanes, investigate options for a protected two way cycle route
- 4 Upgrade Bayfield Street intersection to prioritise active transport users safety and experience
- 5 WSUD and greening - canopy trees, bioswales

3 4 Kangaroo Bay & Bellerive Esplanade

Expanding on existing streetscape upgrades to link the waterfront to north of Rosny Hill Road.

- 1 Investigate providing cycle lane on Kangaroo Bay Drive and a slow-speed cycle link to Bellerive Esplanade
- 2 Upgrade Rosny Hill Road intersection to prioritise active transport users safety and experience, for example, removal of slip lanes
- 3 Manage potential conflicts between pedestrians and cyclists towards the waterfront with slow speed zones or dismount areas
- 4 Pedestrians only along waterfront edge
- 5 Connection through to ferry terminal

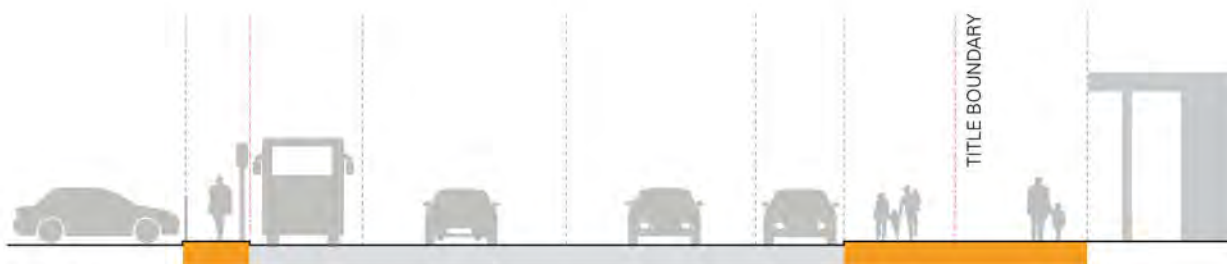
Bligh Street corridor Proposed Rosny Park section

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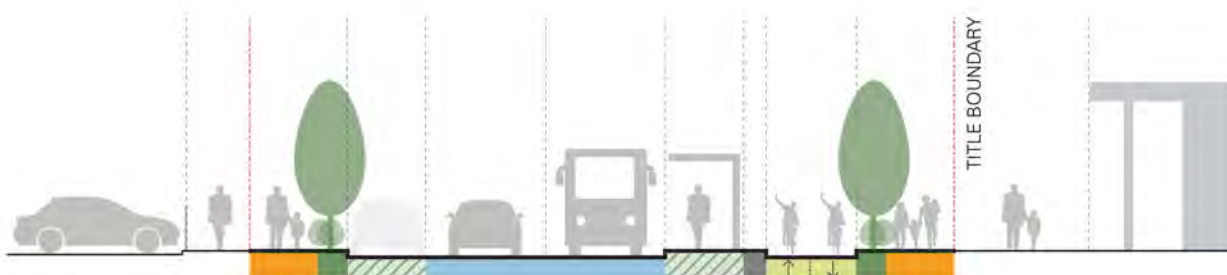


The proposed road cross section for Rosny Park demonstrates the opportunity to create a separated two-way cycleway while balancing the need for accessible bus stops and improved footpaths and greenery.

- Existing carriageway and on street parking
- Shared slow road
- Pedestrian only footpath
- Planting zone
- Two-way cycle way
- Safety separation buffer
- Kerbside extension to facilitate bus stop / extended footpath and landscaping or parallel on-street parking where there is no bus stop



Bligh Street Existing



Bligh Street Proposed (southbound bus stop)



Bligh Street Proposed (northbound bus stop)

Other strategic opportunities

Other strategic opportunities

Eastlands

Outcomes



As a key destination in the City Heart, investment and renewal of Eastlands can contribute to a safer, better connected and more vibrant City Heart.



Key moves

- 1 Explore opportunities for renewal to Bligh Street interface including hospitality and public realm improving pedestrian safety and contributing to a lively streetscape
- 2 Investigate east-west connection through the site to Rosny Parklands aligned with Bayfield Street with opportunity for new hospitality uses overlooking the park
- 3 Opportunity to improve building facades and appearance to the parklands to present the buildings more sensitively to the park address
- 4 Improvements to high profile corner to manage level changes and improve activation, maintain vehicle access
- 5 Investigate the utilisation of rooftop areas to support hospitality, entertainment and recreation offer, taking advantage of outlooks to the adjacent parklands, Kangaroo Bay and to Hobart Central beyond
- 6 Explore opportunities for improvements to entertainment offer of intensification of mixed use development, with greater activation of Bligh Street interface and less dominant car parking areas
- 7 Maintain and improve existing east/west pedestrian connection to Rosny Farm and parklands which is currently not safe
- 8 Opportunity for a gateway building to Rosny Hill Road with outlook to Kangaroo Bay
- 9 Opportunity to improve activation to Bligh St through removal or reprogramming of vehicle ramps and the inclusion of active uses

Case studies

Burwood Brickworks

Burwood, Victoria



- Most sustainable shopping centre in the Southern Hemisphere
- 20.5-hectare former brickworks site
- 2,000m² rooftop urban farm
- Small community room available for booking (40-60 person capacity)
- Within 5 mins walk (400m) to tram stop, 100m to two bus routes

Ringwood Eastland

Ringwood, Victoria



- New redesigned town square for a large regional shopping centre adjacent to Civic Centre
- New urban plaza/square surrounded by outward facing uses (retail and hospitality)
- New Library (REALM Library)
- Within 5 mins walk (400m) to station

Westfield Knox

Knox, Victoria



- New 2000m² library delivered as part of redevelopment of suburban shopping centre
- Other facilities to be delivered include a swim school, basketball court and a multi-purpose community space

Other strategic opportunities

Rosny College

Outcomes

3 5

Re-orienting the college towards Charles Hand Park to maximise and provide new spaces for life-long learning



Key moves

- 1 New spaces for school use and outside school hours for community.
- 2 New Rosny College semi-private open space with hospitality and arts enterprise opportunities.
- 3 Terrace overlooking the park.
- 4 Expanded active recreation spaces for all (refer to Open Space network section for concept design)
- 5 Strengthened circulation routes directing movement away from college and cricket club.

Legend

- City Heart Study Area
- Open space and landscape
- Council site boundary
- Existing building
- Existing pedestrian access

Proposed

- Recreational space
- Open space
- Development footprint
- Building footprint
- Building entry
- Vehicular access
- Potential pedestrian access
- Potential new recreation space

Case studies

South Melbourne Primary School South Melbourne, Victoria



- The vertical school was designed as part of the Ferrars Street Education and Community Precinct
- New streetscape and public realm works to improve accessibility and connectivity – including pedestrian, cyclist and public transport access

Collingwood Yards Collingwood, Victoria



- Redevelopment of a heritage listed former TAFE site into a contemporary arts precinct
- Multi-functional courtyard space for events, festivals and concerts
- Affordable office and creative spaces for artists, creative and social enterprise

Oodi Library Helsinki, Finland



- Three-level library with facilities including a café, restaurant, public balcony, movie theatre, audio-visual recording studios and a makerspace
- Significantly located to be adjacent to major civic institutions and public space

Other strategic opportunities

Warrane

Outcomes



Sensitively renew a valued residential neighbourhood over time to support increased choice and diversity of housing and connectivity to the Town Centre and Rosny Parklands.

Principles

- 1** Encourage well-designed in-fill development on sites with ageing housing at end of life
- 2** Aggregate Homes Tasmania sites to deliver affordable housing at scale on larger sites that are close to the Rivulet and within walking distance to Rosny Park activity centre and bus routes on Bligh Street.
- 3** Upgrade streetscapes (concurrently with development) to ensure that the neighbourhood supports greater residential densities and enhances liveability
- 4** Provide new pedestrian connections to improve access to destinations (Kangaroo Bay Rivulet and Rosny Park) and walkability



Case studies

Goulburn Street Housing

Hobart, Tasmania



- 25 affordable dwellings
- Site area 1,400m²
- 179 dwellings/ha
- 4 storeys
- *Public housing with state government investment*
- *Provides accessible housing for those with disabilities*
- *Two street addresses with built form to address scale and character of streetscapes*

McIntyre Drive Social Housing

Altona, Victoria



- 69 affordable dwellings
- Site area 5,425m²
- 125 dwellings/ha
- 2-3 storeys
- *Creates a communal sense of place around a shared courtyard and community gardens*
- *Architectural expression reflects surrounding residential context and provides sense of identity*
- *Incorporates sustainable design principles*

Rushall Park Housing

Fitzroy, Victoria



- 35 affordable dwellings
- Site area 5,425m²
- 154 dwellings/ha
- 2-3 storeys
- *Independent living for the elderly set in a 150-year old heritage setting*
- *Adjacent to Rushall Train Station*
- *Designed specifically for the accessibility needs of older people with stepless access throughout*
- *Features a courtyard-style garden separating the apartment buildings and winter gardens*

Next steps

4

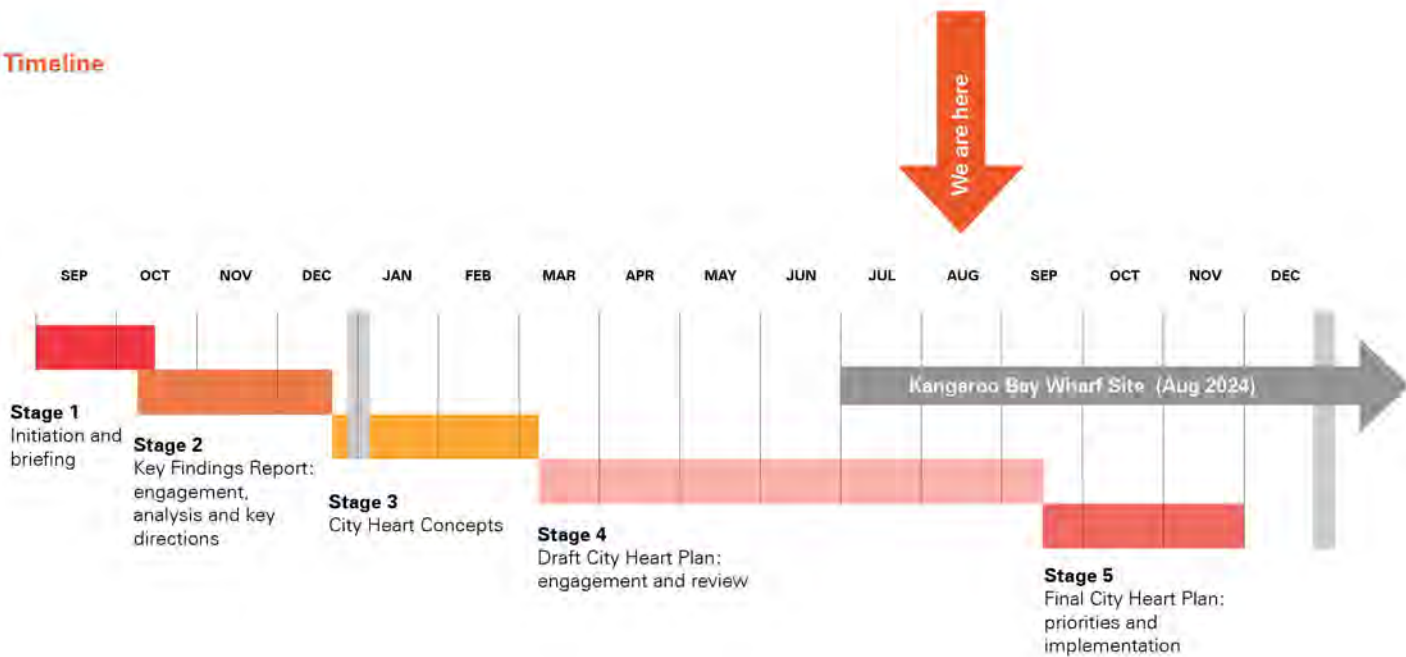
Next Steps
Engagement and timeline

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The next stage of the project focuses on consultation and mapping out an approach to implementation and priorities.

To find out more and provide feedback go to yoursay.ccc.tas.gov.au/cityheart

Timeline



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10-22 Manton Lane
Melbourne Victoria
3000 Australia
T 03 9291 9900
mgsarchitects.com.au

mgs

AFL High Performance Centre

Proposed Engagement & Consultation Plan

Purpose

The key purpose of the community engagement is to provide council with a strong understanding of how the Clarence community feels about having an AFL High Performance Centre. This will help inform key future decisions.

Engagement background

Councils across Greater Hobart have been in early discussions with representatives of the Department of State Growth as site options for the AFL High Performance Centre are starting to be considered.

General benefits that are proposed:

- Upfront and ongoing investment for the preferred site.
- Potential economic benefits from the staff, players and visitors coming to the area.
- The extent of the benefits is likely to differ site to site.

General risks and potential issues already identified:

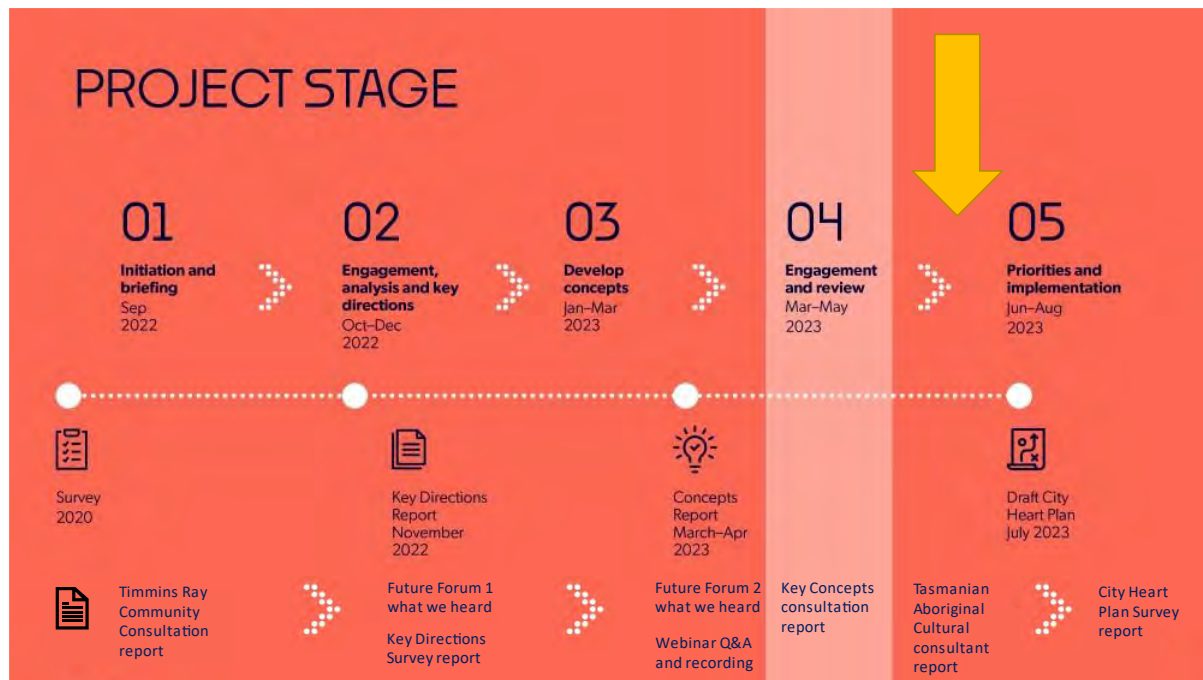
- Potential for a reduction in community access and community use of the site.
- Potential displacement of existing community sports or uses.
- Opportunity costs associated with ideas or plans for sites being changed.

Clarence City Council has been asked to seek community feedback to gain a strong understanding of Clarence community views about having an AFL High Performance Centre.

- (a) in Clarence generally,
- (b) fully located at the Rosny Parklands specifically,
- (c) at dual sites located at Rosny Parkland and Charles Hand Park, and
- (d) at other sites within the City that may be identified as suitable for the purpose.

Even at this very early stage, it is clear that the identified site and any other potential sites will offer benefits as well as risks and opportunity costs that will need to be better understood from the perspective of our Clarence community.

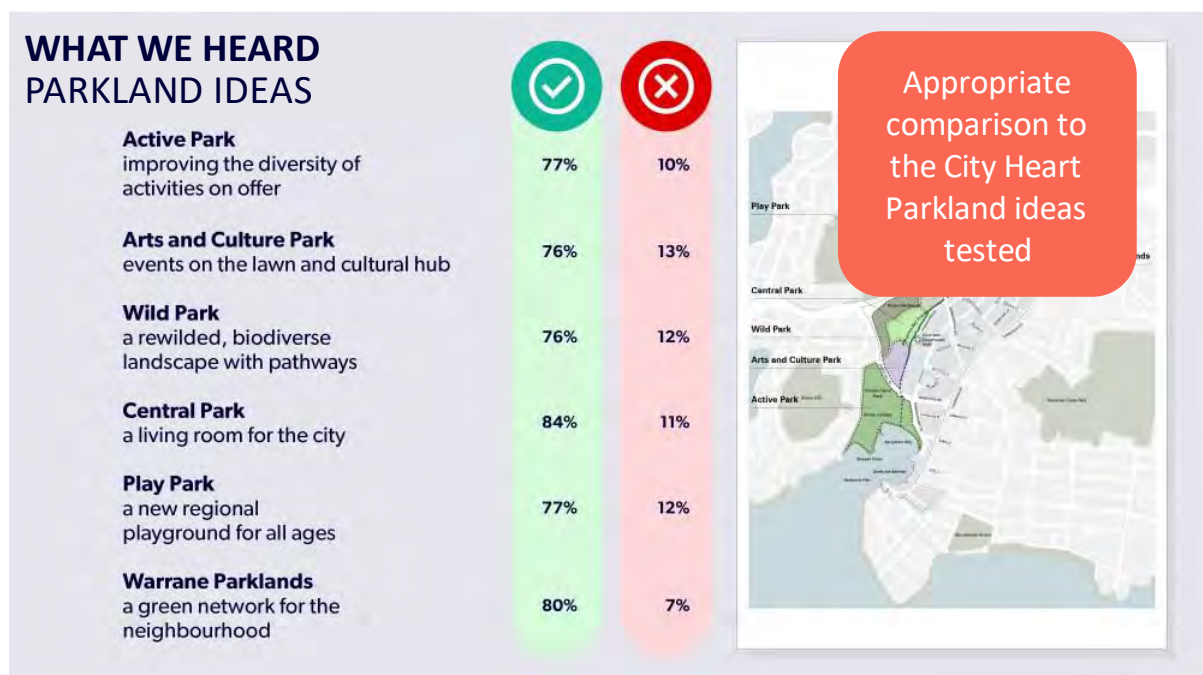
We are proposing that this consultation occur as an extra stage of City Heart Plan consultation - sitting between the Key Concept engagement and Stage 5 (which was expected to be the next step). This new stage will be focused exclusively on Rosny Parklands and on the idea for an AFL High Performance Centre. It will also canvass support for Clarence to be the home of the AFL/AFLW team.



Rosny Parklands has some significant site-specific constraints that will need to be considered alongside the community feedback.

The community has already responded positively to the City Heart ideas for the Rosny Parklands, and this data can be used to compare community sentiment towards an AFL High Performance Centre in the area.

It is important that this new consultation fits in with the stages already concluded and critically this is ensuring that any comparison isn't against the existing state of the parklands but is against the key ideas for parklands within the City Heart Key Concepts (see these recent results below).



Compared with the City Heart's 20 year vision for the Rosny Parklands, an AFL High Performance Centre is a more intensive and challenging use of the site. The need for large flat areas for ovals and a large building are a significant departure from the vision already shared with the community. We will need to position this potential opportunity carefully and transparently with the community as this consultation could be divisive if not carefully managed. There is a high likelihood of strong opinions both for and against.

As part of being open and transparent we also need to communicate clearly around the level of influence council will have with the outcome. Ultimately, the State will be the final decision makers on the site selected. We need to manage future expectations and be very clear what this engagement is and isn't.

Insights we are seeking

The engagement will seek to generate the following insights through a survey and conversations:

General in-principle support for a High Performance Centre:

- Overall support in Clarence for being the home of ALF in Tasmania.
- Level of agreement with the overall benefits and issues of having a High Performance Centre.

Sites and specific support:

- Level of support for being fully located at the Rosny Parkland location.
- Community perceptions of the benefits and issues with the Rosny Parkland site.
- Ratings against City Heart parkland principles.

- Level of support for dual sites located at Rosny Parkland and Charles Hand Park.
- Community perceptions of the benefits and issues with the Rosny Parkland site.
- Ratings against City Heart parkland principles.

Understand level of interest and support for other sites in Clarence

- Provide an opportunity to suggest any other alternative sites.
- Perceptions of benefits and issues with other sites.

Demographics and classification section

- Age
- Gender
- Suburb.

Timings

The consultation will be for a minimum of four weeks as is standard. Where there are School Holidays or Public Holidays we sometimes add extra days so five weeks is an option.

Documents

Relevant documents to provide to the community alongside the survey and other engagement activities, for example:

- Link to the City Heart You Say Page and the most recent consultation results.

- Any designs for sites that are able to be shared.
- Any other information such as State Government process and requirements and anything else that will help provide background understanding and transparency.
- A video discussing the background and inviting the community to share their thoughts to help with getting out key messages out clearly. A short video worked well for City Heart and more recently Seven Mile Beach.

Engagement/Consultation Plan

- High Risk and Complexity – strong need to provide multiple ways to provide feedback.

Indicative program

Stage 1: Preparation and Planning

Timing	Activities
Week Commencing: 25/09/2023	Briefing meetings (internal)
	Development of engagement plan and engagement tools
	Development of communications plan
	Consideration of dates and times for activities <ul style="list-style-type: none"> - Pop up at the Spring Festival Warrane (21st Oct) - Mon-Wed foyer display at Rosny Library - Pop up in the parklands during an activity - Site walks - Webinar and Q&A

Stage 2: Engagement starts (dependant on approval to proceed)

Timing	Activities
Week Commencing: 02/10/2023	Set up Your Say page, documents and survey
	Details for one-on-one sessions/forums/pop ups confirmed
Week Commencing: 10/10/2023	Your Say Page and survey live (if consultation is approved)

Stage 3: Reporting

Timing	Activities
Week Commencing: TBC	Report to Council
Week Commencing: TBC	Share externally via Your Say and other forums

9. MOTIONS ON NOTICE

Nil Items.

10. COUNCILLORS' QUESTION TIME

A Councillor may ask a question with or without notice at Council Meetings. No debate is permitted on any questions or answers.

10.1 QUESTIONS ON NOTICE

(Seven days before an ordinary Meeting, a Councillor may give written notice to the Chief Executive Officer of a question in respect of which the Councillor seeks an answer at the meeting).

Nil

10.2 ANSWERS TO QUESTIONS ON NOTICE

Nil

10.3 ANSWERS TO QUESTIONS WITHOUT NOTICE – PREVIOUS COUNCIL MEETING

Cr Goyne

1. Do we have any news on progress on the greyhound off-lead trial area in Bellerive?

ANSWER

(Acting Head Infrastructure and Natural Assets) I understand that there is a report coming to the next council meeting regarding dog management, which includes some details around greyhounds.

2. Are there rules regarding Councillors' attire in particular politically branded logos and the use of props to make public statements in the Chambers?

ANSWER

(Mayor) The short answer is no there is not. If something is overly offensive that would be at the discretion of the chairperson but there is a number of laptops that come in here from time to time with political slogans and messages on them, there are people who might wear clearly branded shirts or badges, from my perspective that is their choice to do that.

Cr Mulder

1. I refer to the planning permit for the use of 3 Droughty Point Road for a landscape supplies depot which council approved in October last year. Neighbours advise that some of the permit conditions are not being met and that they have informed council of this but they do not appear to have been acted upon. Could you please provide an update Mr Mayor, on the current enforcement conditions and whether or not they have been complied with and if so, what action has been taken?

ANSWER

(Acting Head of City Planning) We are very aware of the operations at 3 Droughty Point Road. It is currently under development so we need to allow them time to comply with the conditions of that planning permit, so we are regularly inspecting the site to make sure that they are in compliance.

2. My question relates to the question I asked at the previous council meeting regarding the Kangaroo Bay apartments. In the further information the Acting Chief Executive Officer has stated that there will be a requirement to vary the Sale and Development Agreement between Council and Hunter Developments. My question is what is the nature of this variation, and particularly does it relate to the Valuer-General's valuation of the sites?

ANSWER

(Head of Governance) Reference to a variation to the Sale and Development Agreement refers to a different development application being presented to Council.

Cr Hulme

Does council gather any statistics or produce any reports about performance against our customer charter?

ANSWER

(Acting Head Strategic Development, Communications and Engagement) Not currently but that is something we are looking into when we deliver our Customer Service Strategy which we hope to be early next year.

Cr James

1. My question follows from Cr Mulder's question in regard to a different development application being presented to council in regard to the Hunter Development. If that is presented to council will that make the earlier development application null and void?

ANSWER

Taken on Notice.

(Further information)

The planning permit for Demolition, 86 Multiple Dwellings, Shops, Food Services and Museum was issued on 9 November 2021. That permit will lapse after two years from the date on which it was granted, unless the development/use has been substantially commenced. New applications do not void approved planning permits.

2. What is the procedure for non-executive members and community members to resolve issues related to the [Aldermen] Statement of Expectations.

ANSWER

(Mayor) I thought we answered that last meeting.

Question contd

Well, it is a bit different if you look at the documentation.

ANSWER

(Mayor) We will come back to you in writing. The process is actually set out in the document quite clearly.

Taken on Notice.

(Further information)

1. Council adopted an Aldermen Statement of Expectations and Aldermen Issues Resolution Procedure on 25 July 2022. The Issues Resolution Procedure applies to the conduct and behaviour of elected members towards each other, council employees and community members. The resolution process outlined applies to Elected Members and Executive Members. This Procedure does not apply to non-executive members or community members.
2. Non-executive members have access to an internal Issue Resolution Procedure (approved April 2016). This procedure provides for non-executive members to resolve issues via self-management, indirect approach, or a direct approach. In circumstances of concerns regarding councillor conduct or behaviour, a non-executive member would be required to refer the matter through their Executive Leader, or through council's People, Safety and Culture Group. While not expressly stated in the procedure, a non-executive member should not approach a councillor directly to seek resolution of an issue.
3. Members of the public may make a complaint about a councillor's conduct through the formal code of conduct process which is administered by the Department of Premier and Cabinet Office of Local Government.

Cr Walker

1. I have an elected member request on this matter but if I could obtain any illumination prior that would be very useful. There is quite a lot of conjecture around a rather large 2m sign that has been erected for accommodation along Otago Bay Road. I would like to understand whether that is a typical thing and some of the actions that might be taken, as it is out of character with the area. My understanding is that with accommodation signage, applications are made to have a small discreet sign added to a street pole but this appears to be two poles being put in for a sign. The sooner that we can come to some understanding with that, the better.

ANSWER

(Acting Head Infrastructure and Natural Assets) We are aware of the sign we are in the process of working through it as it is bigger than anticipated. The sign that was supplied was incorrect, but it was installed, nonetheless. We are in the process of working through whether the business qualifies for the sign and the appropriate size for the sign. If the sign is eligible, it should be smaller such as a street name blade underneath the street sign for that location.

2. As we are notionally owners of TasWater are we able to make representations around the fact that the bills are now being oblique around how pay with credit card you can access it but with most bills as it is with council you can pay with credit card. Can you please raise that with TasWater and use all the influence you can as an 11% shareholder to allow people to easily pay with a credit card?

ANSWER

Taken on notice.

(Further information) TasWater has discussed the matter with Cr Walker and advised that their Customer Assistance team has approached their billing provider to discuss how it can be made clearer for customers who want to pay via credit card. TasWater has undertaken to follow up with Cr Walker when a solution is in place.

Cr Chong

I noticed today that Kingborough Council is looking at compostable dog poo bags. Are ours compostable and if not, can we look at putting that into the budget cycle for next year?

ANSWER

Taken on notice.

(Further information) We do not currently offer compostable dog waste bags to our community. The bags in use are degradable. At present, we do not have access to a facility that can responsibly process organic material containing compostables. However, should a suitable in-vessel composting facility become accessible in southern Tasmania, transitioning to compostable bags and liners will be a key priority for council to enhance our resource recovery endeavours.

10.4 QUESTIONS WITHOUT NOTICE

A Councillor may ask a Question without Notice of the Chairman or another Councillor or the Chief Executive Officer. Note: the Chairman may refuse to accept a Question without Notice if it does not relate to the activities of the Council. A person who is asked a Question without Notice may decline to answer the question.

Questions without notice and their answers will be recorded in the following Agenda.

The Chairman may refuse to accept a question if it does not relate to Council's activities.

The Chairman may require a question without notice to be put in writing. The Chairman, a Councillor or the Chief Executive Officer may decline to answer a question without notice.

11. CLOSED MEETING

Regulation 15 of the Local Government (Meetings Procedures) Regulations 2015 provides that Council may consider certain sensitive matters in Closed Meeting.

The following matter has been listed in the Closed Meeting section of the Council Agenda in accordance with Regulation 15 of the Local Government (Meeting Procedures) Regulations 2015.

11.1 APPLICATIONS FOR LEAVE OF ABSENCE

This report has been listed in the Closed Meeting section of the Council agenda in accordance with Regulation 15 of the Local Government (Meeting Procedures) Regulation 2015 as the detail covered in the report relates to:

- applications by Councillors for a Leave of Absence.

Note: The decision to move into Closed Meeting requires an absolute majority of Council.

The content of reports and details of the Council decisions in respect to items listed in “Closed Meeting” are to be kept “confidential” and are not to be communicated, reproduced or published unless authorised by the Council.

PROCEDURAL MOTION

“That the Meeting be closed to the public to consider Regulation 15 matters, and that members of the public be required to leave the meeting room”.