

Prior to the commencement of the meeting, the Mayor will make the following declaration:

“I acknowledge the Tasmanian Aboriginal Community as the traditional custodians of the land on which we meet today, and pay respect to elders, past and present”.

The Mayor also to advise the Meeting and members of the public that Council Meetings, not including Closed Meeting, are audio-visually recorded and published to Council’s website.

CLARENCE CITY COUNCIL (PLANNING AUTHORITY) MEETING**MONDAY 8 MAY 2017****TABLE OF CONTENTS**

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BUSINESS TO BE CONDUCTED AT THIS MEETING IS TO BE CONDUCTED IN THE ORDER IN WHICH IT IS SET OUT IN THIS AGENDA UNLESS THE COUNCIL BY ABSOLUTE MAJORITY DETERMINES OTHERWISE

COUNCIL MEETINGS, NOT INCLUDING CLOSED MEETING, ARE AUDIO-VISUALLY RECORDED AND PUBLISHED TO COUNCIL'S WEBSITE

1. APOLOGIES

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2. DECLARATIONS OF INTERESTS OF ALDERMAN OR CLOSE ASSOCIATE

(File No 10-03-09)

In accordance with Regulation 8 of the Local Government (Meeting Procedures) Regulations 2015 and Council's adopted Code of Conduct, the Mayor requests Aldermen to indicate whether they have, or are likely to have a pecuniary interest (any pecuniary benefits or pecuniary detriment) or conflict of interest in any item on the Agenda.

3. REPORTS OF OFFICERS

NB: Requests for Deputations will be finalised on the Friday prior to the Meeting

3.1 DEVELOPMENT APPLICATION D-2016/554 - 38 GORDONS HILL ROAD, LINDISFARNE - RETIREMENT VILLAGE
(File No D-2016/554)**EXECUTIVE SUMMARY****PURPOSE**

The purpose of this report is to consider the application made for a Retirement Village at 38 Gordons Hill Road, Lindisfarne.

RELATION TO PLANNING PROVISIONS

The land is zoned Particular Purpose (Zone 3) – Gordons Hill Precinct and subject to the Bushfire Prone Areas, Natural Assets, Waterways and Coastal Protection Areas, Road and Railways Assets, Parking and Access and Stormwater Management Codes under the Clarence Interim Planning Scheme 2015 (the Scheme). In accordance with the Scheme the proposal is a Discretionary development.

LEGISLATIVE REQUIREMENTS

The report on this item details the basis and reasons for the recommendation. Any alternative decision by Council will require a full statement of reasons in order to maintain the integrity of the Planning approval process and to comply with the requirements of the Judicial Review Act and the Local Government (Meeting Procedures) Regulations 2015.

Note: References to provisions of the Land Use Planning and Approvals Act 1993 (the Act) are references to the former provisions of the Act as defined in Schedule 6 – Savings and transitional provisions of the Land Use Planning and Approvals Amendment (Tasmanian Planning Scheme Act) 2015. The former provisions apply to an interim planning scheme that was in force prior to the commencement day of the Land Use Planning and Approvals Amendment (Tasmanian Planning Scheme Act) 2015. The commencement day was 17 December 2015.

Council is required to exercise a discretion within the statutory 42 day period which expires on 11 May 2017.

CONSULTATION

The proposal was advertised in accordance with statutory requirements and 8 representations were received (one outside the statutory advertising period), including 2 from State Government departments, raising the following issues:

- bike paths to the northern side of the Tasman Highway;
- general support for the application;
- impact on wildlife and natural values and no wildlife corridors provided;
- existing natural values of the site not considered;
- the applicant's natural values report is limited in important respects and the development should be redesigned to retain the Eucalyptus vegetation;
- further surveys should be undertaken; and
- contrary to the Environmental Protection and Biodiversity Conservation Act (EPBCA).

The proposal was considered by the Tracks and Trails Committee who raised the following issues:

- perimeter track within the site;
- internal road crossings; and
- upgrade of the existing “Gregson Track” located within the site and in proximity to its southern boundary.

RECOMMENDATION:

A. That the Development Application for a Retirement Village at 38 Gordons Hill Road, Lindisfarne (CI Ref D-2016/554) be approved subject to the following conditions and advice.

1. GEN AP1 – ENDORSED PLANS.
2. The layout of car parking spaces, access aisles, circulation roadways and ramps must be designed and constructed to comply with Section 2 “Design of Parking Modules, Circulation Roadways and Ramps” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking.
3. Parking and vehicle circulation roadways and pedestrian paths must be provided with lighting in accordance with Clause 3.1 “Basis of Design” and Clause 3.6 “Car Parks” in AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting.
4. An amended site plan showing any revisions to the independent living units (in compliance with the approved standard types and variations) as may be required throughout the construction process, depending on the requirements of prospective purchasers, must be submitted and approved by Council’s Manager City Planning prior to applications for building and plumbing permits for each unit. When approved, the amended site plan will form part of the development permit.
5. Prior to the commencement of the development, a Construction Management Plan must be submitted to and approved by Council’s Group Manager Asset Management. The Construction Management Plan must include the following:
 - public safety, amenity and site security;
 - air and dust management;
 - operating hours, noise and vibration controls;
 - stormwater and sediment control;
 - the management of waste collection;
 - car parking and traffic management during the construction stage;
 - measures to protect trees to be retained as part of the landscaping;
 - works are undertaken generally in accordance with “Wetlands and Waterways Works Manual” (DPIWE, 2003) and “Tasmanian Coastal Works Manual” (DPIPWE, Page and Thorp, 2010), and the unnecessary use of machinery within watercourses or wetlands is avoided; and

- measures to minimise the impact of construction vehicles arriving and departing from the site.
6. Access to the development is to be provided from Gordons Hill Road via a round-about, to be constructed by the developer, which will also formalise access to Toogood Drive. The round-about and associated infrastructure must be constructed and commissioned prior to a completion certificate being issued for the first stage of the proposed development, or the development commencing use (whichever occurs first). This access point is to form the sole and principle point of access to the site, however, a secondary construction access point may be utilised whilst the principle point of access is being constructed and until the first stage of the development is in use.
 7. The development must be undertaken, maintained and operated in accordance with the Bushfire Hazard Management Plan and an Emergency Management Plan prepared by Ireneinc and dated March 2017.
 8. GEN M14 – STORAGE AREAS.
 9. ENG A5 – SEALED CAR PARKING.
 10. ENG A7 - REDUNDANT CROSSOVER.
 11. ENG S1 – INFRASTRUCTURE REPAIR.
 12. ENG S10 – UNDERGROUND SERVICES.
 13. ENG M1 – DESIGNS DA.
 14. ENG M5 – EROSION CONTROL.
 15. ENG M6 – CONSTRUCTION FENCING.
 16. ENG M7 – WEED MANAGEMENT.
 17. ENG R1 – ROAD NAMES.
 18. ENG R2 – URBAN ROAD.
 19. ENG R5 – ROAD EXTENSION.

20. All stormwater run-off from impervious surfaces within the site must be treated and discharged from site using Water Sensitive Urban Design principles to achieve stormwater quality and quantity targets in accordance with the State Stormwater Strategy 2010. Detailed engineering designs accompanied with a report on all stormwater design parameters and assumptions (or the MUSIC model) must be submitted to Council's Group Manager Asset Management for approval prior to the issue of building or plumbing permits or certificates of likely compliance. This report is to include the maintenance management regime/replacement requirements for the treatment facility. Easements must be placed over all Council stormwater pipes and overland flow paths through the development in order to facilitate the development.
21. All new dwellings within 50m of the Tasman Highway are to incorporate attenuation measures to achieve a noise level not exceeding the following:
- | | Night Time | Day Time |
|-----------------|------------|----------|
| Living Areas: | 35 db(A) | 45 db(A) |
| Sleeping Areas: | 30 db(A) | 40 db(A) |
22. These attenuation measures are to be certified by a recognised Acoustic Engineer or other appropriately qualified person to the satisfaction of Council's Senior Environmental Health Officer and submitted as part of the application for a Building Permit.
23. Accumulated vegetation or the like must be managed by:
- (a) chipping/mulching for removal or reuse on the site; or
 - (b) removal from the site to a location to be approved by Council's Senior Environmental Health Officer prior to such removal; or
 - (c) removal to a waste management facility licensed to take such material (receipts must be retained as proof of disposal location); or
 - (d) other method approved in writing by Council's Senior Environmental Health Officer.
24. EHO 4 – NO BURNING.
25. LAND 1A – LANDSCAPE PLAN.

26. A weed management plan identifying methods to control weeds, must be submitted to and approved by Council's Group Manager Asset Management prior to commencement of works. The plan must reference any Weeds of National Significance and Declared Weeds under the Weed Management Act and address the spread of soil based pathogens in accordance with the Tasmanian Washdown Guidelines for Weed and Disease Control. The plan must identify the weed species, initial treatment, on-going management and maintenance period thereof. The plan may include manual removal of larger plants and/or chemical control as recommended by the relevant Government department. The plan must include a detailed breakdown of estimated costs.

The works for each development stage must be completed prior to the commencement of the use.

27. The development must meet all required Conditions of Approval specified by TasWater notice dated 20 January 2017 (TWDA 2016/01970-CCC).

ADVICE

The Department of State Growth notes that there is to be stormwater detention on-site to minimise peak flows through the Gordons Hill Road underpass beneath the Tasman Highway. Changes to stormwater discharge into the Tasman Highway reservation will require approval under Section 17B of the Roads and Jetties Act, 1935.

- B. That the details and conclusions included in the Associated Report be recorded as the reasons for Council's decision in respect of this matter.

ASSOCIATED REPORT

1. BACKGROUND

- 1.1. The subject site and the Fairway Rise retirement village and nursing home (38 and 55 Gordons Hill Road respectively) were previously part of the 18 hole Rosny Public Golf Course, which some 20 years ago was redesigned as a 9 hole course confined to the southern side of the Tasman Highway.
- 1.2. The land opposite the subject site at 55 and 75 Gordons Hill Road was rezoned in 2008, initially under the Eastern Shore Planning Scheme 1963 to the Gordons Hill Road Development zone. This was subsequently reiterated under the Clarence Planning Scheme 2007 through Amendment A-2008/10.

These amendments created a unique zone applicable only to the land at 55 and 75 Gordons Hill Road.

- 1.3.** The subject site at 38 Gordons Hill Road represents the balance of the redundant 9 holes of the golf course. The land has long been identified by the State (Housing Tasmania) as an area that may be suitable for community or housing redevelopment. Over recent years the land has been poorly maintained and used informally by mountain bike riders and pedestrians.
- 1.4.** Development Application D-2011/328 was approved in 2012 for an Aged Care Retirement Village comprising a Residential Aged Care Facility (60 beds), 84 independent Living Units, 16 Independent Living Apartments and a Community Building opposite the subject site at 55 and 75 Gordons Hill Road. That proposal was accompanied by a Traffic Impact Assessment outlining that the expected total traffic generated was anticipated to be to the order of 450 trips per day with 60 total peak hour trips and that this number will not impact on the traffic movement along Gordons Hill Road.
- 1.5.** Initially a ‘T’ junction was proposed to provide access to the site. However, independent of the application before Council, Housing Tasmania indicated that it had intended to develop its land at 38 Gordons Hill Road (the land the subject of this report) and for this reason discussions between the applicant, DIER, Housing Tasmania and Council, resulted in an amended proposal including a roundabout to provide access to the land at both 38 and 55 Gordons Hill Road (adjacent to each other on the opposite sides of the road). The cost of construction was an issue that required negotiation with Housing Tasmania outside of the application before Council. Importantly, the permit required the construction of the roundabout as part of that development. Following failed negotiations with Housing Tasmania over the funding of the roundabout, the Aged Care Retirement Village permit was altered through a Section 56 minor amendment providing for an alternative “T” intersection and a Part 5 Agreement providing for a contribution towards a future roundabout. The remaining cost of the roundabout is to be borne by the developer of 38 Gordons Hill Road.

- 1.6.** The Part 5 Agreement was executed on 1 December 2014 and provides that, when called upon to do so, the owners of 55 Gordon Hill Road will be required to pay Council one half of the cost of constructing a roundabout. The roundabout specified in the agreement is to be located on Gordons Hill Road adjacent to Toogood Drive immediately opposite 38 Gordons Hill Road (the site the subject of this application).
- 1.7.** Rezoning of the subject site from Open Space to Particular Purpose Zone 3 – Gordons Hill Precinct was initiated by the Council Meeting of 5 October 2015 before being exhibited later that month. A total of 4 representations were received, after which Council resolved not to modify its Scheme Amendment at its 7 December 2015 Meeting. Following hearings, the Tasmanian Planning Commission handed down its approval of the Scheme amendment with minor modifications, which became operative on 13 June 2016.

2. STATUTORY IMPLICATIONS

- 2.1.** The land is zoned Particular Purpose (Zone 3) – Gordons Hill Precinct and subject to the Bushfire Prone Areas, Natural Assets, Waterways and Coastal Protection Areas, Road and Railways Assets, Parking and Access and Stormwater Management Codes under the Clarence Interim Planning Scheme 2015 (the Scheme).
- 2.2.** The proposal is discretionary because of its land use and it does not meet the Acceptable Solutions under the Scheme.
- 2.3.** The relevant parts of the Planning Scheme are:
- Section 8.10 – Determining Applications;
 - Section 10 – Particular Purpose (Zone 3) – Gordons Hill Precinct; and
 - Section E6.0 – Bushfire Prone Areas, Natural Assets, Waterways and Coastal Protection Areas, Road and Railways Assets, Parking and Access and Stormwater Management Codes.

- 2.4.** Council's assessment of this proposal should also consider the issues raised in any representations received, the outcomes of the State Policies and the objectives of Schedule 1 of the Land Use Planning and Approvals Act, 1993 (LUPAA).

3. PROPOSAL IN DETAIL

3.1. The Site

The site is a 5.15ha vacant lot that was previously part of the 18 hole Rosny Public Golf Course, which some 20 years ago was redesigned as a 9 hole course on the southern side of the Tasman Highway. The site is bounded by the Tasman Highway to the south and Gordons Hill Road to the east opposite the existing Fairway Rise retirement village and nursing home, which was also located on land that previously formed part of the golf course.

The land has varying topography with an ephemeral watercourse bisecting the site in a south-easterly direction toward Gordons Hill Road. Grades vary across the site from around 1:20 to 1:5 with flatter parts of the site located in the lower lying areas in the south-east corner.

The presentation of the site reflects its past use as a golf course and is largely cleared of native vegetation with only small remnant patches of degraded native vegetation and scattered remnant blue gum trees.

The land is located in proximity to reticulated water, sewerage and stormwater systems.

Sothorn Cross Care (SCC) has stated in its application that it has tenure of the land and will obtain full ownership on receipt of a Planning Permit. The title reference is CT 1/164083-1.

3.2. The Proposal

The proposal is for a Retirement Village to be operated by Southern Cross Care (SCC) and will comprise the following:

- 84 two and three bedroom independent living units (ILU) in a mix of stand-alone villas and conjoined duplex units;
- a community building; and
- associated infrastructure, roadworks, access roads and a roundabout, civil works and landscaping.

All proposed buildings will not exceed 7.5m in height above natural ground level and the overall density is 613m² per unit.

There are a total of 157 car parking spaces proposed with all ILU having a garage and tandem parking spaces. A further 27 visitor layby spaces and 46 spaces (mainly around the clubhouse) are also proposed.

The applicants describe the proposal as: “...*living units for elderly residents aged over 55 years, in a mix of duplex “two up two down” and villa units, a Village Community Building to be known as Parkview Lodge and all associated infrastructure, services, roadworks and landscaping. The buildings will be developed by, and remain in the ownership of, SCC and will be managed under SCC’s Village management arrangements under the one title*”.

As a component of the first stage of the development, a multi-purpose community centre will be constructed to service the Village. This will operate in the same manner as the Fairway Rise “Clubhouse” opposite. The centre will provide for a number of social activities, low impact sports, bar, dining, library, management and sales offices and a caretakers residence. As such, the proposal is a separate, self-contained development that is not reliant on the neighbouring Fairway Rise development.

The village surrounds “parkland” providing an informal landscaped circulation route around and through the retirement village. The central village parkland will be landscaped and will provide for paths and paving to courtyards and other outdoor areas, covered ways, retaining walls and embankments, drainage, planting beds, plants and turf. The applicants intend a perimeter fence for security, similar to Fairway Rise.

A Natural Values Report prepared by enviro-dynamics and dated 29 March 2017 has been lodged in support of the application. The assessment concludes that the site is largely cleared of intact native vegetation with only small remnant patches of degraded native vegetation present and scattered remnant blue gum trees. No threatened flora species were recorded and significant fauna habitat was restricted to the blue gums which provide potential foraging for the endangered swift parrot and open grassy areas, which provide foraging habitat for the eastern barred bandicoot. The proposed development will involve the removal of most of the vegetation from the site including the mature blue gums.

Weed Management on the site, in conjunction with the control of weeds along the western margins of the site, will remove a significant seed source of boneseed, blackberry, canary broom, gorse and banana passion fruit and lead to improved management outcomes for the adjoining Gordons Hill Nature Recreation Area.

A Traffic Impact Assessment (TIA) has been prepared by Milan Prodanovic and dated December 2016. A roundabout, pedestrian refuges and bus stops are proposed within the Gordons Hill road reservation. The TIA concludes that the development will not have any adverse traffic impact on the surrounding area and no unfavourable traffic or amenity issues will be created.

Bushfire Hazard Management Plan and an Emergency Management Plan have been prepared by Ireneinc and both dated March 2017 in compliance with the requirements of the Bushfire Code. The plans establish BAL levels and procedures.

An Engineering Infrastructure Report has been prepared by Aldanmark Consulting Engineers dated December 2016 and supplemented by amended plans dated March 2017. The report and plans deal with the designs for roads (including the roundabout and bus laybys), stormwater, sewer and water supply.

Due to proximity to the Tasman Highway, a noise management assessment was previously undertaken by Vipac Engineers for the adjacent Fairway Rise development. The applicant proposes to incorporate the same recommendations for the design of buildings which included double glazed windows; 110mm thick brick veneer walls, building wrap, 50mm cavity, 90x35 MGP10 pine stud frame, minimum R2.5 fibreglass cavity insulation, 10mm plasterboard; and thermal and acoustic insulation to ceilings and roofs. The construction of the units is intended to achieve the following levels:

| | | |
|-----------------|----------------|--------------|
| Living areas: | Night: 35db(A) | Day: 45db(A) |
| Sleeping Areas: | Night: 30db(A) | Day: 40db(A) |

4. PLANNING ASSESSMENT

4.1. Determining Applications [Section 8.10]

“8.10.1 In determining an application for any permit the planning authority must, in addition to the matters required by s51(2) of the Act, take into consideration:

- (a) all applicable standards and requirements in this planning scheme; and*
- (b) any representations received pursuant to and in conformity with ss57(5) of the Act;*

but in the case of the exercise of discretion, only insofar as each such matter is relevant to the particular discretion being exercised”.

Reference to these principles is contained in the discussion below.

4.2. Compliance with Zone and Codes

The proposal meets the Scheme's relevant Acceptable Solutions of the Particular Purpose (Zone 3) – Gordons Hill Precinct zone and the Bushfire Prone Areas, Natural Assets, Waterways and Coastal Protection Areas, Road and Railways Assets, Parking and Access and Stormwater Management Codes with the exception of the following.

Particular Purpose Zone 3 - Gordons Hill Precinct

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|------------|--------------|-------------------------------|--|
| 34.4.1(A1) | Urban Design | No acceptable solution | The application is supported by plans and a report prepared by Phil Lighton Architects |

The proposed variation must be considered pursuant to Performance Criteria P1 of Clause 34.4.1 as follows:.

| Performance Criteria | Comment |
|---|---|
| <p>The design of buildings fronting the Tasman Highway must incorporate variations in wall and rooflines to minimise the visual mass of the building and must ensure:</p> <p>(a) external finishes are limited to muted tones;</p> <p>(b) construction materials and colour of buildings must be complementary to the landscape setting;</p> <p>(c) all plant and equipment, except for those for a dwelling, must be located within the overall building envelope or screened from view;</p> <p>(d) excavation of land should be kept to a minimum and buildings should step down the slope;</p> | <p>The proposed finishes are similar to Fairway Rise, walls are face clay brick and rendered block in soft greys, with roofs in mid to dark grey/green colorbond roofing and spouting. Windows will be powdercoated aluminium in complimentary colours.</p> <p>All plant and equipment to the Community Building will be screened from view. Air-conditioning units for the units will be wall mounted for ease of servicing, but will be placed between the units and concealed from the street.</p> <p>Buildings have been designed to respond to the sloping nature of the site.</p> |

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| <p>(e) application for more than one dwelling must provide for a diversity of dwelling sizes and types to accommodate differing community needs;</p> <p>(f) the design of habitable buildings within 50m of the Tasman Highway and future connecting ramp reservation boundary will be adequately protected from noise as determined by a suitably qualified and experienced person. Appropriate design features may include, but are not limited to, double glazing and location of noise tolerant rooms on the side of the building facing the Highway and future connecting ramp.</p> | <p>The proposal provides for a diversity of dwelling sizes and types from single 2 bedroom individual villa units to 2 and 3 bedroom conjoined duplex units to provide for retirees.</p> <p>An acoustic engineer was engaged at the applicant's neighbouring site at 55 Gordons Hill Road. In accordance with the original recommendations of the engineer, the proponent proposes the units include:</p> <ul style="list-style-type: none"> • double glazed windows; • 110mm thick brick veneer walls, building wrap, 50mm cavity, 90x35 MGP10 pine stud frame, minimum R2.5 fibreglass cavity insulation, 10mm plasterboard; • thermal and acoustic insulation to ceilings and roofs. |
|--|--|

Particular Purpose Zone 3 - Gordons Hill Precinct

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|------------|--------------|--|--------------------------------------|
| 34.4.1(A2) | Urban Design | <p>Development does not involve any of the following:</p> <p>(a) the construction of a new roofed building;</p> <p>(b) additions or alterations to existing buildings.</p> | The proposal involves new buildings. |

The proposed variation must be considered pursuant to Performance Criteria P2 of Clause 34.4.1 as follows.

| Performance Criteria | Comment |
|--|--|
| An application for development must be accompanied by an urban design context report, prepared by a suitably qualified person, containing a context statement and a site analysis plan. The context statement must explain how the proposal has been prepared and how it has responded to the site analysis. | The proposal has been designed by architects and lodged with a report providing an analysis of the site and development. |

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| The site analysis plan must provide details of: | |
| (a) Existing vegetation proposed to be retained; | Most of the vegetation is to be removed with the exception of some trees along the site boundaries. |
| (b) Identification of areas proposed to be landscaped; and | The application is accompanied by a conceptual landscaping design which provides an overview of the design and proposed treatments. |
| (c) Description of proposed landscaping treatments. | |

Particular Purpose Zone 3 - Gordons Hill Precinct

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|----------------|----------|---|--|
| 34.4.4 (A1) | Setbacks | Setbacks from Front Boundaries must meet the following: (a) 6m from Gordons Hill Road; (b) 15m from the Tasman Highway and highway connecting ramp; (c) 4.5m from all other roads. | minimum 6m setback from Gordons Hill Road achieved; does not comply – 4 units within setback, down to a 8.37m for one unit; complies - minimum setback from boundary to Selby Place exceeds 12m. |

The proposed variation must be considered pursuant to Performance Criteria 1 of Clause 34.4.4 as follows.

| Performance Criteria | Comment |
|--|--|
| The setback of the building to all boundaries, including the primary frontage is demonstrated through an Urban Design Context Report as being appropriate having regard to the Objective of this clause. | The relevant Objective of the Development Standard requires buildings should be appropriately located, in the context of its streetscape and its neighbours, allowing for view corridors between buildings and activity spaces within the primary frontage. The applicant's report considers the site context and setbacks to the Tasman Highway and it is considered an appropriate response to the location. |

Natural Assets Code

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|---------------|-------------------------------------|---|---|
| E27.8.1 | Vegetation clearance or disturbance | Maximum clearance of 2500m ² of native vegetation for a Negligible impact. | The proposal involves the clearance of native vegetation from most of the 5.15ha site |

The proposed variation must be considered pursuant to the Performance Criteria 1 (for a negligible impact) of Clause E27.8.1 as follows.

| Performance Criteria | Comment |
|--|---|
| The clearance of native vegetation in excess of 2500m ² is the minimum extent necessary for the development (including bushfire hazard minimisation). | The applicant's ecological consultant notes that the current development layout necessitates the clearance of most of the native vegetation on-site and is in excess of 2500m ² . There is scope for some native vegetation including large trees to be retained around the boundary of the site. Some managed vegetation may be retained within the bushfire hazard management area (refer to bushfire plan). |

Waterway and Coastal Protection Code

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|---------------|---------------------|---|---|
| E11.7.1 | Buildings and Works | Building and works within a Waterway and Coastal Protection Area must be within a building area on a plan of subdivision approved under this planning scheme. | There is no building area on a plan of subdivision. |

The proposed variation must be considered pursuant to the Performance Criteria P1 of the Clause E11.7.1 as follows.

| Performance Criteria | Comment |
|--|---|
| Building and works within a Waterway and Coastal Protection Area must satisfy all of the following: (a) avoid or mitigate impact on natural values; | The WCPA mostly contains non-native vegetation of minimal natural value and as such impacts will be minor. A mature white gum within the WCPA is likely to be removed under the current design. |

| | |
|---|--|
| (b) mitigate and manage adverse erosion, sedimentation and run-off impacts on natural values; | A soil and Water Management plan will be developed and implemented. Best practice measures will be followed to ensure erosion, sedimentation and run-off impacts are managed. |
| (c) avoid or mitigate impacts on riparian or littoral vegetation; | The WCPA does not contain any substantial riparian vegetation other than several patches of remnant rushes. Some of these rushes and other riparian species will be retained or replaced as part of the water sensitive urban design measures. |
| (d) maintain natural streambank and streambed condition, (where it exists); | The drainage line is not considered to contain a natural streambed due to small catchment, modification and past use as a golf course. |
| (e) maintain in-stream natural habitat, such as fallen logs, bank overhangs, rocks and trailing vegetation; | There are no habitat features that could be described as “in stream natural habitat” remaining due to prior modification. |
| (f) avoid significantly impeding natural flow and drainage; | Development incorporates water sensitive urban design features that will avoid impeding natural water flows. |
| (g) maintain fish passage (where applicable); | There are no water bodies suitable for supporting fish populations within the site. |
| (h) avoid landfilling of wetlands; | There are no habitats that could be described as wetlands within the site. |
| (i) works are undertaken generally in accordance with “Wetlands and Waterways Works Manual” (DPIWE, 2003) and “Tasmanian Coastal Works Manual” (DPIPWE, Page and Thorp, 2010), and the unnecessary use of machinery within watercourses or wetlands is avoided. | Works within the WCAP can be conditioned to be undertaken in accordance with best practice standards for managing waterways. |

Road and Railway Assets Code

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|---------------|--------------------------------------|--|---|
| E5.5.1(A3) | Existing road accesses and junctions | The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of 60km/h or less, must not increase by more than 20% or 40 vehicle movements per day, whichever is the greater. | The existing access currently does not receive any regular vehicle movements. |

The proposed variation must be considered pursuant to the Performance Criteria 3 of Clause E5.5.1 as follows.

| Performance Criteria | Comment |
|--|--|
| Any increase in vehicle traffic at an existing access or junction in an area subject to a speed limit of 60km/h or less, must be safe and not unreasonably impact on the efficiency of the road, having regard to: | |
| (a) the increase in traffic caused by the use; | the increase risk of traffic turning manoeuvres is being mitigated by the provision of a roundabout; |
| (b) the nature of the traffic generated by the use; | the nature of the traffic is small residential vehicles with some commercial vehicles and the proposed arrangements are considered satisfactory; |
| (c) the nature and efficiency of the access or the junction; | a new access and improved junction facilities are provided; |
| (d) the nature and category of the road; | Gordons Hill Road is sub arterial and the roundabout is an optimal solution; |
| (e) the speed limit and traffic flow of the road; | the speed limit is 50kph with no alterations considered necessary; |
| (f) any alternative access to a road; | there is no alternative; |
| (g) the need for the use; | the use is appropriate in this location; |

| | |
|--|---|
| (h) any traffic impact assessment; and | a TIA has been provided and supports the proposal; |
| (i) any written advice received from the road authority. | Gordons Hill Road is a Council owned and maintained road – however, the State road authority has advised of drainage requirements in respect of the Tasman Highway. |

Road and Railway Assets Code

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|----------|--|--|---|
| E5.6.1() | Development adjacent to roads and railways | <p>A1.1 Except as provided in A1.2, the following development must be located at least 50m from the rail network, or a Category 1 road or Category 2 road, in an area subject to a speed limit of more than 60km/h:</p> <p>(a) new buildings;</p> <p>A1.2 Buildings, may be:</p> <p>(a) located within a row of existing buildings and setback no closer than the immediately adjacent building;</p> | New buildings are proposed on a vacant lot. The lot is adjacent to the Tasman Highway which is a Category 1 State arterial highway. |

The proposed variation must be considered pursuant to the Performance Criteria 1 of Clause E5.6.1 as follows.

| Performance Criteria | Comment |
|---|---|
| <p>The location of development, from the rail network, or a Category 1 road or Category 2 road in an area subject to a speed limit of more than 60km/h, must be safe and not unreasonably impact on the efficiency of the road or amenity of sensitive uses, having regard to:</p> <p>(a) the proposed setback;</p> <p>(b) the existing setback of buildings on the site;</p> | <p>the minimum setback is not less than 8m and the road surface is 15m away;</p> <p>there is no existing setback on the site;</p> |

| | |
|--|--|
| (c) the frequency of use of the rail network; | not applicable; |
| (d) the speed limit and traffic volume of the road; | the speed limit is 110kph but the development will not impact the efficiency of the road; |
| (e) any noise, vibration, light and air emissions from the rail network or road; | the proposed buildings will be subject to road noise and therefore attenuated as previously described. It is not dissimilar to the neighbouring retirement village where similar issues have been addressed satisfactorily in the same way; |
| (f) the nature of the road; | the road is a Category 1 State arterial highway with limited access and the development has been designed to address this; |
| (g) the nature of the development; | the development is a retirement village with suitable mitigation measures incorporated in the design; |
| (h) the need for the development; | the need for the retirement village has previously been considered through the rezoning; |
| (i) any traffic impact assessment; | the TIA did not address noise impacts associated with the proximity of the highway; |
| (j) any recommendations from a suitably qualified person for mitigation of noise, if for a habitable building for a sensitive use; and | the applicants have undertaken a noise assessment which has resulted in recommended noise mitigation measures to be incorporated into the build. |
| (k) any written advice received from the rail or road authority. | Council has received written advice from DSG which raises concern that there is to be stormwater detention on-site to minimise peak flows through the Gordons Hill Road underpass beneath the Tasman Highway but that no details on these stormwater flows have been included in the advertised documentation. |

| | |
|--|---|
| | It should be noted that changes to stormwater discharge into the Tasman Highway reservation, as appears to be intended, will require approval under Section 17B of the Roads and Jetties Act 1935. It is recommended that the applicants are advised of these concerns. |
|--|---|

Parking and Access Code

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|----------------|-------------------------------------|--|------------------------------------|
| E6.6.3 (A1) | Number of Motorcycle Parking Spaces | The number of on-site motorcycle parking spaces provided must be at a rate of 1 space to each 20 car parking spaces after the first 19 car parking spaces except if bulky goods sales, (rounded to the nearest whole number). Where an existing use or development is extended or intensified, the additional number of motorcycle parking spaces provided must be calculated on the amount of extension or intensification, provided the existing number of motorcycle parking spaces is not reduced. | No motorcycle spaces are proposed. |

The proposed variation must be considered pursuant to the Performance Criteria 1 of Clause E6.6.3 as follows.

| Performance Criteria | Comment |
|---|--|
| The number of on-site motorcycle parking spaces must be sufficient to meet the needs of likely users having regard to all of the following, as appropriate: (a) motorcycle parking demand; (b) the availability of on-street and public motorcycle parking in the locality; (c) the availability and likely use of other modes of transport; (d) the availability and suitability of alternative arrangements for motorcycle parking provision. | The development is a Residential use. Any retiree would be able to garage or park a motorcycle at their unit. Any visitor would easily be able to park as there is an oversupply of car parking. |

Stormwater Management Code

| Clause | Standard | Acceptable Solution (Extract) | Proposed |
|---------------|----------------------------------|---|--|
| E7.7.1 | Stormwater Drainage and Disposal | <p>A stormwater system for a new development must incorporate water sensitive urban design principles R1 for the treatment and disposal of stormwater if any of the following apply:</p> <p>(a) the size of new impervious area is more than 600m²;</p> <p>(b) new car parking is provided for more than 6 cars;</p> <p>(c) a subdivision is for more than 5 lots.</p> | A concept plan has been provided at this stage only. |

The proposed variation must be considered pursuant to the Performance Criteria P2 of Clause E7.7.1 as follows.

| Performance Criteria | Comment |
|---|---|
| A stormwater system for a new development must incorporate a stormwater drainage system of a size and design sufficient to achieve the stormwater quality and quantity targets in accordance with the State Stormwater Strategy 2010, as detailed in Table E7.1 unless it is not feasible to do so. | This is proposed in the concept strategy for which detailed designs can be conditioned. |

5. REPRESENTATION ISSUES

The proposal was advertised in accordance with statutory requirements and 8 representations were received (one outside the statutory advertising period and 2 from State Government departments). The following issues were raised by the representors.

5.1. Bike Paths

Two representors referenced the Council's Bicycle Action Plan 2013-2017 (BAP) which identifies routes ("Moderate Intensity Route 10" and "High Intensity Route 2") to be established along the Tasman Highway in the vicinity of the proposed development and believe the application should allow for or facilitate these bike paths.

- **Comment**

The BAP does identify these routes but also identifies that Council and DSG will facilitate provision. High Intensity Route 2 – Airport to Tasman Bridge identifies a section between Gordons Hill Road and Rosny Hill Road; the action calls for installation of bicycle stand up lanes on approach to Rosny Hill Road intersection as part of intersection re-design. Moderate Intensity Route 10 – Mornington to Rose Bay identifies a section of the Tasman Highway and recommends that proposed DSG works incorporate a pathway to the north. Both of these outcomes have nothing to do with the current application being off-site.

Furthermore, the application before Council is not for a subdivision and therefore the provisions of the Local Government (Buildings and Miscellaneous Provisions) Act, 1993 and Council's Public Open Space Policy 2013 do not apply when determining applications for change of use and development under LUPAA. As such, there is no head of power for Council to seek such provision.

In a further representation received outside the statutory advertising period, one of the representors commented that the proposal does not address connectivity of current tracks through the development site. This concern is more fully addressed at Section 7 of this report.

5.2. General Support

A representor notes the high standard of residential development proposed and the current deteriorated state of the site when compared to the neighbouring Fairway Rise development. The representor believes the proposal is justified by various consultant reports and had many locational benefits given its proximity to Rosny.

- **Comment**

The comments are noted.

5.3. Impact on Wildlife and Natural Values

A representor believes that the development does not take into account the impact on wildlife and the natural values of the Gordons Hill Nature Reserve and contends that the development does not provide wildlife corridors.

- **Comment**

The applicant's Natural Values Report states that fauna issues are not significant and corridors are not required as there are no other areas to link. Notwithstanding this, the bushfire management areas around the perimeter of the site would act as a corridor.

5.4. Existing Natural Values of the Site not Considered

A representor believes that the development does not adequately consider the existing natural values of the site or retention of natural vegetation.

- **Comment**

The applicant's ecology consultant has considered all natural values on the site and is not proposing retention of vegetation. This is a reasonably anticipated outcome given the rezoning of the site for a retirement village where higher urban densities might be expected.

5.5. The Applicant's Natural Values Report is Limited in Important Respects and the Development should be redesigned to retain the Eucalyptus Vegetation

A representor was concerned that the applicant's natural values report is limited as it: did not consider the potential for hollows to develop in the *Eucalyptus viminalis*; the potential for the site to regenerate and be more important for swift parrot (which is a vital consideration with an endangered species); makes no comment regarding the historical records that show the site as regenerating over the last 12 years and the potential for regeneration to continue; did not consider the importance of the *Eucalyptus ovata* as a foraging species for swift parrot limited in assessing whether important trees could be retained; makes no reference to the need to design the development so that some habitat/trees can more safely be retained; and the absence of a detailed collisions risk assessment.

The representor believes that the development should be redesigned to avoid, to the maximum possible extent, loss of *Eucalyptus globulus* forest, scattered *Eucalyptus globulus* trees, individual *Eucalyptus ovata* trees and *Eucalyptus viminalis* forest, being foraging habitat for the swift parrot. Any vegetation which cannot be retained should be off-set. The development should be redesigned to minimise the potential for swift parrot to collide with windows, fences and other structures.

- **Comment**

The applicant's assessment of the significance of natural values is based on the condition of the vegetation at time of the survey only. Whilst the applicant's ecologist acknowledges there are signs that the site is regenerating and will continue to regenerate the assessment can only make comment on the current conditions in terms of impacts on the swift parrot. The impacts of the tree loss in the context of the remaining surrounding vegetation, considered current conditions only and not future conditions when these areas could also regenerate future and provide additional habitat.

Whilst the white gum is mature and may develop hollows this may not occur for many years and as such the significance of the tree as a hollow bearing resource was not considered in relation to the current development.

Black gums (*Eucalyptus ovata*) were not recorded on the site (trees occur on the adjacent site at the head of the drainage line) and as such their significance as a forging resource was not considered as part of the assessment.

The applicant's assessment is based on a design that utilised most of the site. Under the proposal there is limited opportunity to retain these trees. Off-set mechanisms were considered appropriate in the circumstances.

Collision risk is covered under the Scheme and there are specific design measures that have been implemented to minimise the risk. The report identified that trees retained within a developed area present an increased collision risk. The removal of the trees from the site will have a smaller impact than retaining the trees and having an increased strike risk.

5.6. Further Surveys should be Undertaken

Several representors believe that further surveys and reviews are required to demonstrate that the *Chaostola* skipper butterfly is not present at the site, a peer review of the assessment of the EPBCA listed species, potentially a nocturnal survey to determine the presence and extent of the eastern barred bandicoot (EBB) population and a further assessment of other animals that use the site.

- **Comment**

The applicant's ecologist has confirmed that all plants were surveyed for Chaostola skipper larval shelters and distinctive feeding marks and that no evidence of this species was recorded. Based on this and the lack of historical records nearby to the site, the habitat was deemed to be of limited significance to this species. In addition, there are large populations of potential habitat within the Gordons Hill Reserve.

Nocturnal surveys of the site were not undertaken for the EBB or other mammal species as it is assumed that the EBB and other mammals such as wallabies and possums will utilise the site. The impact of the loss of the site for the EBB was addressed but given the secure and common status of this species in Tasmania and the presence of the adjacent Gordons Hill Reserve, the impact is considered to be minor (and will not require referral under the EPBCA). Whilst the development will lead to the loss of an area of foraging habitat for species such as wallabies, these species are not listed under threatened species legislation and as such the development does not need to consider the impacts of the loss of habitat in regard to these species.

5.7. Contrary to the Environmental Protection and Biodiversity Conservation Act (EPBCA)

Several representors commented that if the proposal is not redesigned to better protect the habitat of the swift parrot and to minimise collision risk, it is likely that the project will require referral to the Federal Government under the EPBCA.

- **Comment**

The development and loss of potential habitat for the swift parrot on the site was assessed by the applicant's ecologist against the significant impact criteria of the EPBCA and deemed not to trigger a referral. Advice from the DPIPW Threatened Species Unit also indicated that referral was unlikely to be required for this development (refer below).

6. EXTERNAL REFERRALS

A referral response from the DPIPWE Policy and Conservation Advice Branch (PCAB) confirmed that there are no records on the property of threatened flora listed under the Threatened Species Protection Act, 1995 (TSPA) or Environment Protection and Biodiversity Conservation Act, 1999 (EPBCA). The response noted that given the property is highly disturbed exotic and urban vegetation, it is anticipated that the proposed activity is unlikely to impact on listed flora species. Given the proposed development is on highly disturbed land, PCAB has no concerns regarding the impact of the development on threatened fauna on-site. However, PCAB noted that there are records in the area for Swift parrots (*Lathamus discolor*), listed as endangered under the TSPA and the EPBCA and the site is within 500m of potential foraging habitat (*Eucalyptus globulus* dry forest and woodland).

There are a number of plant species that are declared weeds under the Weed Management Act, 1999 recorded within 5km of the proposed development. PCAB recommends that the DPIPWE (2015) Weed and Disease Planning and Hygiene Guidelines - Preventing the spread of weeds and diseases in Tasmania be adhered to during any development activities to prevent the spread of any weeds and that any weeds present on the property be properly managed.

Comments from the Department of State Growth (DSG) note that the development takes account of the future transport infrastructure issues and that it is not intended to modify or alter pedestrian pathways and tracks outside the site, with the proposed development being fenced for security purposes. DSG further notes that there is to be stormwater detention on-site to minimise peak flows through the Gordons Hill Road underpass beneath the Tasman Highway and expects changes to stormwater discharge into the Tasman Highway reservation will require approval under Section 17B of the Roads and Jetties Act, 1935.

The proposal was referred to TasWater, which has provided a number of conditions to be included on the planning permit if granted.

7. COUNCIL COMMITTEE RECOMMENDATION

A submission was received from Council's Tracks and Trails Recreational Planner in respect of the application shortly after it was lodged. The submission sought a perimeter track within the site itself, internal road crossings and an upgrade of the existing "Gregson Track" located within the site and in proximity to its southern boundary.

Unlike subdivision applications which are subject to the provisions of the Local Government (Buildings and Miscellaneous Provisions) Act, 1993 and Council's Public Open Space Policy 2013, there is no head of power for Council to seek open space for the provision of tracks and trails when determining applications for change of use and development under LUPAA. The applicant has already noted its intention to prevent general public access through the site for reasons of security (which is also the case on the existing site on the opposite side of Gordons Hill Road). As such there is no scope to achieve these outcomes on land owned by the Crown and which is to be transferred to Southern Cross Care. It is also considered reasonable as it is private property and of concerns about the security and safety of elderly residents.

8. STATE POLICIES AND ACT OBJECTIVES

8.1. The proposal is consistent with the outcomes of the State Policies, including those of the State Coastal Policy.

8.2. The proposal is consistent with the objectives of Schedule 1 of LUPAA.

9. COUNCIL STRATEGIC PLAN/POLICY IMPLICATIONS

There are no inconsistencies with Council's adopted Strategic Plan 2016-2026 or any other relevant Council Policy.

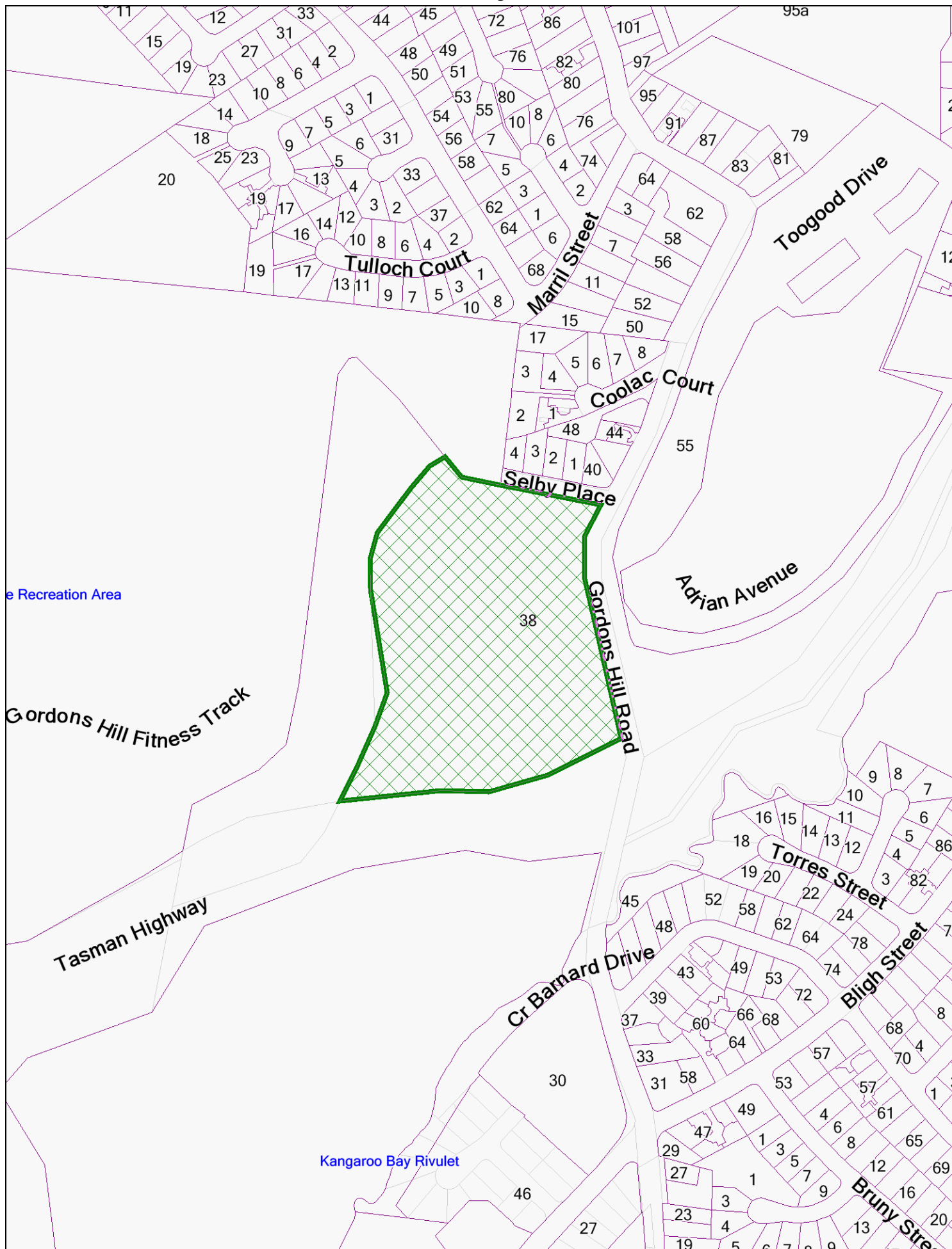
10. CONCLUSION

The proposal for a Retirement Village at 38 Gordons Hill Road, Lindisfarne is considered to meet the development standards of the Scheme and is therefore recommended for approval subject to reasonable and relevant conditions.

Attachments: 1. Location Plan (1)
2. Proposal Plan (30)
3. Site Photo (1)

Ross Lovell
MANAGER CITY PLANNING

Clarence City Council



Disclaimer: This map is a representation of the information currently held by Clarence City Council. While every effort has been made to ensure the accuracy of the product, Clarence City Council accepts no responsibility for any errors or omissions. Any feedback on omissions or errors would be appreciated. Copying or reproduction, without written consent is prohibited. **Date:** Wednesday, 3 May 2017 **Scale:** 1:4,410 @A4



DEVELOPMENT APPLICATION
SPRINGHAVEN
LIFESTYLE VILLAGE
38 GORDONS HILL ROAD
LINDISFARNE TASMANIA

PRINCIPAL
SOUTHERN CROSS CARE (TAS) INC.

PROJECT MANAGER
STANTON MANAGEMENT GROUP

ARCHITECT
PHILP LIGHTON ARCHITECTS

CONSULTING STRUCTURAL,
CIVIL & HYDRAULIC SERVICES
ALDANMARK

BUILDING SERVICES ENGINEER
ENGINEERING SOLUTIONS TASMANIA

BUILDING SURVEYOR
PITT & SHERRY

LAND SURVEYOR
LEARY & COX

TOWN PLANNER & BUSHFIRE
HAZARD MANAGEMENT
IRENEINC

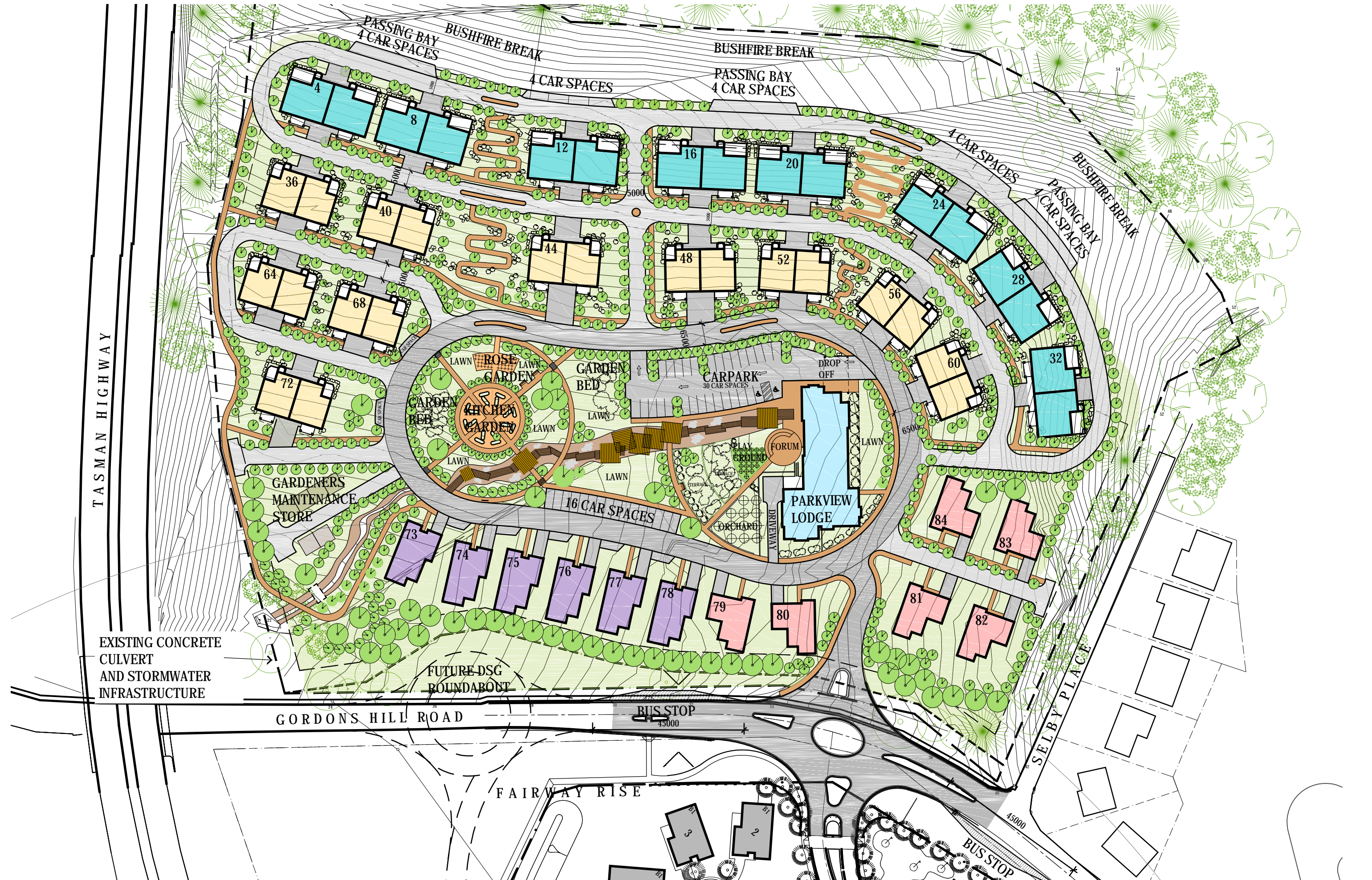
TRAFFIC ENGINEER
MILAN PRODANOVIC



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|--------|----------------------|------|--|-------------|------------|---------|--|-------------|-----------|-------|
| Rev No | Revision Description | Date | <div>PhilpLighton Architects</div> <div>49 SANDY BAY ROAD HOBART TASMANIA 7004 T +61 (3) 6223 2333 F +61 (3) 6223 2433 hobart@philplighton.com.au</div> | Dates | 19.01.2017 | Project | SOUTHERN CROSS CARE (TAS) INC. SPRINGHAVEN LIFESTYLE VILLAGE 38 GORDONS HILL ROAD LINDISFARNE | Project No: | 004.13122 | DA100 |
| - | - | - | | Drawn | RH | Checked | - | Drawing No: | A | |
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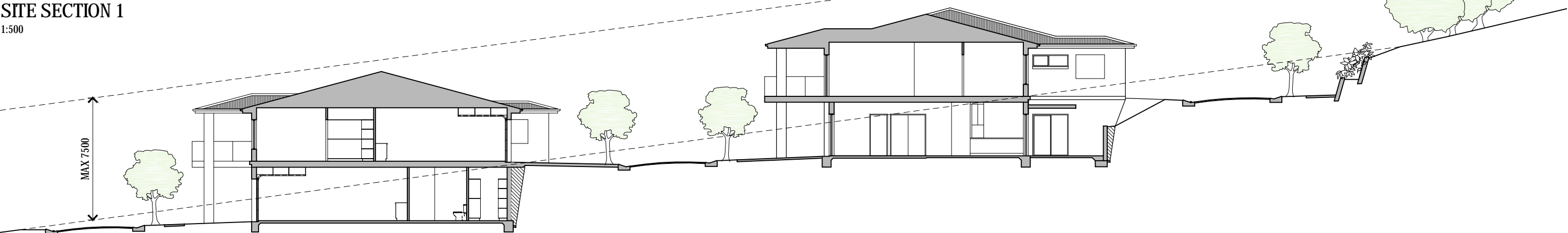


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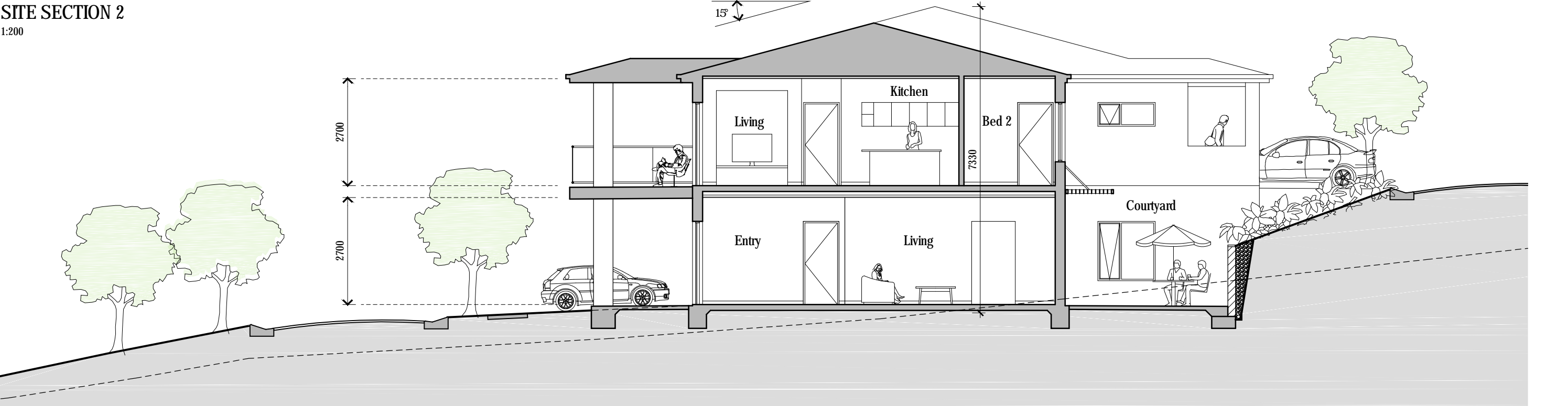
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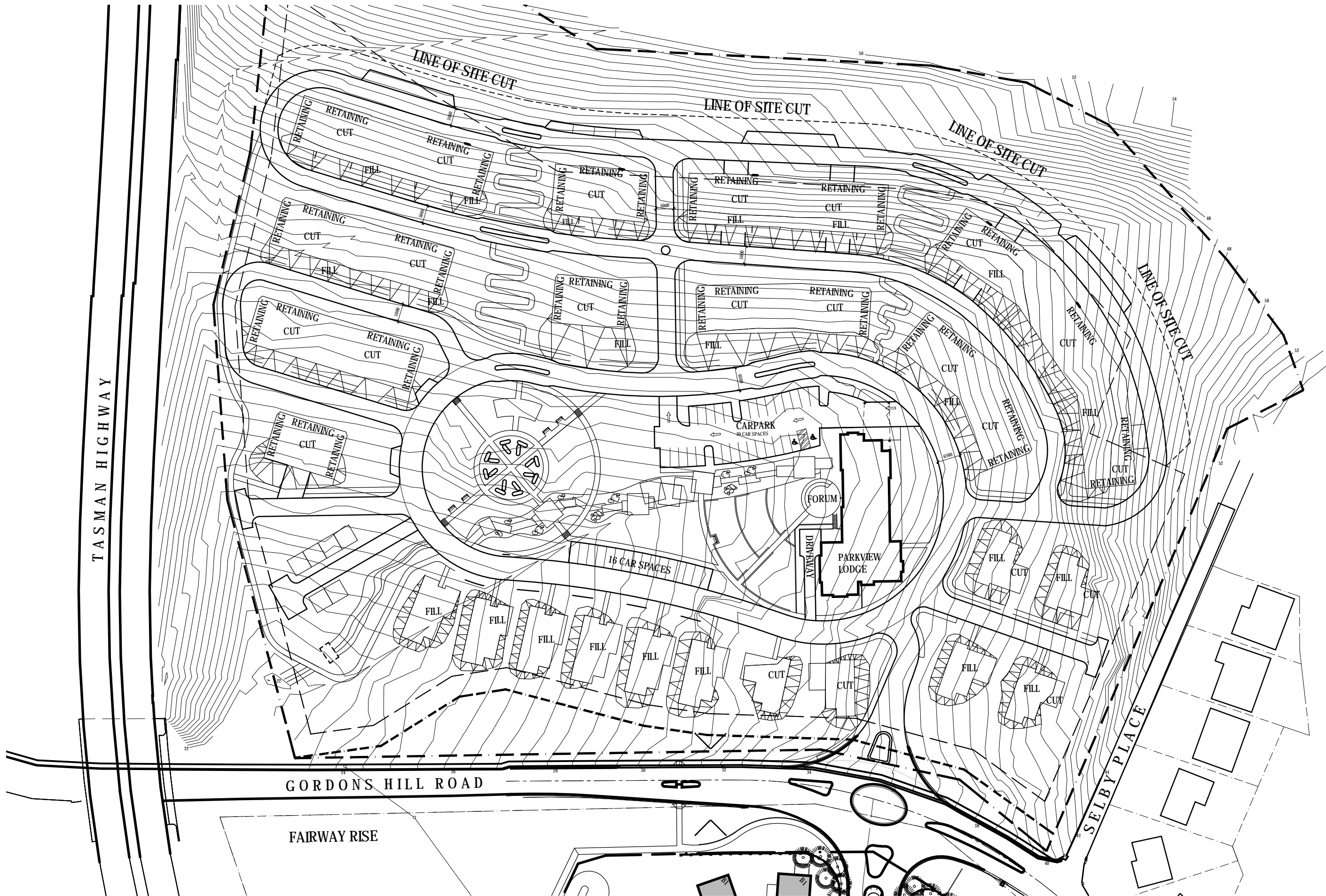
SITE SECTION 1
1:500



SITE SECTION 2
1:200



SITE SECTION 3
1:100



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| Rev No | Revision Description | Date |  |  |  | Date: 19.01.2017 | Project: SOUTHERN CROSS CARE (TAS) INC. SPRINGHAVEN LIFESTYLE VILLAGE 38 GORDONS HILL ROAD LINDISFARNE | Project No: 004.13122 |
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| | | | | | | Checked: - | Title: SITE CUT AND FILL - INDICATIVE | Rev No: A |
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DA104



| Rev No | Revision Description |
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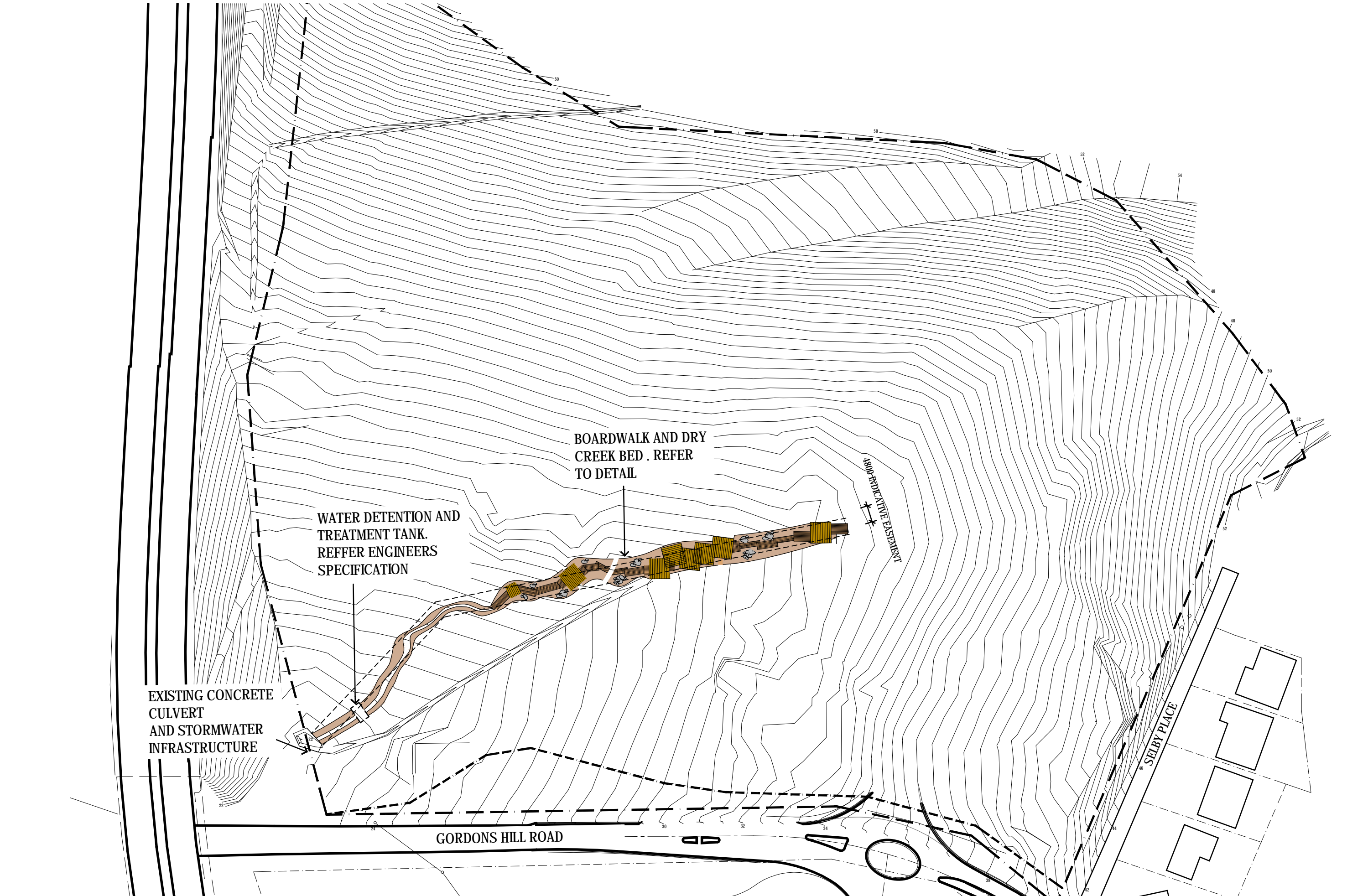
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Date: 19.01.2017
Drawn: BF
Checked:
Scale @ A3: 1:200

Project: SOUTHERN CROSS CARE (TAS) INC.
SPRINGHAVEN LIFESTYLE VILLAGE
38 GORDONS HILL ROAD
LINDISFARNE
Title: SITE - SETBACK PLAN

Project No: 004.13122
Drawing No:
Rev No: A

DA105

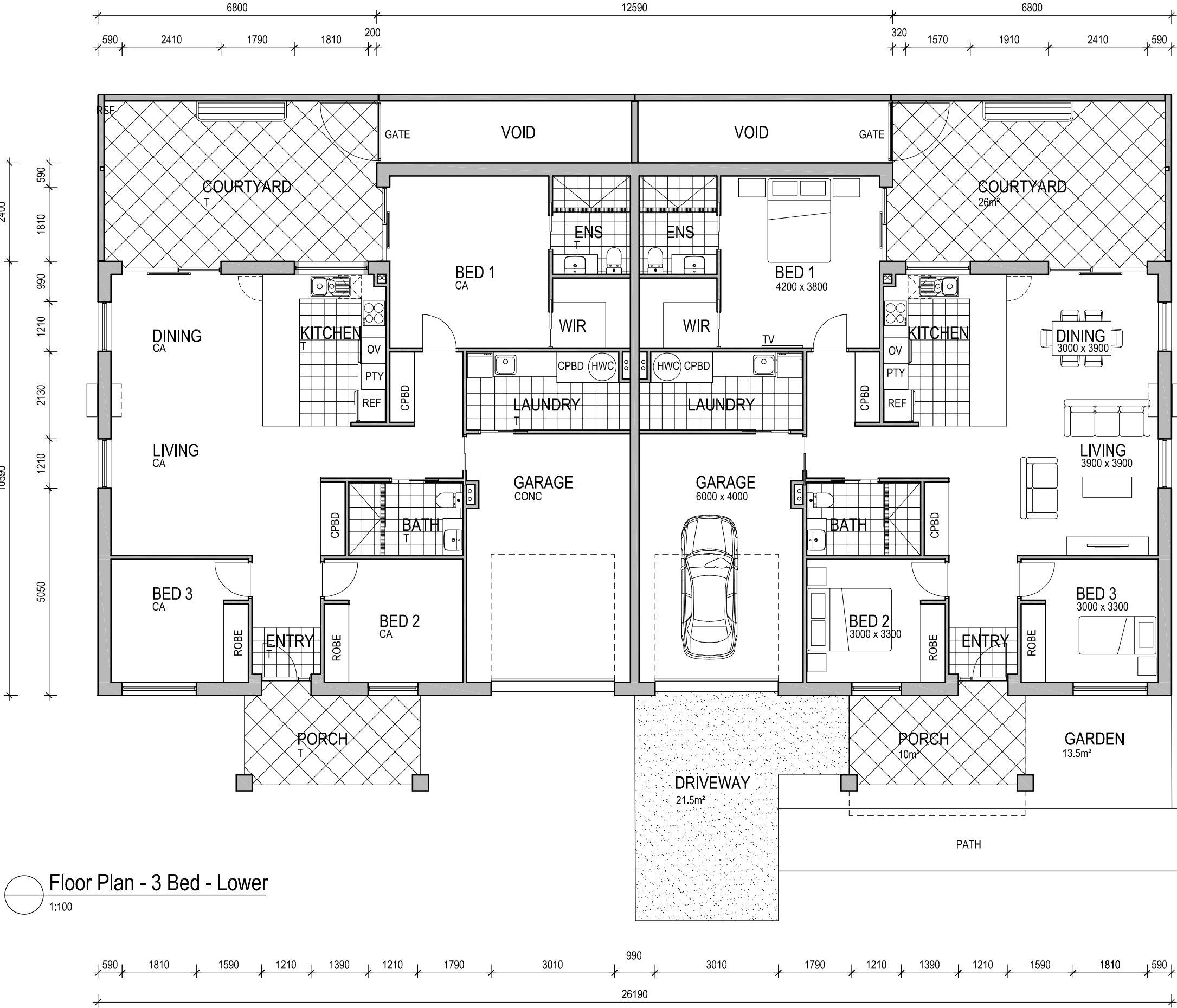


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| Rev No | Revision Description | Date |  |  |  | Date: 19.01.2017 | Project: SOUTHERN CROSS CARE (TAS) INC. SPRINGHAVEN LIFESTYLE VILLAGE 38 GORDONS HILL ROAD LINDISFARNE | Project No: 004.13122 |
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| | | | | | | Checked: - | | Rev No: A |
| | | | | | | Scale @ A3: 1:1000 | Title: SITE PLAN - WATERCOURSE | DA106 |

Garden & Recreation Space

| | |
|-----------|--------|
| Porch | 10m² |
| Garden | 13.5m² |
| Courtyard | 26m² |
| TOTAL | 49.5m² |

Driveway 21.5m²



A Series

| LEGEND | |
|--------|-----------------------------|
| AC | AIR CONDITIONING UNIT |
| CA | CARPET |
| CONC | SEALED CONCRETE |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |
| SHR | SHOWER |
| SWB | SWITCHBOARD |
| T | TILE |
| TAP7 | EXTERNAL TAP |

| | |
|--------|---------------|
| D## | DOOR NUMBER |
| W # | WINDOW NUMBER |

NOTES
KITCHEN, BATH 1, POWDER & LAUNDRY 1 - REFER DRAWINGS AA-08-10 INCLUSIVE
EXTERNAL & INTERNAL DETAILS -REFER 'ILU STANDARD' AE DRAWING SERIES
OPTION A - REFER DRAWING AA-10
REFER STAGE 2 PLANS FOR DRYING COURT, DRIVEWAY, PATHS & SCREENS

CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT

| Rev | Description | Dwn | Date |
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| A | GENERAL REVISIONS | JPI | 28.02.17 |

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Southern Cross Care (Tas) Inc.
Caring Across Tasmania



SMG
STANTON MANAGEMENT GROUP

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Hobart / Launceston / Burnie

Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
38 Gordons Hill Road
Lindisfame

| | | | |
|------------|--------------|------------------------------|--------------|
| Title | | A2 Floor Plan - Lower | |
| Drawn by | JPI | Appr | PG |
| Scale | 1:100 @ A3 | Date | 14.02.17 |
| Drawing No | AA-01 | | Rev A |

A Series

LEGEND

| | |
|-------|-----------------------------|
| AC | AIR CONDITIONING UNIT |
| CA | CARPET |
| CONC | SEALED CONCRETE |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |
| SHR | SHOWER |
| SWB | SWITCHBOARD |
| T | TILE |
| TAP7 | EXTERNAL TAP |

D## DOOR NUMBER

| | |
|---|---------------|
| W | WINDOW NUMBER |
| # | |

NOTES

KITCHEN, BATH 1, POWDER & LAUNDRY 1 - REFER DRAWINGS AA-08-10 INCLUSIVE
EXTERNAL & INTERNAL DETAILS -REFER 'ILU STANDARD' AE DRAWING SERIES
OPTION A - REFER DRAWING AA-10
REFER STAGE 2 PLANS FOR DRYING COURT, DRIVEWAY, PATHS & SCREENS

CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| | | | |
| A | GENERAL REVISIONS | JPI | 28.02.17 |

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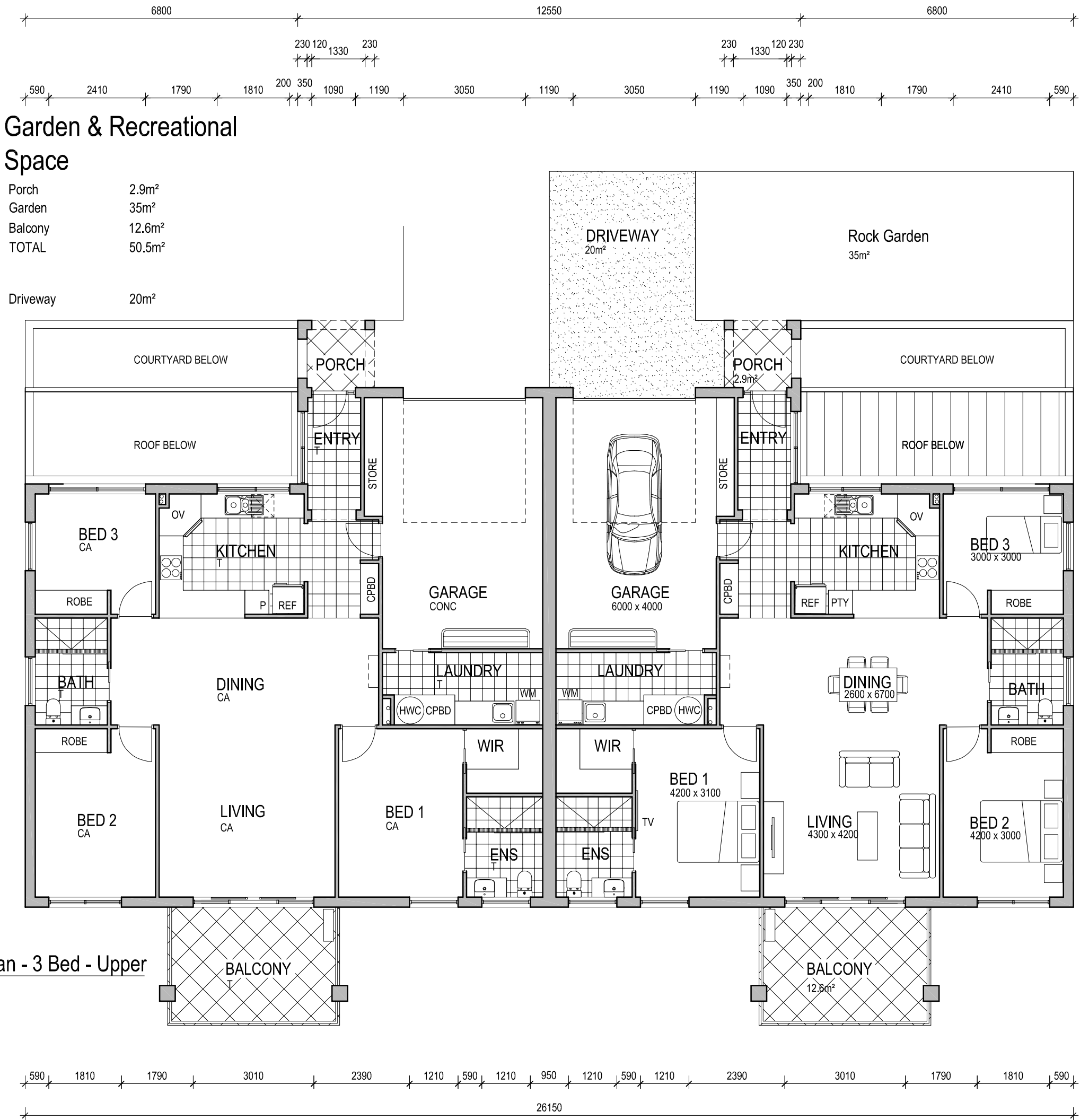
Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
38 Gordons Hill Road
Lindisfame

Title **A2 Floor Plan - Upper**

Drawn by JPI Appr PG Project Number

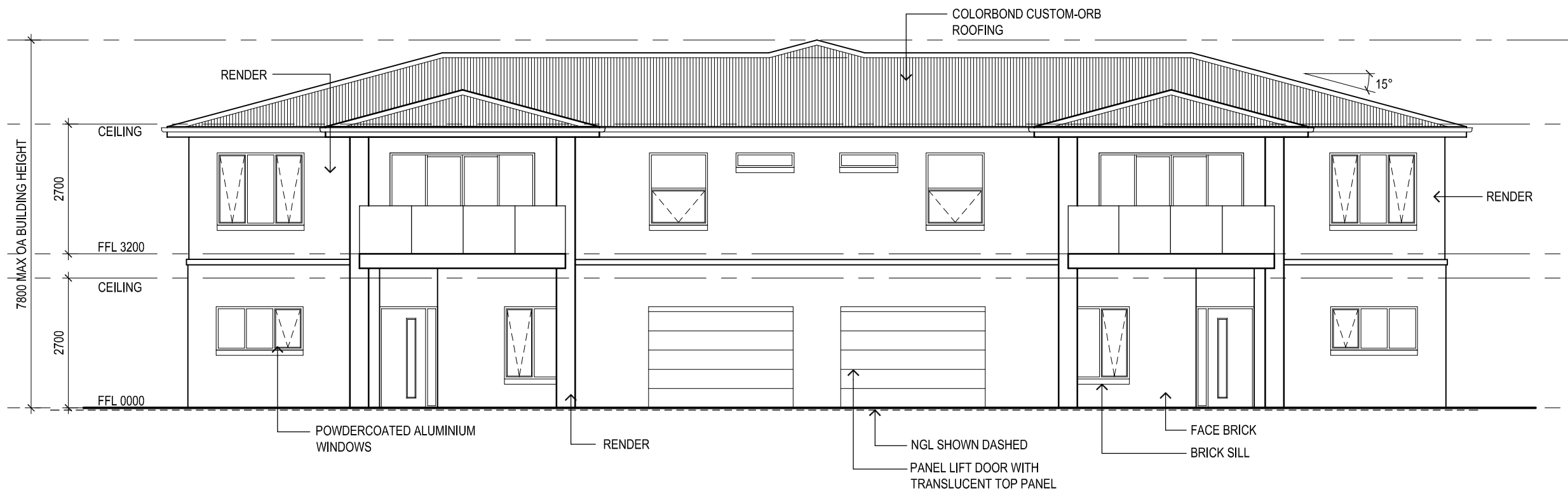
Scale 1:100 @ A3 Date 14.02.17 004.13122

Drawing No **AA-02** Rev **A**



Floor Plan - 3 Bed - Upper
1:100

A Series



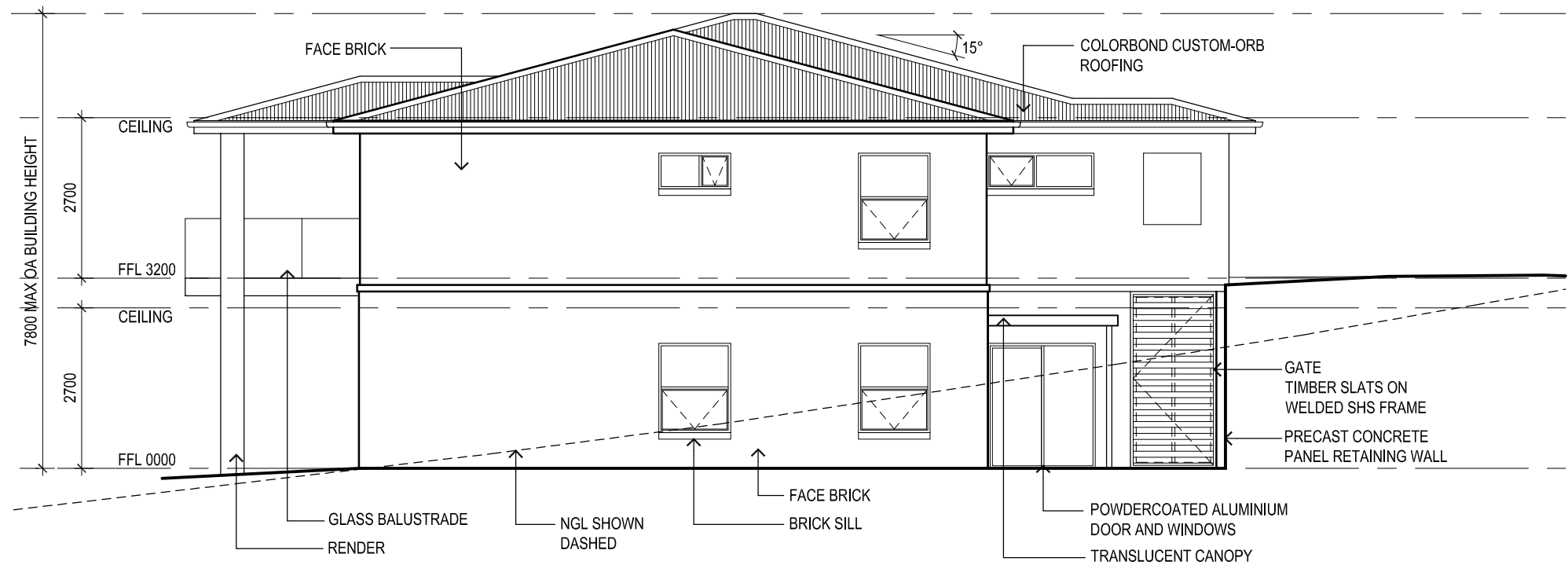
LEGEND

| | |
|-------|-----------------------------|
| A/C | AIR CONDITIONING UNIT |
| CJ | CONTROL JOINT |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |

NOTES

REFER 'ILU STANDARD' AE DRAWING SERIES FOR DETAILS & OPTIONS

01 Front Elevation - Lower Level - 3 Bed
1:100



02 Side Elevation 1 - 3 Bed
1:100

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| | | | |
| A | GENERAL REVISIONS | JPI | 28.02.17 |

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Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
38 Gordons Hill Road
Lindisfame

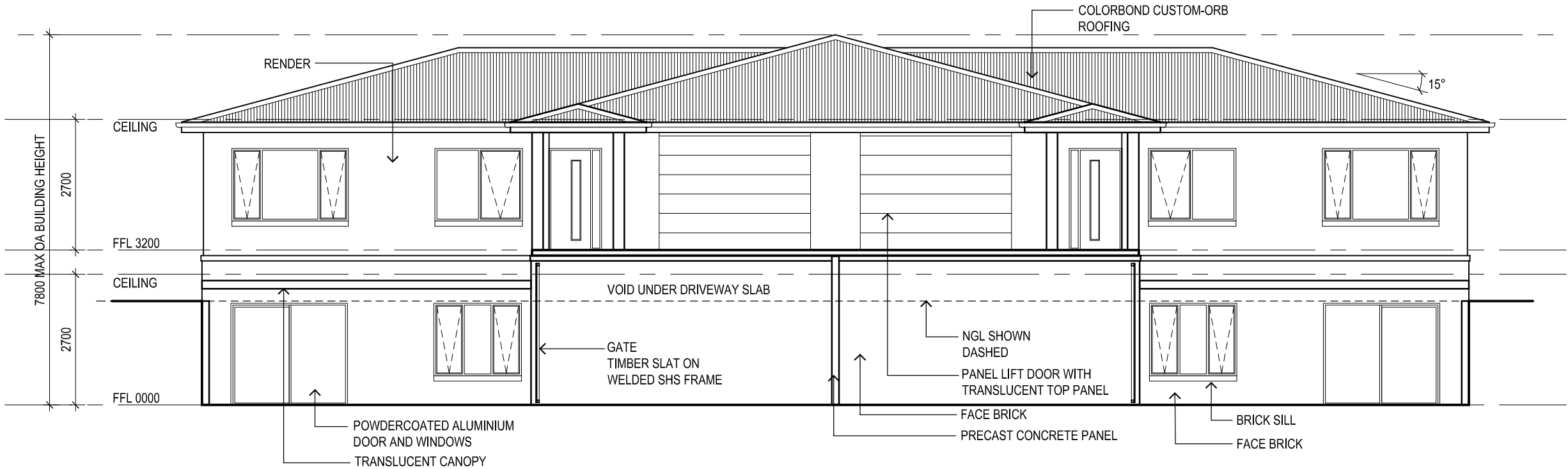
Title **A2 Elevations 1**

| | | | | |
|------------|------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:100 @ A3 | Date | 15.02.17 | 004.13122 |
| Drawing No | AA-03 | | | Rev A |

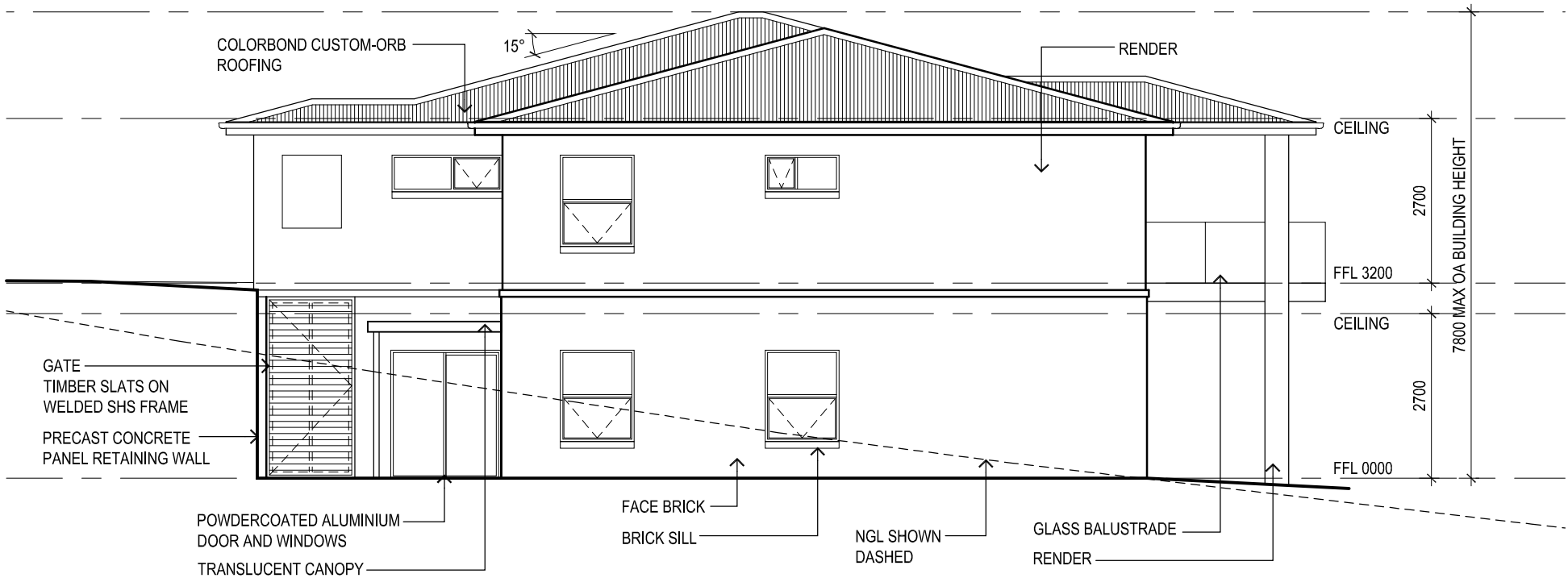
A Series

- LEGEND**
- A/C AIR CONDITIONING UNIT
 - CJ CONTROL JOINT
 - DP DOWNPIPE
 - GAS INSTANTANEOUS GAS HOT WATER
 - LINE CLOTHESLINE
 - METER METER BOX
 - NBN NATIONAL BROADBAND NETWORK

NOTES
REFER 'ILU STANDARD' AE DRAWING
SERIES FOR DETAILS & OPTIONS



01 Front Elevation - Upper Level - 3 Bed
1:100



02 Side Elevation 2 - 3 Bed
1:100

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| | | | |
| A | GENERAL REVISIONS | JPI | 28.02.17 |

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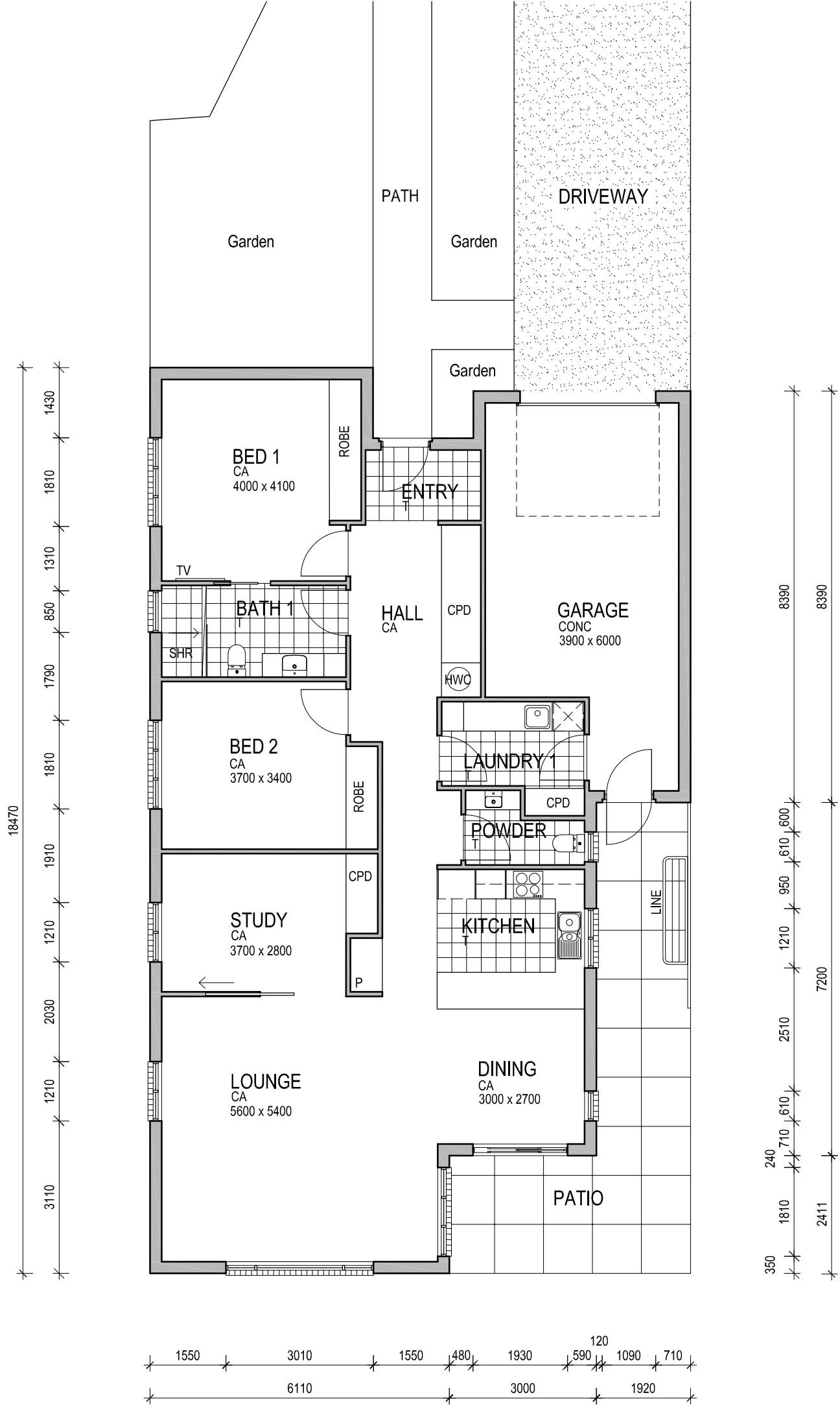
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Springhaven Lifestyle Village
38 Gordons Hill Road
Lindisfame

Title **A2 Elevations 2**

| | | | | |
|------------|------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:100 @ A3 | Date | 15.02.17 | 004.13122 |
| Drawing No | AA-04 | | | Rev A |



B Series

| LEGEND | |
|--------|-----------------------------|
| A/C | AIR CONDITIONING UNIT |
| CJ | CONTROL JOINT |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |

NOTES
REFER 'ILU STANDARD' AE DRAWING
SERIES FOR DETAILS & OPTIONS

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| | | | |
| A | GENERAL REVISIONS | JPI | 28.02.17 |

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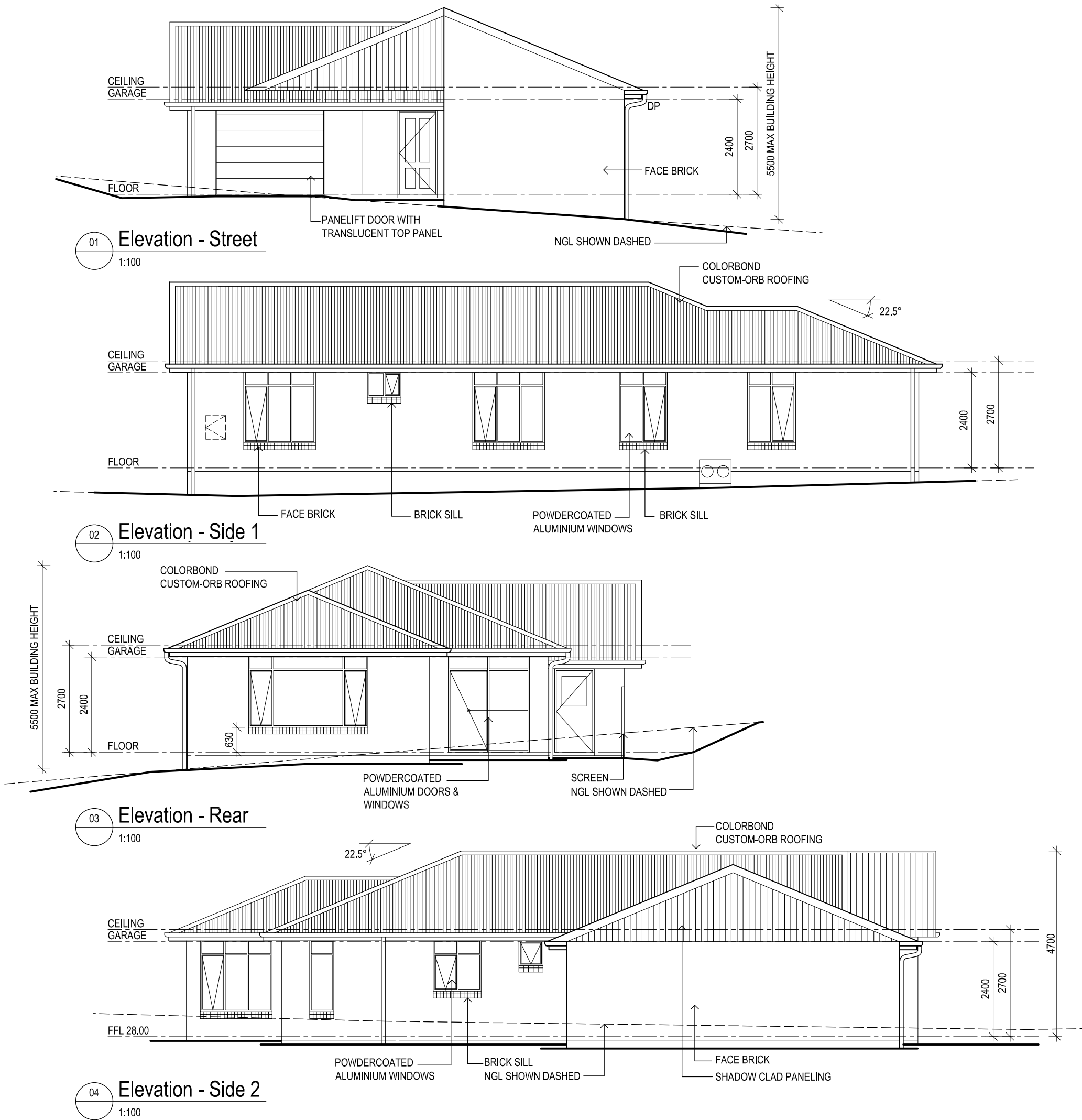


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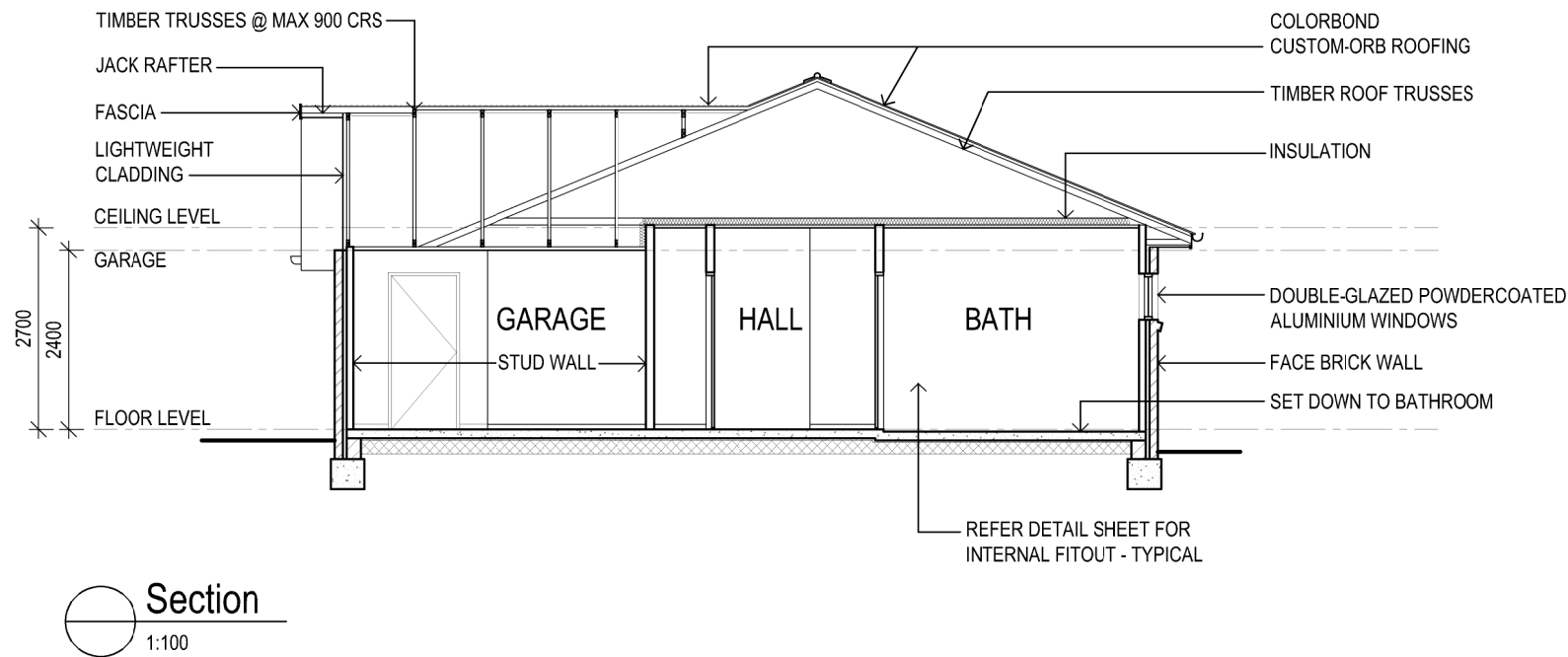
Hobart / Launceston / Burnie

| | | | |
|----------------|---|-----------|----------|
| Project | Southern Cross Care (Tas) Inc. Springhaven Lifestyle Village 38 Gordons Hill Road Lindisfame | | |
| Title | B2 Elevations 1 | | |
| Drawn by | JPI | Appr | PG |
| Scale | 1:100 @ A3 | Date | 19.01.17 |
| Drawing No | AB-02 | | Rev A |
| Project Number | | 004.13122 | |



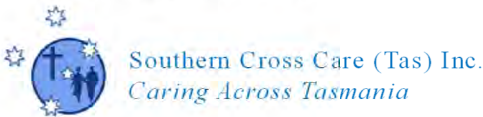
B Series

NOTES
REFER STANDARD DRAWING DETAILS
DRAWINGS AE01 - AE03 INCLUSIVE



| Rev | Description | Dwn | Date |
|-----|-------------|-----|------|
| | | | |
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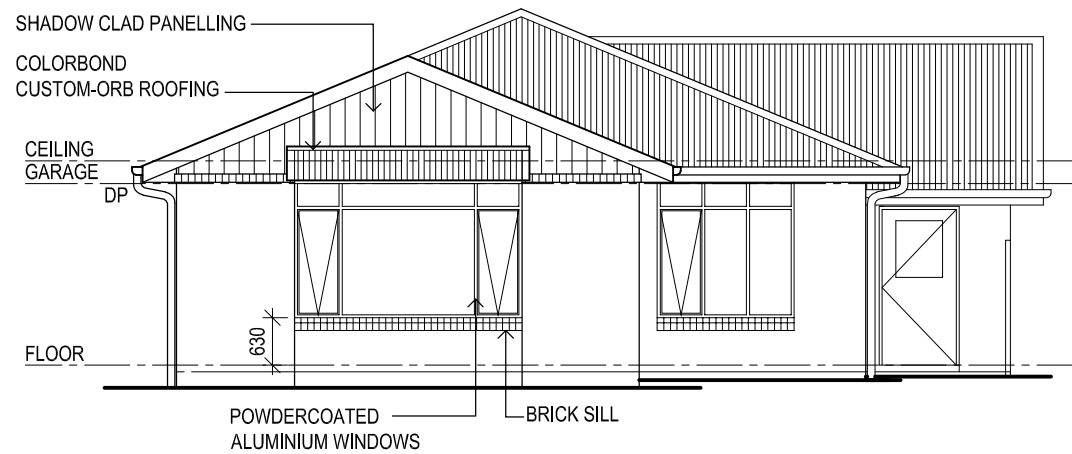
Project **Southern Cross Care (Tas) Inc.**
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Lindisfarne

Title **B1 & B2 Section**

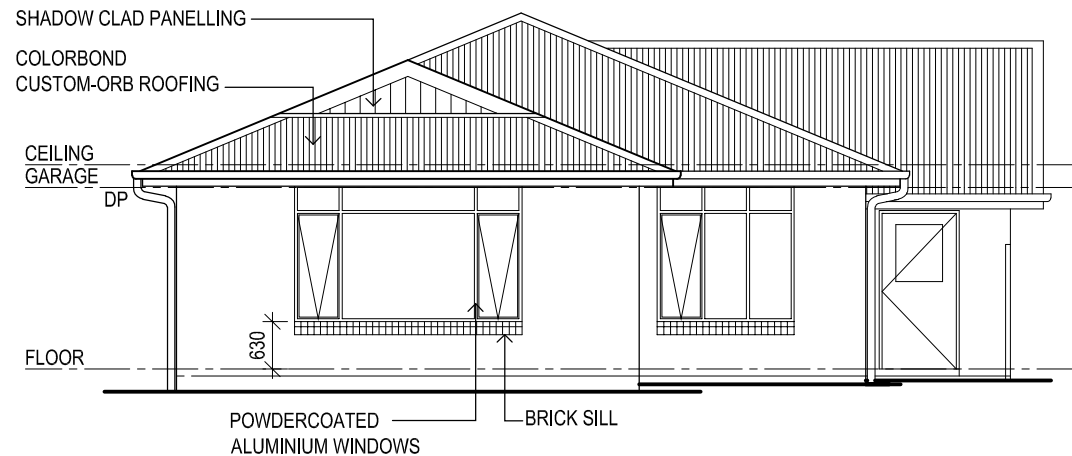
Drawn by JPI Appr PG Project Number

Scale 1:100 @ A3 Date 14.02.17 004.13122

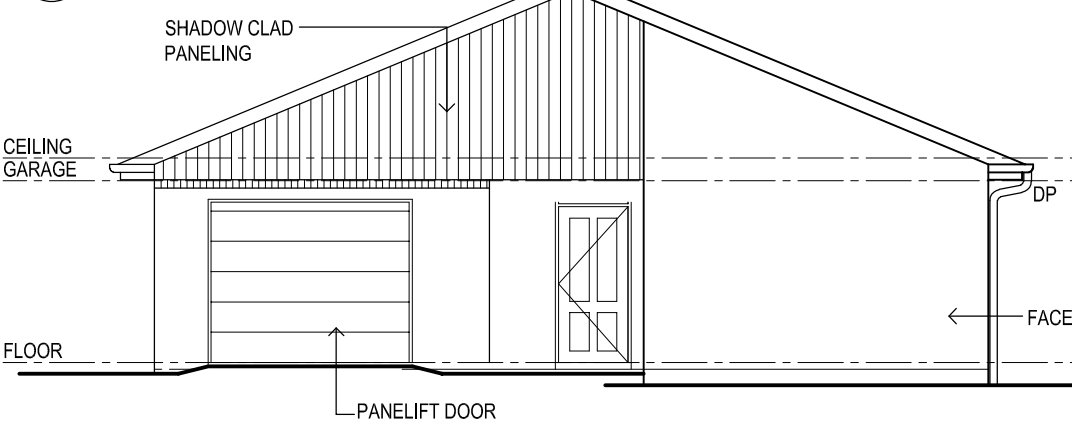
Drawing No **AB-03** Rev **A**



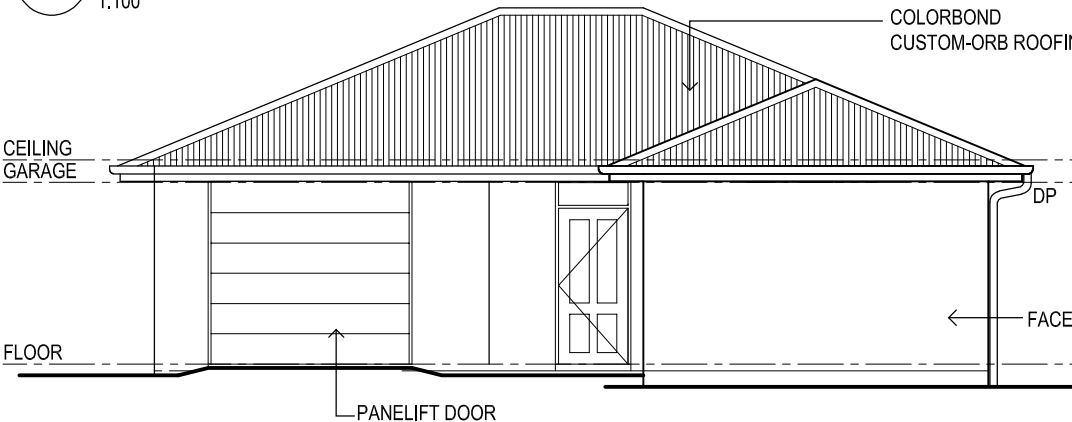
01 Elevation - Rear - Option 1
1:100



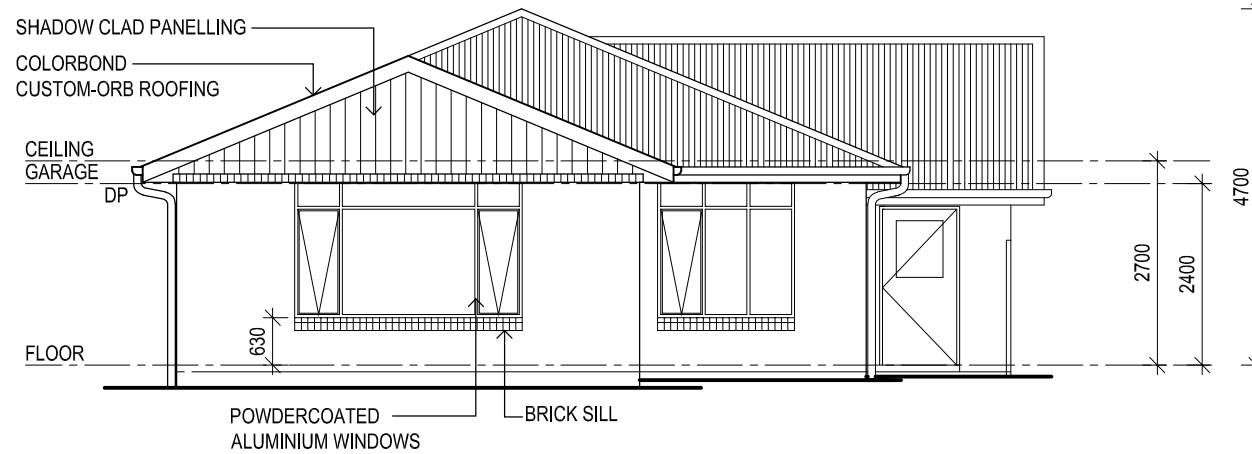
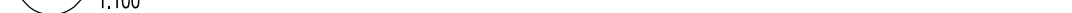
03 Elevation - Rear - Option 3
1:100



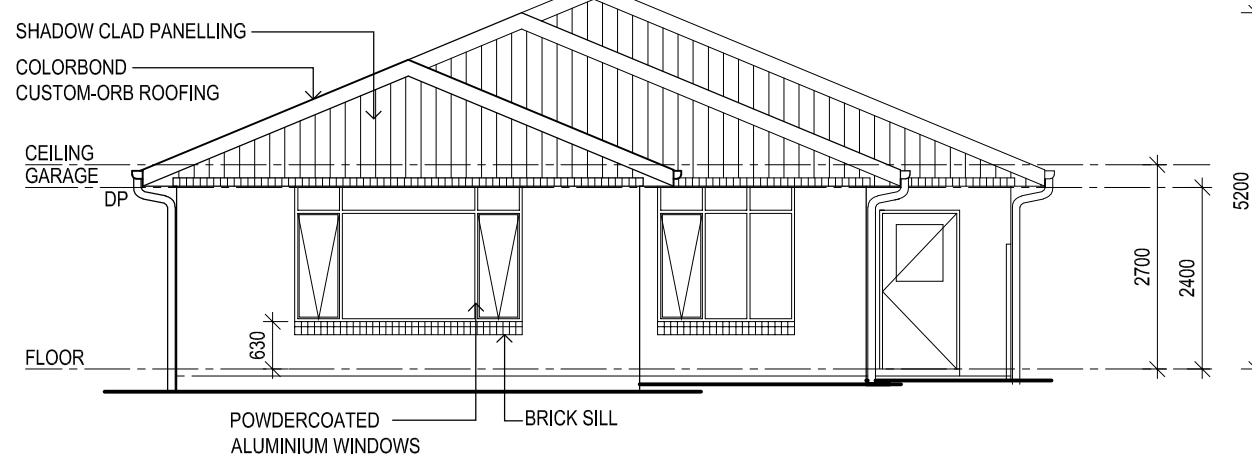
05 Elevation - Street - Option 1
1:100



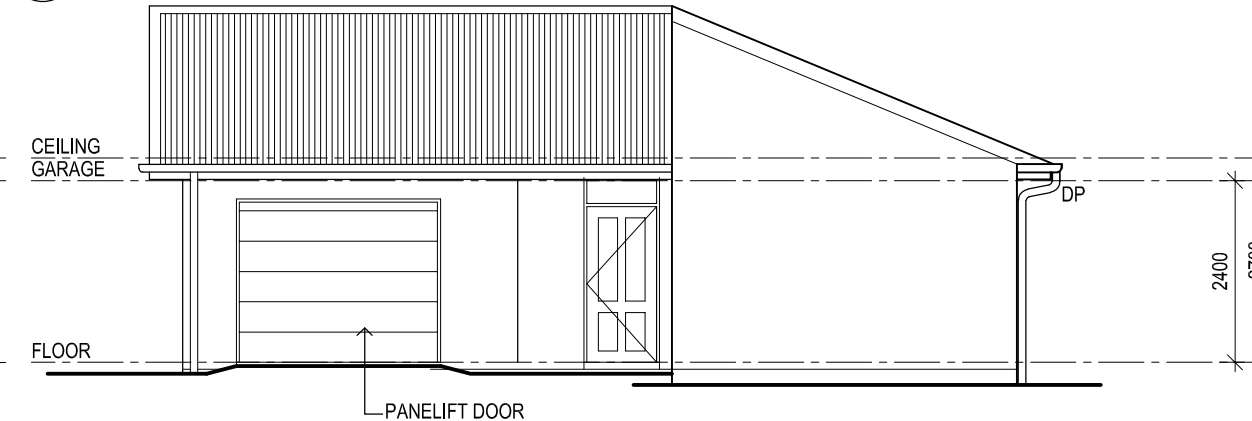
07 Elevation - Street - Option 3
1:100



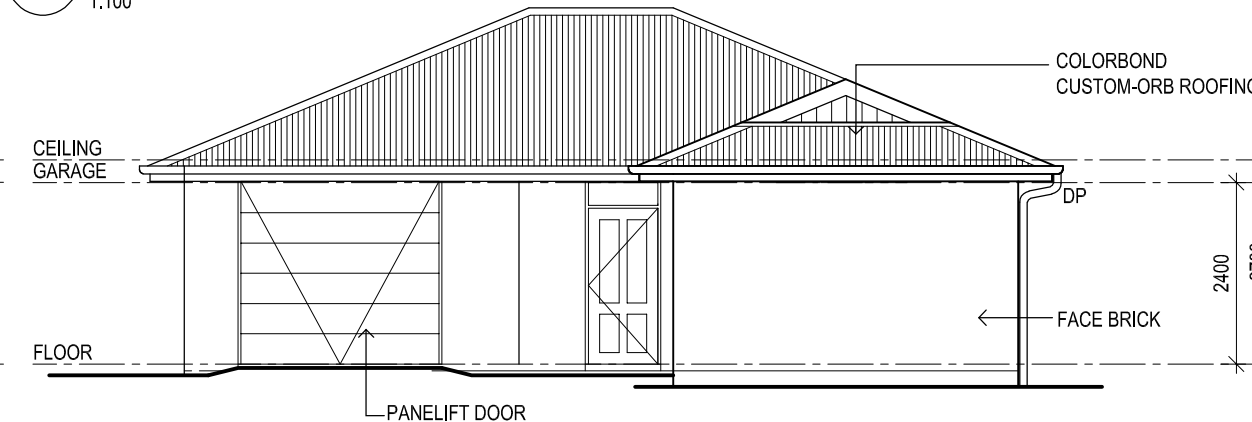
02 Elevation - Rear - Option 2
1:100



04 Elevation - Rear - Option 4
1:100



06 Elevation - Street - Option 2
1:100



08 Elevation - Street - Option 4
1:100



B Series

LEGEND

| | |
|-------|-----------------------------|
| A/C | AIR CONDITIONING UNIT |
| CJ | CONTROL JOINT |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |

NOTES

REFER 'ILU STANDARD' AE DRAWING
SERIES FOR DETAILS & OPTIONS

CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF
WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| A | GENERAL REVISIONS | JPI | 28.02.17 |

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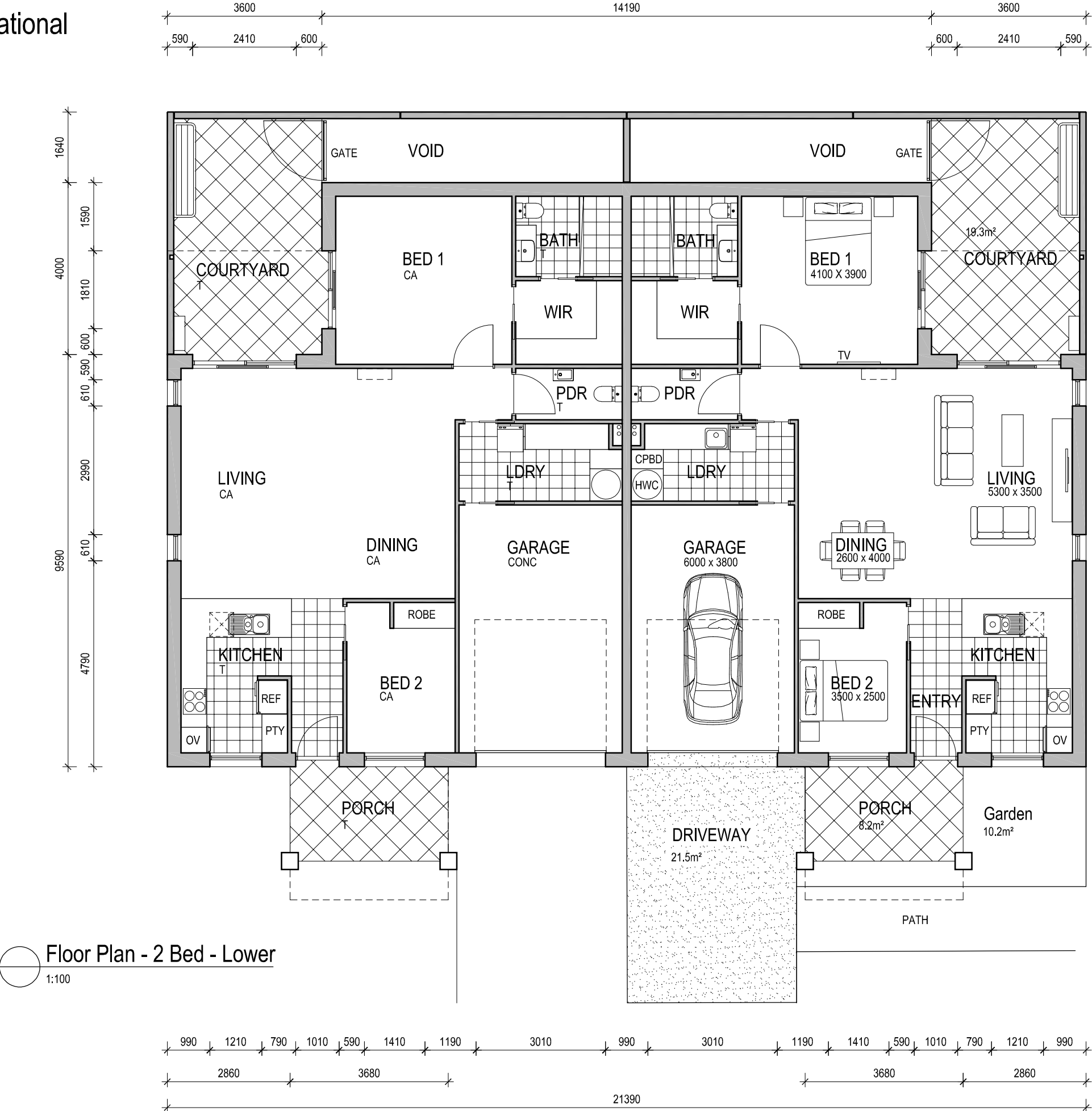
Project **Southern Cross Care (Tas) Inc.**
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Lindisfame

Title **B2 Elevations 2 - Options**

| | | | | |
|------------|--------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:100 @ A3 | Date | 19.01.17 | 004.13122 |
| Drawing No | AB-06 | Rev | A | |

Garden & Recreational Space

| | |
|-----------|--------------------|
| Porch | 8.2m ² |
| Garden | 10.2m ² |
| Courtyard | 19.3m ² |
| TOTAL | 37.7m ² |
| | |
| Driveway | 21.5m ² |



Floor Plan - 2 Bed - Lower
1:100

C Series

| LEGEND | |
|--------|-----------------------------|
| AC | AIR CONDITIONING UNIT |
| CA | CARPET |
| CONC | SEALED CONCRETE |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |
| SHR | SHOWER |
| SWB | SWITCHBOARD |
| T | TILE |
| TAP7 | EXTERNAL TAP |

| | |
|--------|---------------|
| D## | DOOR NUMBER |
| W # | WINDOW NUMBER |

NOTES
KITCHEN, BATH 1, POWDER & LAUNDRY 1 - REFER DRAWINGS AA-08-10 INCLUSIVE
EXTERNAL & INTERNAL DETAILS -REFER 'ILU STANDARD' AE DRAWING SERIES
OPTION A - REFER DRAWING AA-10
REFER STAGE 2 PLANS FOR DRYING COURT, DRIVEWAY, PATHS & SCREENS

CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| | | | |
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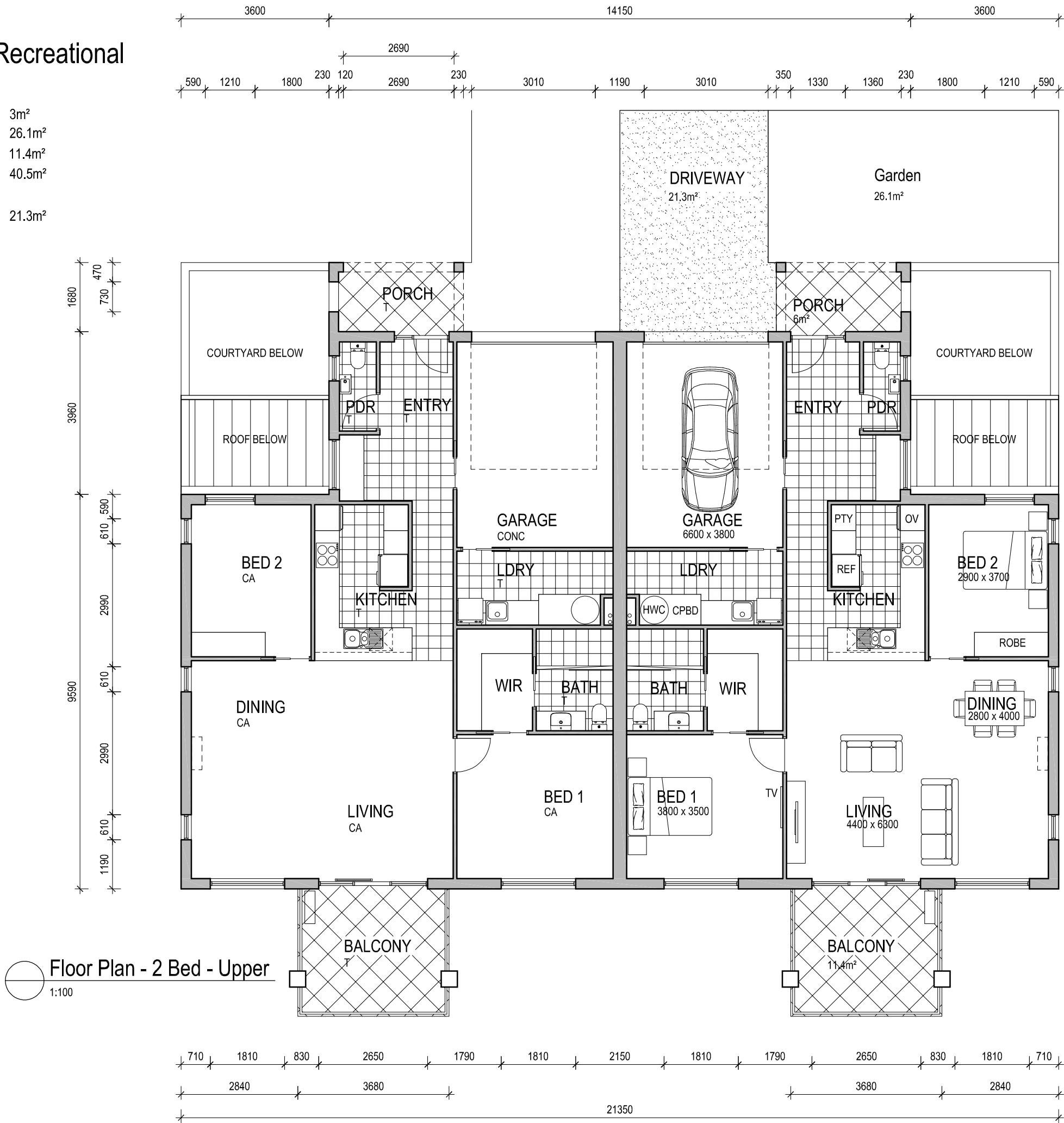
| | |
|---------|--|
| Project | Southern Cross Care (Tas) Inc. Springhaven Lifestyle Village 38 Gordons Hill Road Lindisfame |
|---------|--|

Title **C2 Floor Plan - Lower**

| | | | | |
|------------|--------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:100 @ A3 | Date | 14.02.17 | 004.13122 |
| Drawing No | AC-01 | Rev | A | |

Garden & Recreational Space

| | |
|----------|--------|
| Porch | 3m² |
| Garden | 26.1m² |
| Balcony | 11.4m² |
| TOTAL | 40.5m² |
| | |
| Driveway | 21.3m² |



C Series

| LEGEND | |
|--------|-----------------------------|
| AC | AIR CONDITIONING UNIT |
| CA | CARPET |
| CONC | SEALED CONCRETE |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |
| SHR | SHOWER |
| SWB | SWITCHBOARD |
| T | TILE |
| TAP7 | EXTERNAL TAP |

| | |
|-----|---------------|
| D## | DOOR NUMBER |
| W# | WINDOW NUMBER |

NOTES
KITCHEN, BATH 1, POWDER & LAUNDRY 1 - REFER DRAWINGS AA-08-10 INCLUSIVE
EXTERNAL & INTERNAL DETAILS -REFER 'ILU STANDARD' AE DRAWING SERIES
OPTION A - REFER DRAWING AA-10
REFER STAGE 2 PLANS FOR DRYING COURT, DRIVEWAY, PATHS & SCREENS

CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
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|---------|--|
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|---------|--|

| | |
|-------|------------------------------|
| Title | C2 Floor Plan - Upper |
|-------|------------------------------|

| | | | | |
|------------|--------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:100 @ A3 | Date | 14.02.17 | 004.13122 |
| Drawing No | AC-02 | Rev | A | |

C Series

- LEGEND**
- A/C AIR CONDITIONING UNIT
 - CJ CONTROL JOINT
 - DP DOWNPIPE
 - GAS INSTANTANEOUS GAS HOT WATER
 - LINE CLOTHESLINE
 - METER METER BOX
 - NBN NATIONAL BROADBAND NETWORK

NOTES
REFER 'ILU STANDARD' AE DRAWING
SERIES FOR DETAILS & OPTIONS

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
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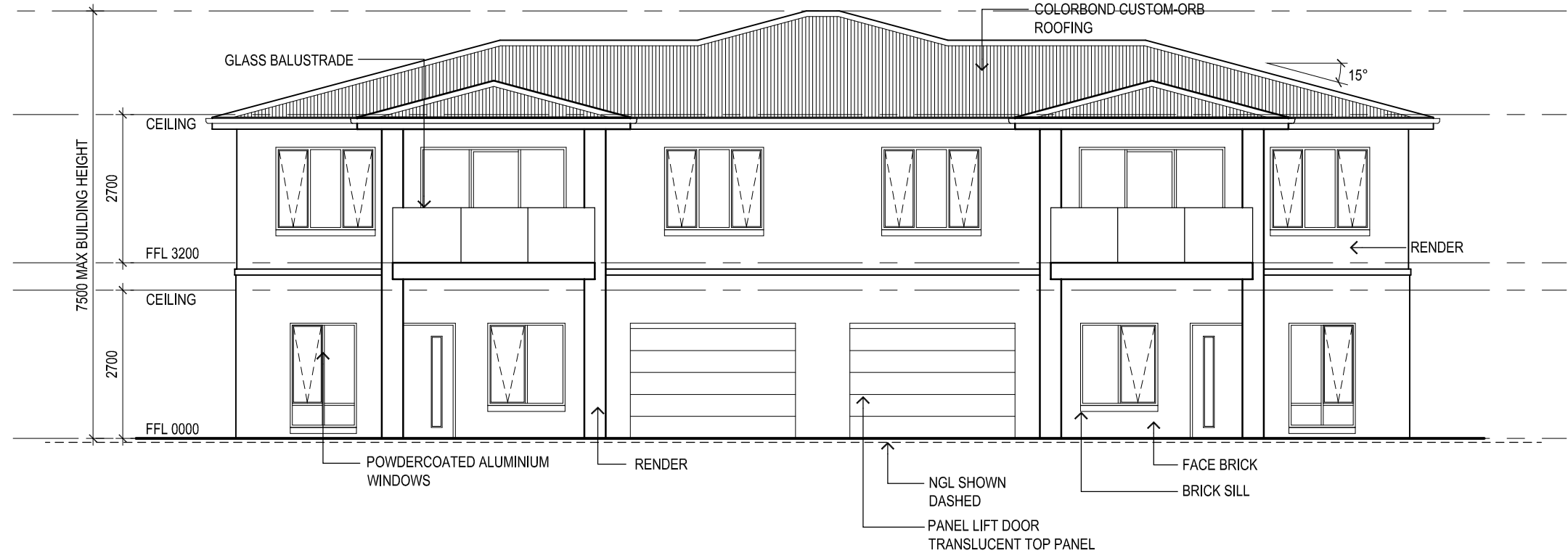
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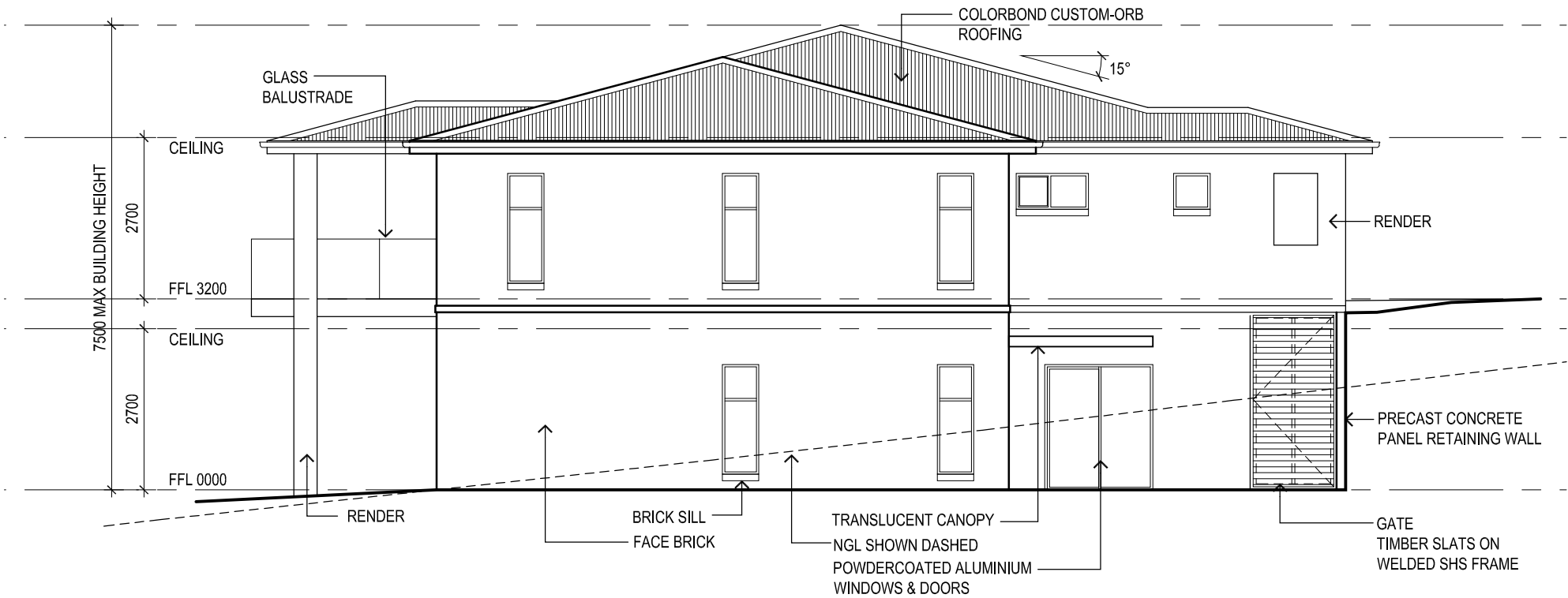
Project **Southern Cross Care (Tas) Inc.**
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Lindisfame

Title **C2 Elevations 1**

| | | | | |
|------------|------------|------|----------|----------------|
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| Scale | 1:100 @ A3 | Date | 19.01.17 | 004.13122 |
| Drawing No | AC-03 | | | Rev A |



01 Front Elevation - Lower Level - 2 Bed
1:100

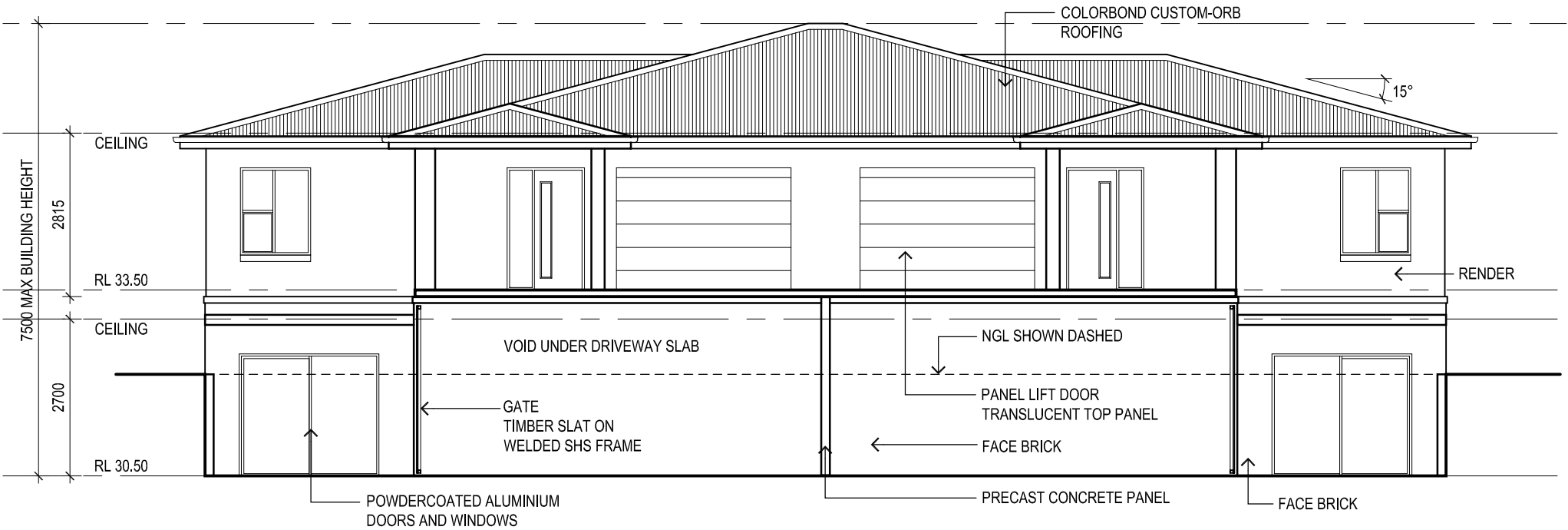


02 Side Elevation 1 - 2 Bed
1:100

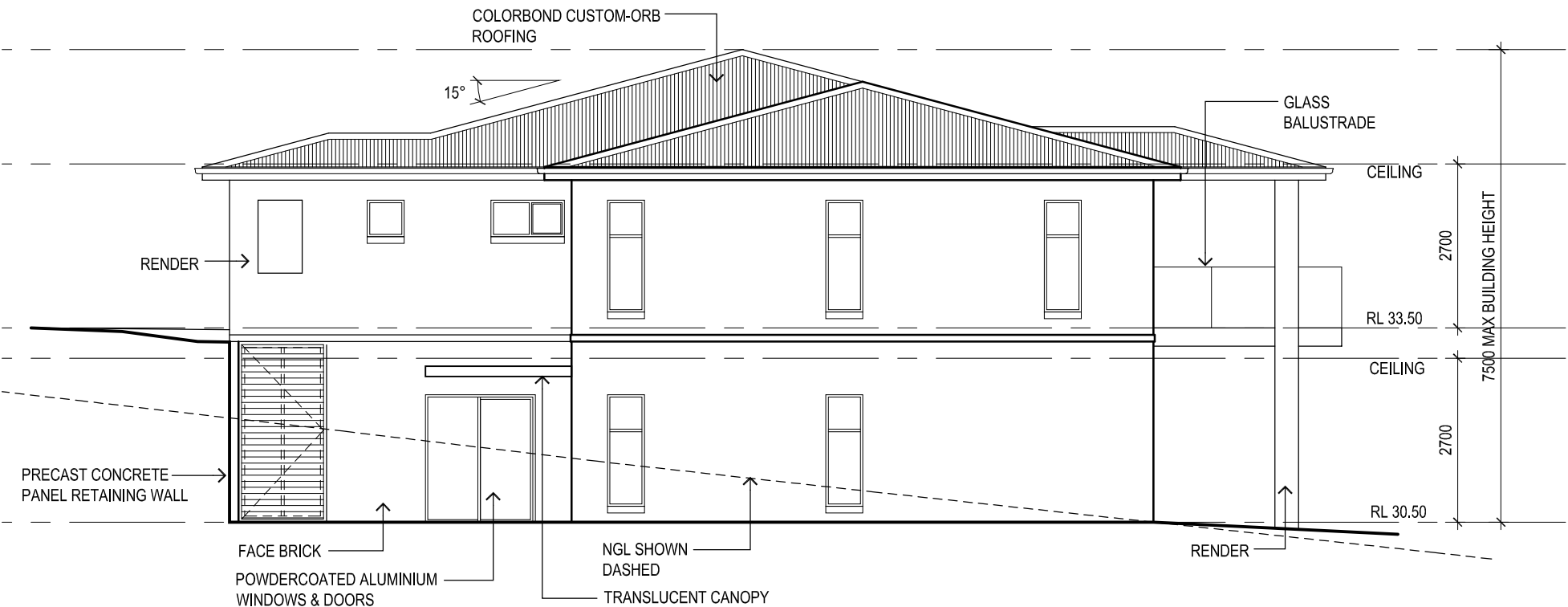
C Series

- LEGEND**
- A/C AIR CONDITIONING UNIT
 - CJ CONTROL JOINT
 - DP DOWNPIPE
 - GAS INSTANTANEOUS GAS HOT WATER
 - LINE CLOTHESLINE
 - METER METER BOX
 - NBN NATIONAL BROADBAND NETWORK

NOTES
REFER 'ILU STANDARD' AE DRAWING
SERIES FOR DETAILS & OPTIONS



01 Front Elevation - Upper Level - 2 Bed
1:100



02 Side Elevation 2 - 2 Bed
1:100

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| | | | |
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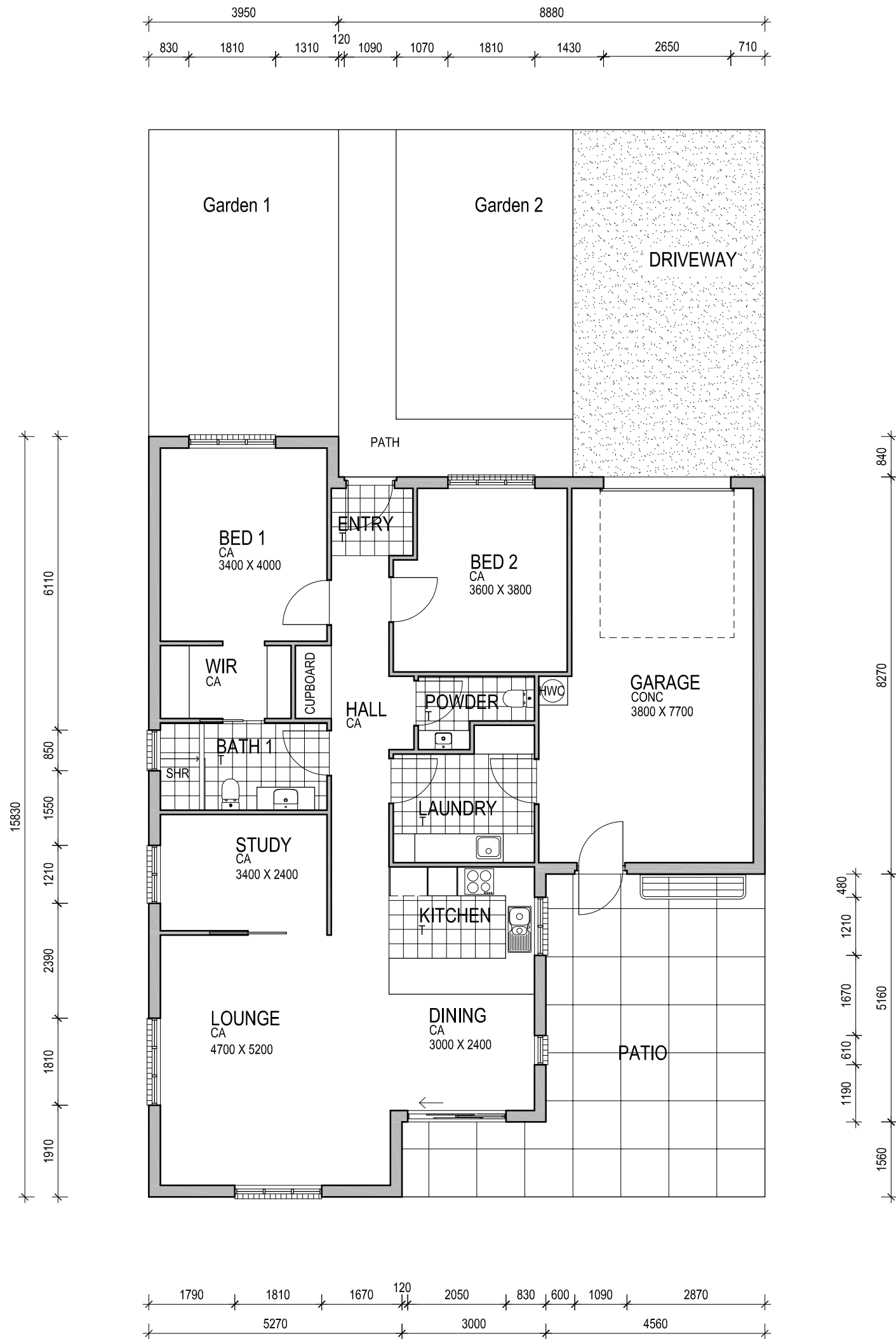
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Lindisfame

Title **C2 Elevations 2**

| | | | | |
|------------|------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:100 @ A3 | Date | 19.01.17 | 004.13122 |
| Drawing No | AC-04 | | | Rev A |



Series D

Floor Area 126.5m²
Garage Area 33.5m²
TOTAL 160m²

D Series

LEGEND

AC AIR CONDITIONING UNIT
CA CARPET
CONC CONCRETE
DP DOWNPIPE
GAS INSTANTANEOUS GAS HOT WATER
LINE CLOTHESLINE
METER METER BOX
NBN NATIONAL BROADBAND NETWORK
SL SKY LIGHT
SHR SHOWER
SWB SWITCHBOARD
T TILE
TAP7 EXTERNAL TAP

D## DOOR NUMBER

W WINDOW NUMBER
#

NOTES

KITCHEN, BATH 1, POWDER & LAUNDRY - REFER DRAWINGS AA-08-10 INCLUSIVE
EXTERNAL & INTERNAL DETAILS REFER 'ILU STANDARD' AE DRAWING SERIES
OPTION A - REFER DRAWING AA-10
REFER STAGE 2 PLANS FOR DRYING COURT, DRIVEWAY, PATHS & SCREENS

CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
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| Title | D4 Floor Plan | | |
| Drawn by | JPI | Appr PG | Project Number |
| Scale | 1:100 @ A3 | Date 14.02.17 | 004.13122 |
| Drawing No | AD-01 | | Rev A |

D Series

LEGEND

| | |
|-------|-----------------------------|
| A/C | AIR CONDITIONING UNIT |
| CJ | CONTROL JOINT |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |

NOTES

REFER 'ILU STANDARD' AE DRAWING
SERIES FOR DETAILS & OPTIONS

CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF
WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT

| Rev | Description | Dwn | Date |
|-----|-------------------|-----|----------|
| | | | |
| | | | |
| | | | |
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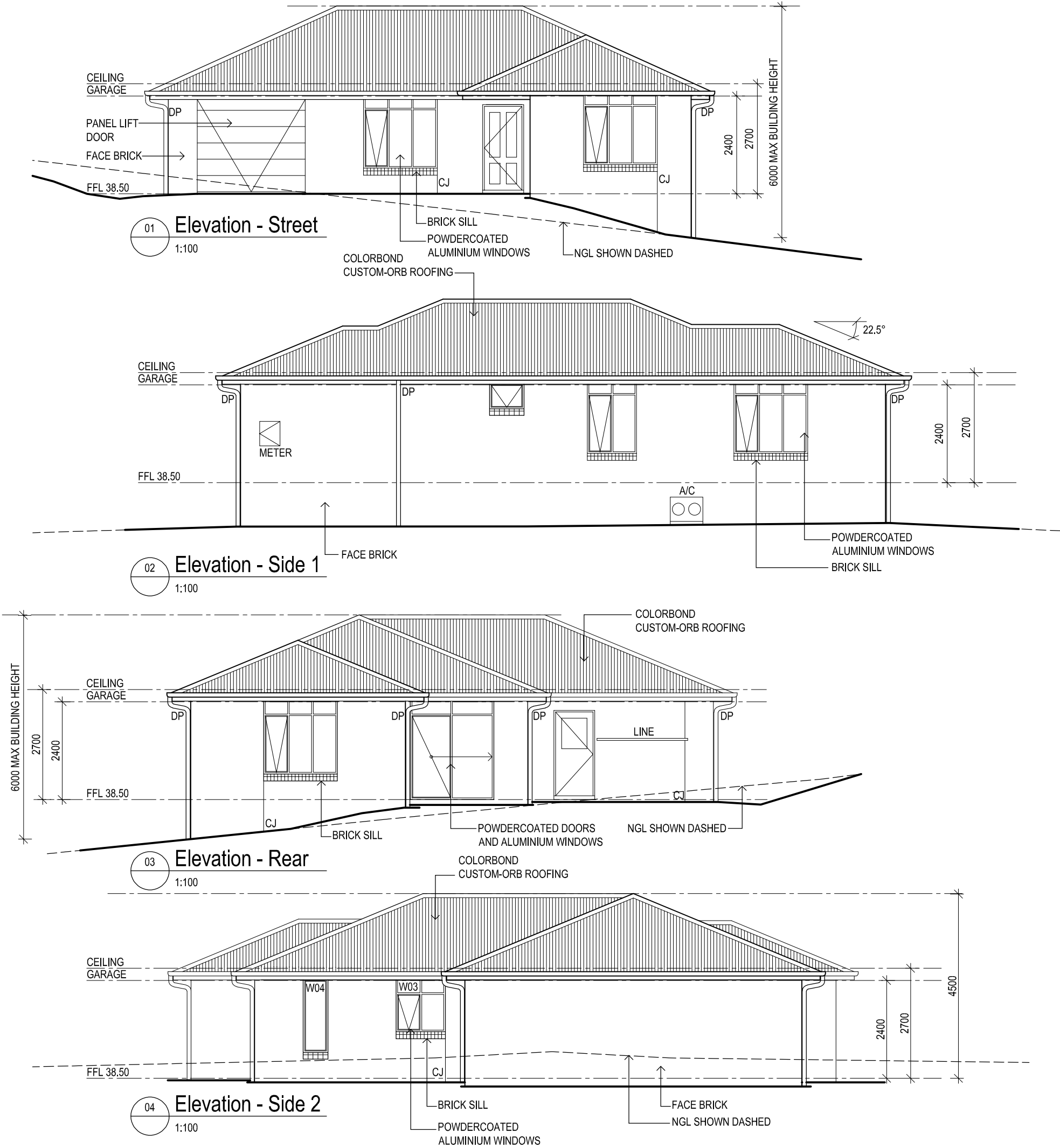
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Lindisfame

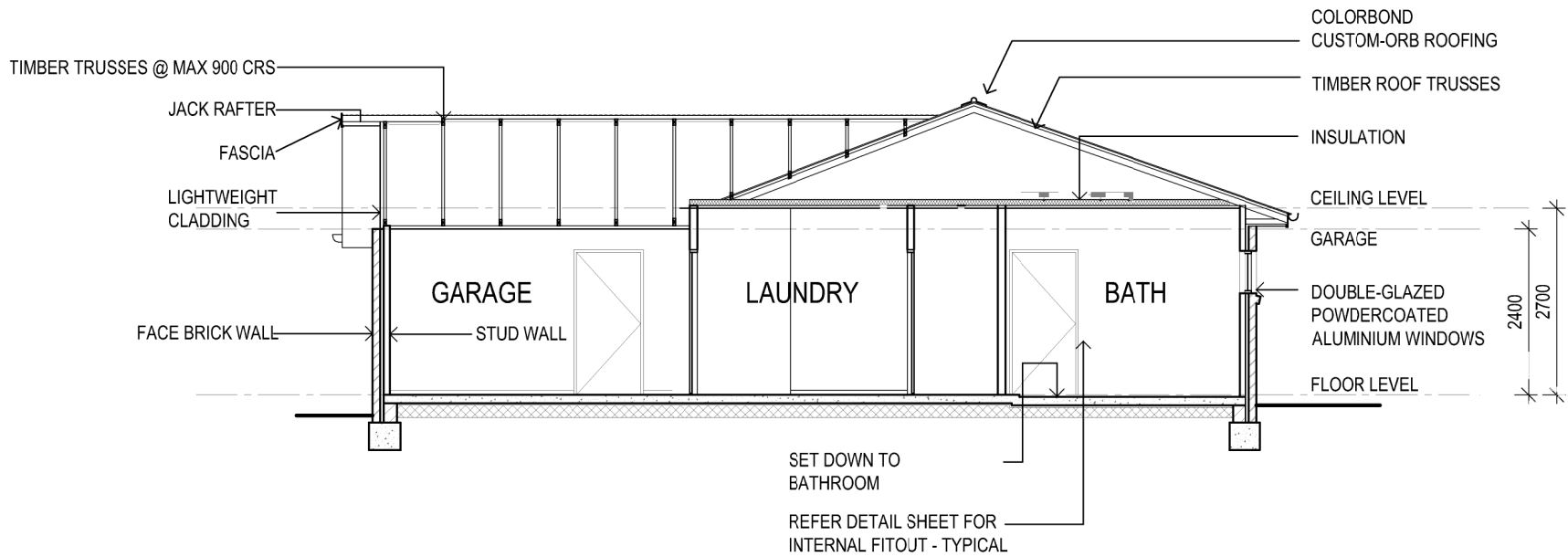
Title **D4 Elevations 1**

| | | | | |
|------------|--------------|------|----------|----------------|
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| Scale | 1:100 @ A3 | Date | 19.01.17 | 004.13122 |
| Drawing No | AD-02 | | | Rev A |



D Series

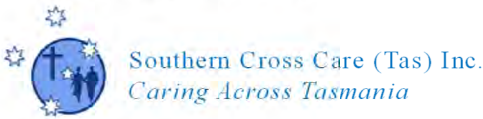
NOTES
REFER STANDARD DRAWING DETAILS
DRAWINGS AE01 - AE03 INCLUSIVE



Section
1:100

| Rev | Description | Dwn | Date |
|-----|-------------|-----|------|
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

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Springhaven Lifestyle Village
38 Gordons Hill Road
Lindisfarne

Title **D4 Section**

Drawn by JPI Appr PG Project Number

Scale 1:100 @ A3 Date 14.02.17 004.13122

Drawing No **AD-03** Rev **A**

D Series

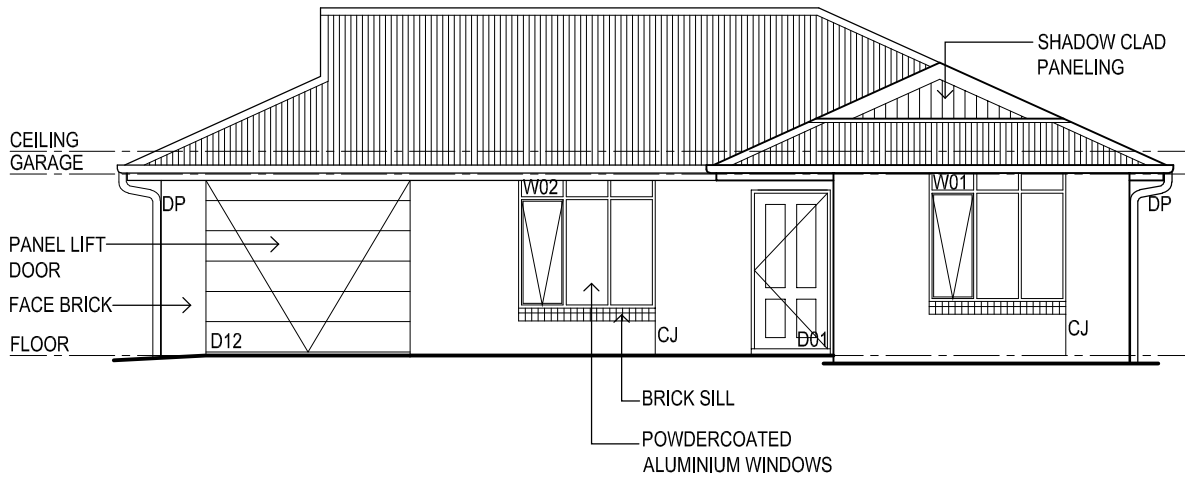
LEGEND

| | |
|-------|-----------------------------|
| A/C | AIR CONDITIONING UNIT |
| CJ | CONTROL JOINT |
| DP | DOWNPIPE |
| GAS | INSTANTANEOUS GAS HOT WATER |
| LINE | CLOTHESLINE |
| METER | METER BOX |
| NBN | NATIONAL BROADBAND NETWORK |

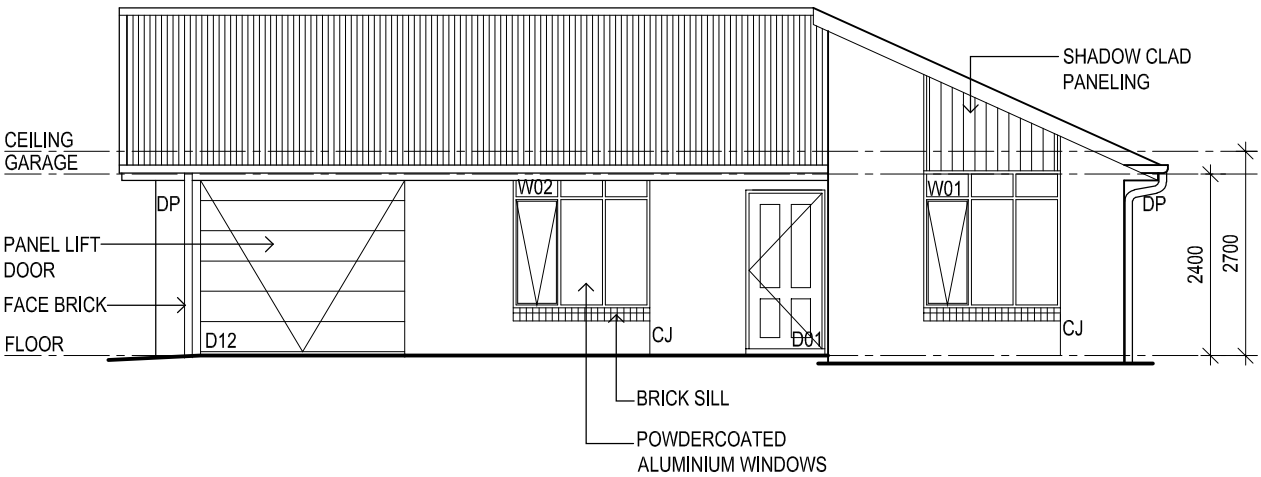
NOTES

REFER 'ILU STANDARD' AE DRAWING
SERIES FOR DETAILS & OPTIONS

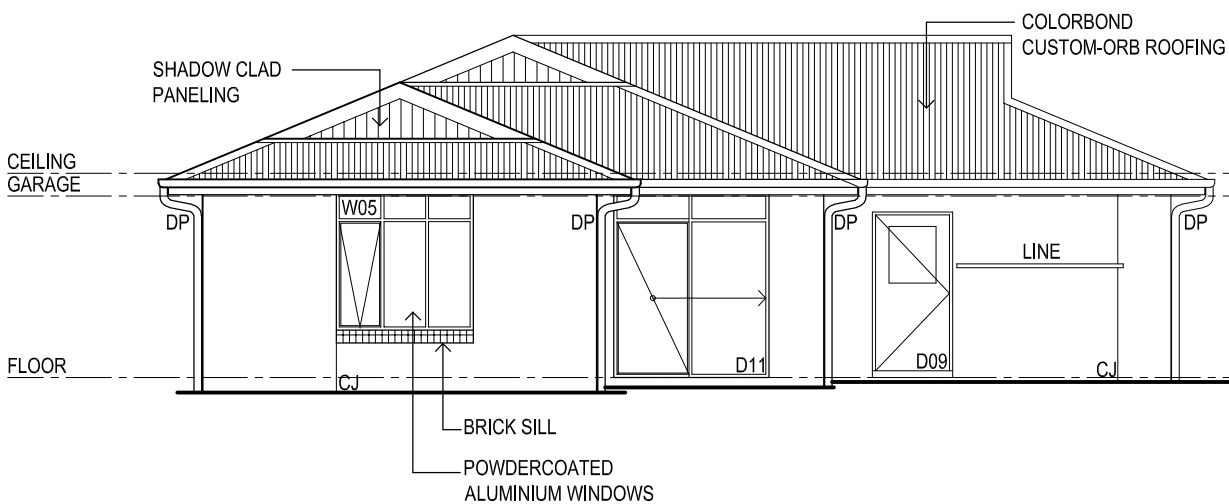
CONFIRM ON 1:200 SITE PLANS FOR LOCATIONS OF
WINDOWS ETC.
SERVICES TO RELOCATE TO SUIT



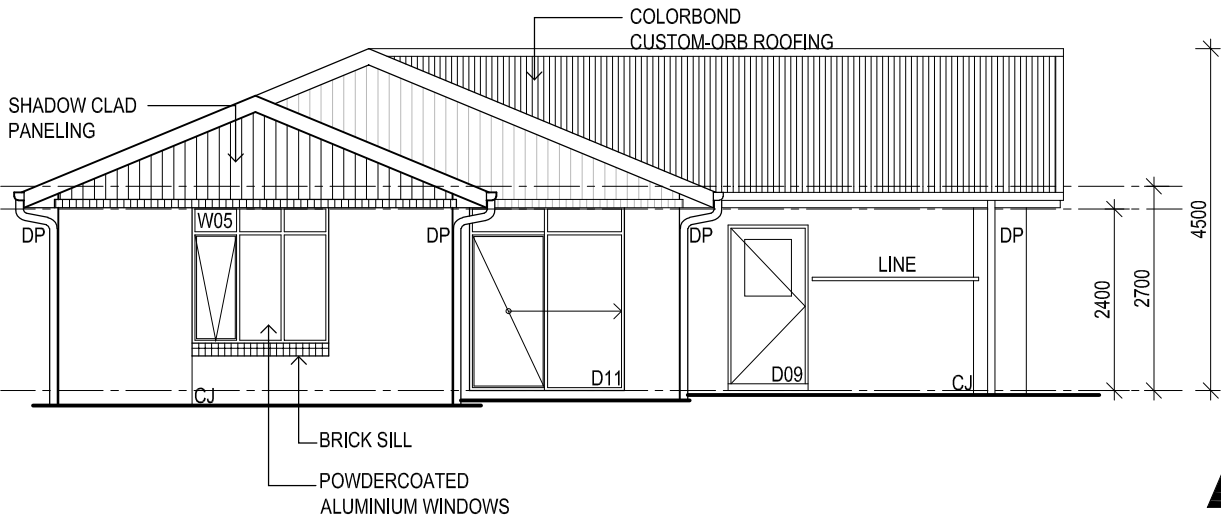
01 Elevation - Street - Option 1
1:100



02 Elevation - Street - Option 2
1:100



03 Elevation - Rear - Option 1
1:100



04 Elevation - Rear - Option 2
1:100

| Rev | Description | Dwn | Date |
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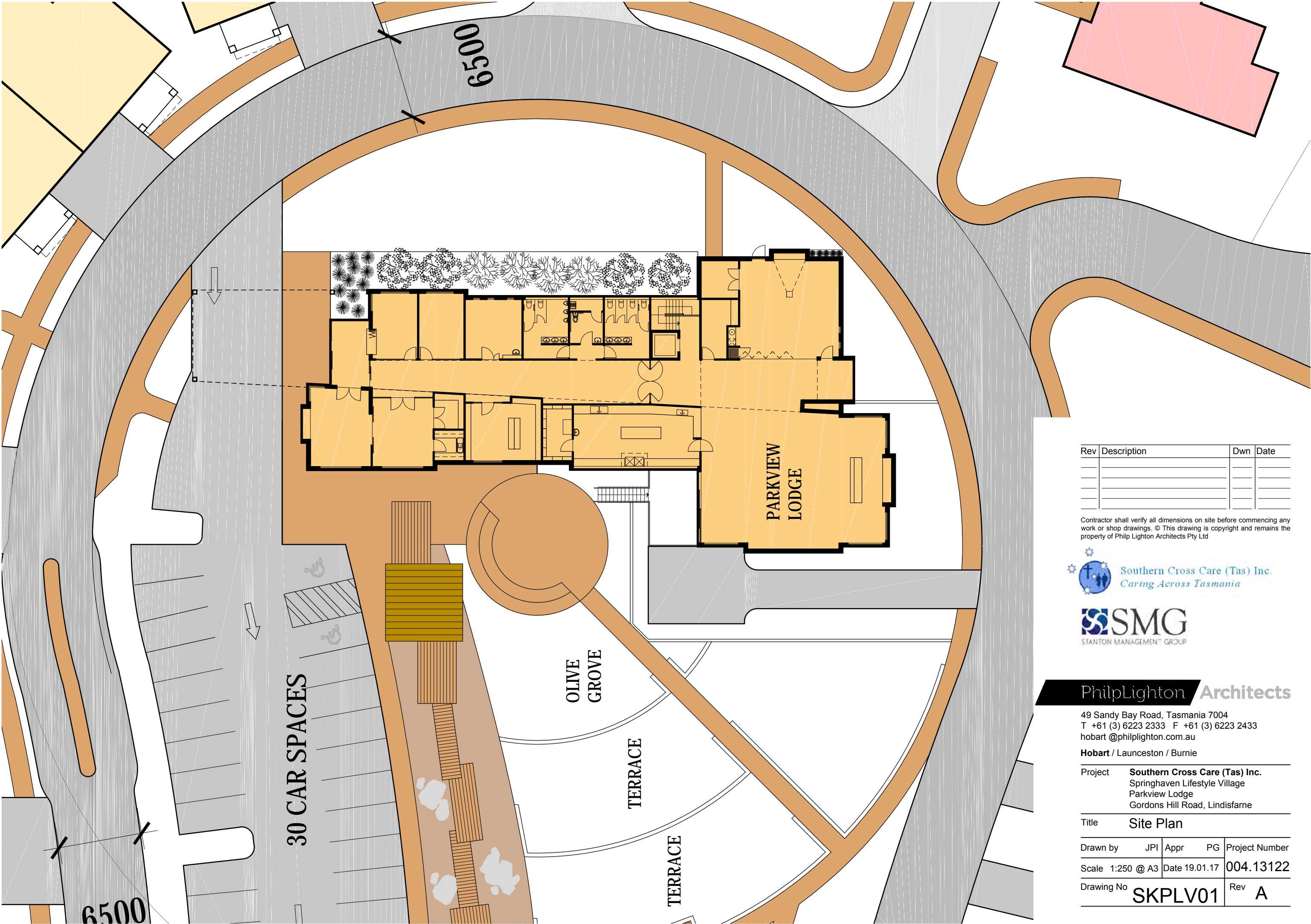
Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
38 Gordons Hill Road
Lindisfame

Title **D4 Elevations 2 - Options**

Drawn by JPI Appr PG Project Number

Scale 1:100 @ A3 Date 19.01.17 004.13122

Drawing No **AD-05** Rev



| Rev | Description | Dwn | Date |
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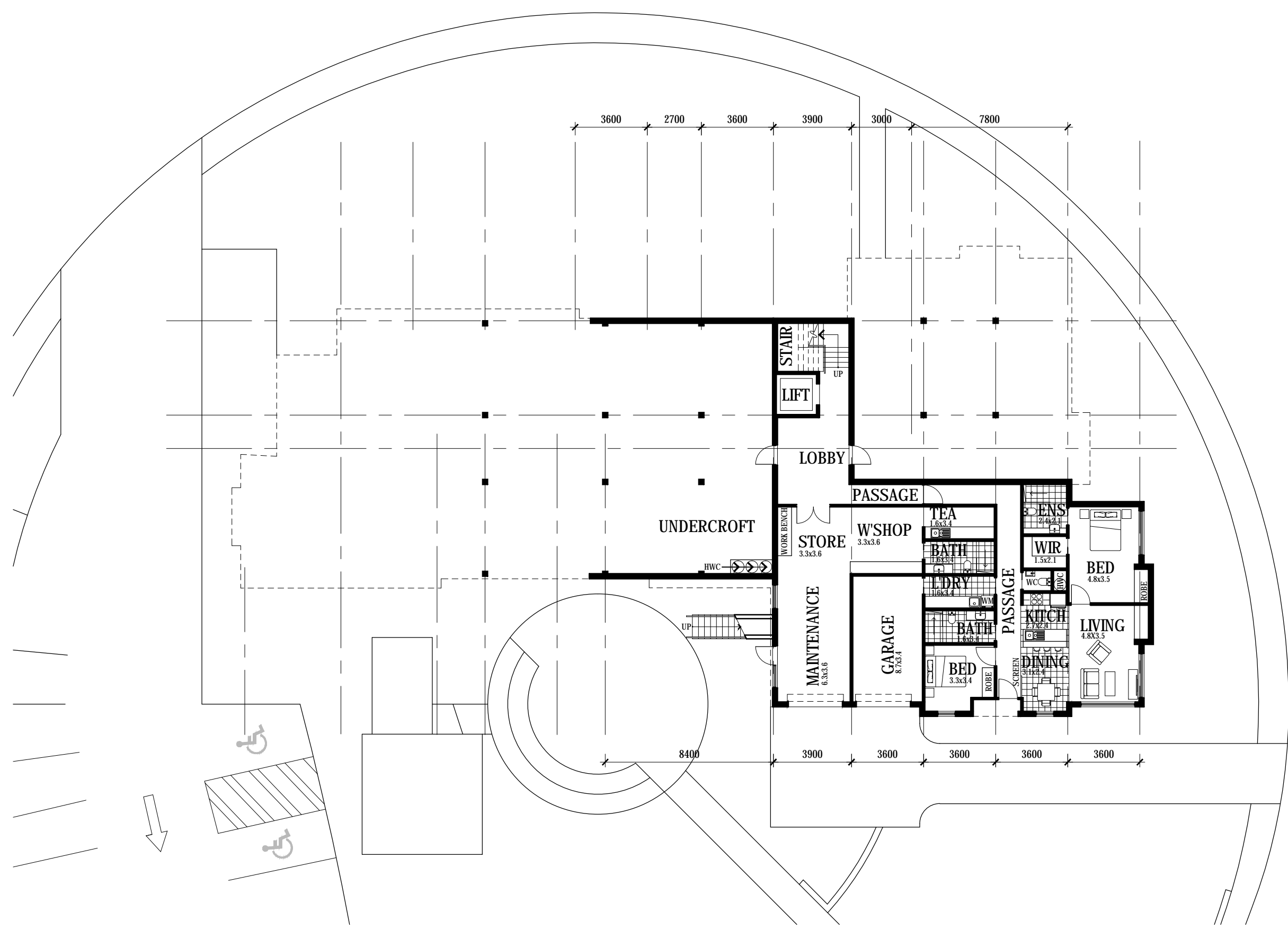
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Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
Parkview Lodge
Gordons Hill Road, Lindisfarne

Title **Site Plan**

| | | | | |
|------------|------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:250 @ A3 | Date | 19.01.17 | 004.13122 |
| Drawing No | SKPLV01 | | Rev | A |



| Rev | Description | Dwn | Date |
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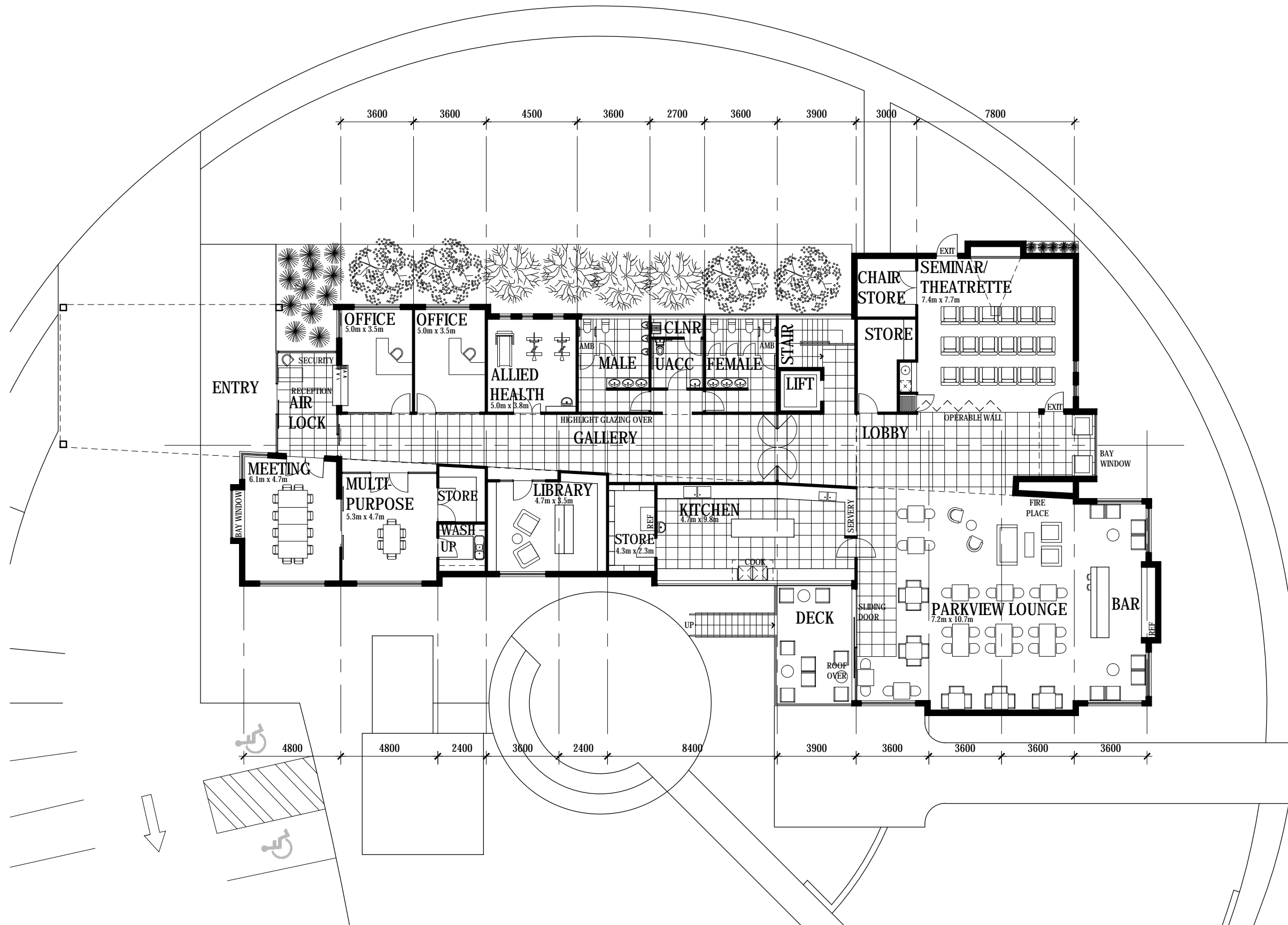
Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
Parkview Lodge
Gordons Hill Road, Lindisfarne

Title **Floor Plan - Lower**

Drawn by JPI Appr PG Project Number

Scale 1:200 @ A3 Date 19.01.17 **004.13122**

Drawing No **SKPLV02** Rev **A**



| Rev | Description | Dwn | Date |
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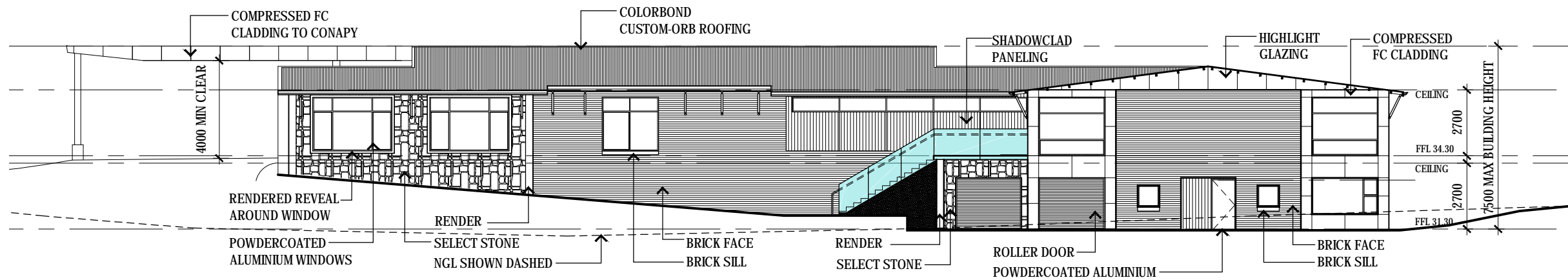
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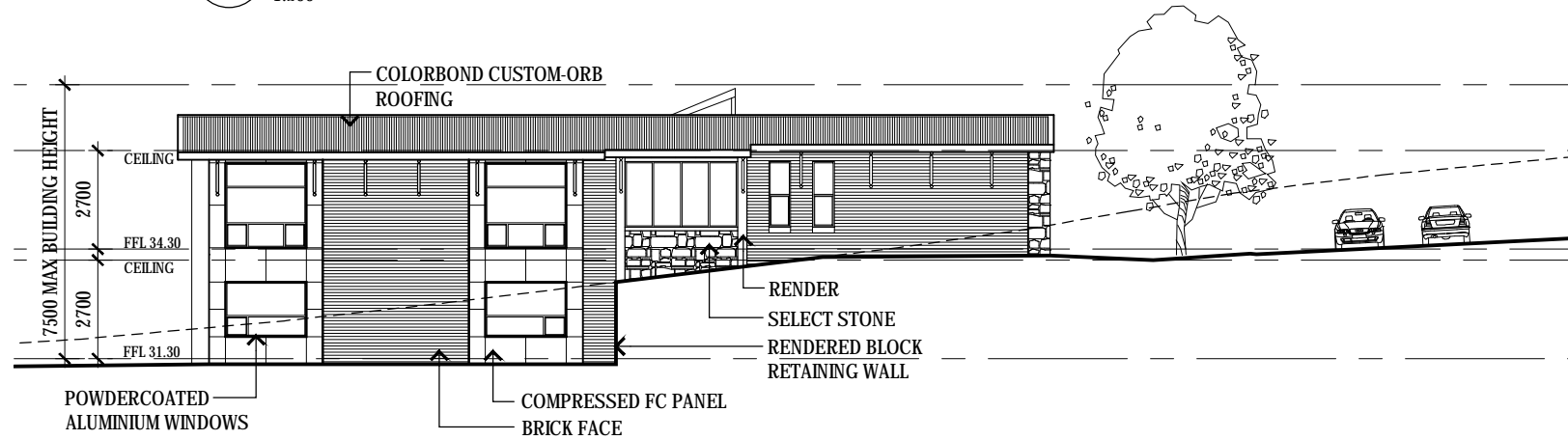
Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
Parkview Lodge
Gordons Hill Road, Lindisfarne

Title **Floor Plan - Upper**

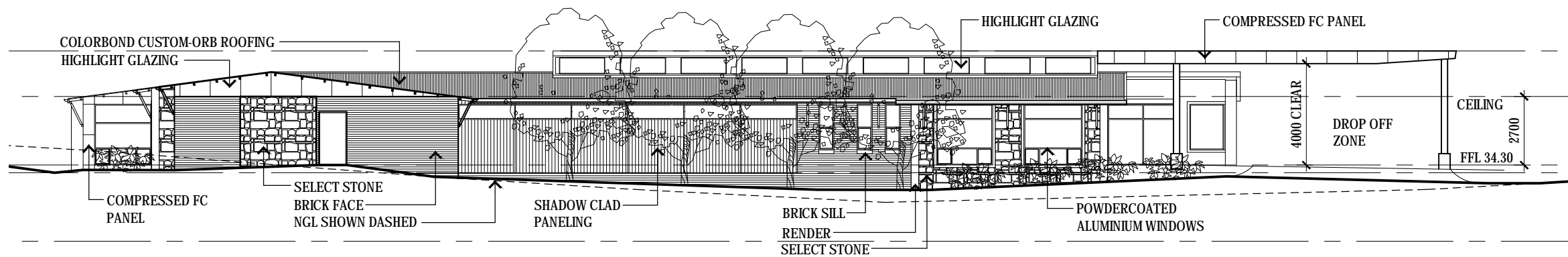
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| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:200 @ A3 | Date | 19.01.17 | 004.13122 |
| Drawing No | SKPLV03 | | | Rev A |



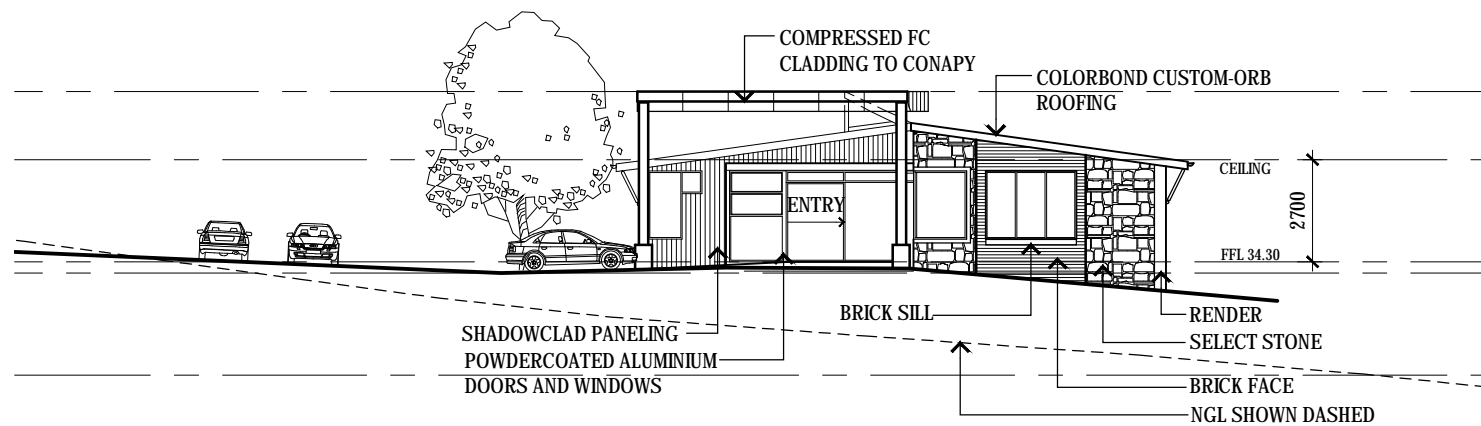
01 Elevation - South
1:200



02 Elevation - East
1:200



03 Elevation - North
1:200



04 Elevation - West
1:200

| Rev | Description | Dwn | Date |
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Project **Southern Cross Care (Tas) Inc.**
Springhaven Lifestyle Village
Parkview Lodge
Gordons Hill Road, Lindisfarne

Title **Elevations**

| | | | | |
|------------|------------|------|----------|----------------|
| Drawn by | JPI | Appr | PG | Project Number |
| Scale | 1:200 @ A3 | Date | 19.01.17 | 004.13122 |
| Drawing No | SKPLV04 | | | Rev A |

GENERAL NOTES:

1. THESE DRAWINGS ARE TO BE READ IN CONJUNCTION WITH THE ARCHITECTURAL AND OTHER SUBMITTED DOCUMENTATION FOR PLANNING.
2. SURVEY DATA UNDERTAKEN AND PROVIDED BY LEAY AND COX.
3. ARCHITECTURAL LAYOUT PROVIDED BY PHILP LIGHTON ARCHITECTS.
4. INTERNAL UNIT HYDRAULICS TO BE DESIGNED DURING DETAILED DESIGN.
5. PLANS PROVIDED ARE CONCEPT PLANS ONLY AND ARE SUBJECT TO FULL ENGINEERING INVESTIGATION AND DETAILED DESIGN.
6. REFER TO MUSIC MODEL FOR PROPOSED STORMWATER TREATMENT.
7. 1% AEP OVERLAND FLOW PATH TO BE PROVIDED DURING DETAILED ENGINEERING DESIGN.
8. FULL ENGINEERING DESIGN TO INCORPORATE WATER SENSITIVE URBAN DESIGN PRINCIPLES FOR DISPOSAL OF STORMWATER AS SHOWN VIA CENTRAL SWALE AND OTHER FUTURE ROAD DRAINAGE/TREATMENT MEASURES.
9. FULL ENGINEERING DESIGN OF DEVELOPMENT WILL BE DESIGNED TO ACCOMMODATE A 5% AEP (1:20 ARI) STORM EVENT.

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OVERALL PLAN
SCALE 1:500 (A1)

HALF SCALE PRINT



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| REV. | DESCRIPTION | DATE | REV. | DESCRIPTION | DATE |
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| A | PLANNING APPROVAL | 16/03/17 | | | |

The logo for Aldanmark Consulting Engineers, featuring a stylized 'A' made of two triangles.

ALDANMARK CONSULTING ENGINEERS

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| | | | |
|---|--------------------------|------------------|---------------|
| CLIENT: SOUTHERN CROSS CARE (TAS) | SHEET: OVERALL SITE PLAN | DRAWN: MM | APPROVED: -- |
| ADDRESS: GORDONS HILL ROAD LINDISFARNE, TAS 7054 | PROJECT: SCC SPRINGHAVEN | SCALE: 1:500 | SIZE: A1 |
| ISSUE: PLANNING APPROVAL | PROJECT No: 16E66-11 | DATE: 16/03/2017 | SHEET No: C01 |
| | | | REV No: A |

The screenshot displays the SanSim software interface for a wastewater treatment plant. The top half shows a process flow diagram with various treatment units and flow streams. The bottom half features a summary table titled "Pollutant Loads (kg/yr)" showing the reduction of various pollutants through the treatment process.

Process Flow Diagram:

- Inputs:** Road to F1000s - 3500m³ (100% imp.), Road to Sewer - 700m³ (100% imp.), Roads/Carparks to G10 - 4240m³ (100% imp.), Driveway to G10 - 3750m³ (100% imp.), Footpath to G10 - 360m³ (100% imp.), Petrolwash - 3000m³ (100% Pers.), Deck/Courtyard - 500m³ (100% imp.).
- Units:** F1000s, Filters, 100m³ Scale, 13 x Fermenting Bed (10°F 0.5M 100H), SF Chamber, 7th & Belmont Flats (MCC), Recirculating Flats.
- Flows:** F1000s to Filters, Filters to 100m³ Scale, 100m³ Scale to 13 x Fermenting Bed, 13 x Fermenting Bed to SF Chamber, SF Chamber to 7th & Belmont Flats, 7th & Belmont Flats to Recirculating Flats, Recirculating Flats to F1000s.
- Outputs:** Road to G10 - 7500m³ (100% imp.).

Pollutant Loads (kg/yr) Table:

| | Sources | Residual Load | % Reduction |
|--------------------------------|---------|---------------|-------------|
| Flow (ML/yr) | 9.49 | 9.41 | 0.8 |
| Total Suspended Solids (kg/yr) | 2430 | 144 | 94.1 |
| Total Phosphorus (kg/yr) | 4.16 | 1.04 | 75 |
| Total Nitrogen (kg/yr) | 22.2 | 10.6 | 51.9 |
| Gross Pollutants (kg/yr) | 361 | 0 | 100 |



SITE TRUNK STORMWATER MAIN PLAN

SCALE 1:500 (A1)

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CLIENT: SOUTHERN CROSS CARE (TAS)

ADDRESS:
GORDONS HILL ROAD
LINDISFARNE, TAS 7054

SHEET: SITE CONCEPT STORMWATER + MUSIC MODEL

| | |
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| PROJECT: | SCC SPRINGHAVEN |
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ISSUE: PLANNING APPROVAL

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| SCALE: 1:500 | SIZE: A1 | DATE: 16/03/2017 |
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| 16E66-11 | C02 | A |
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USED FOR CONSTRUCTION UNLESS

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OVERALL SITE PERSPECTIVE
NOT TO SCALE - 1.5x VERTICAL EXAGGERATION

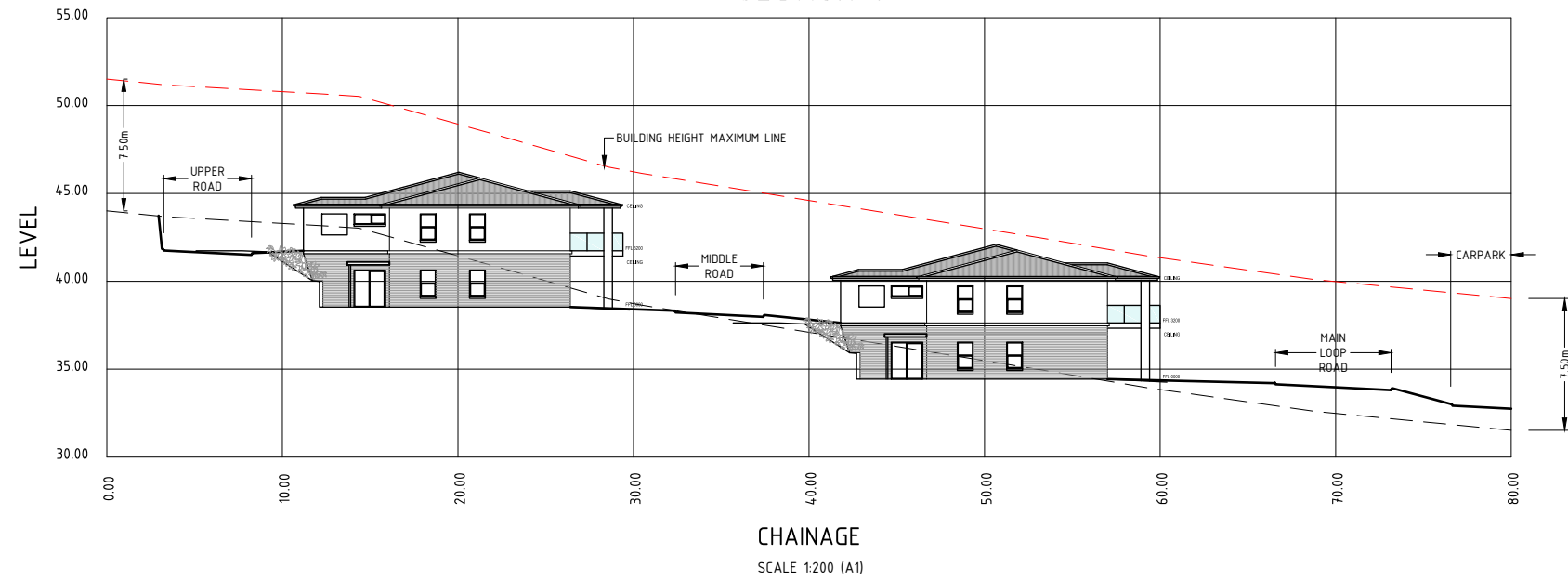


MIDDLE ROAD SITE PERSPECTIVE
NOT TO SCALE - 1.5x VERTICAL EXAGGERATION

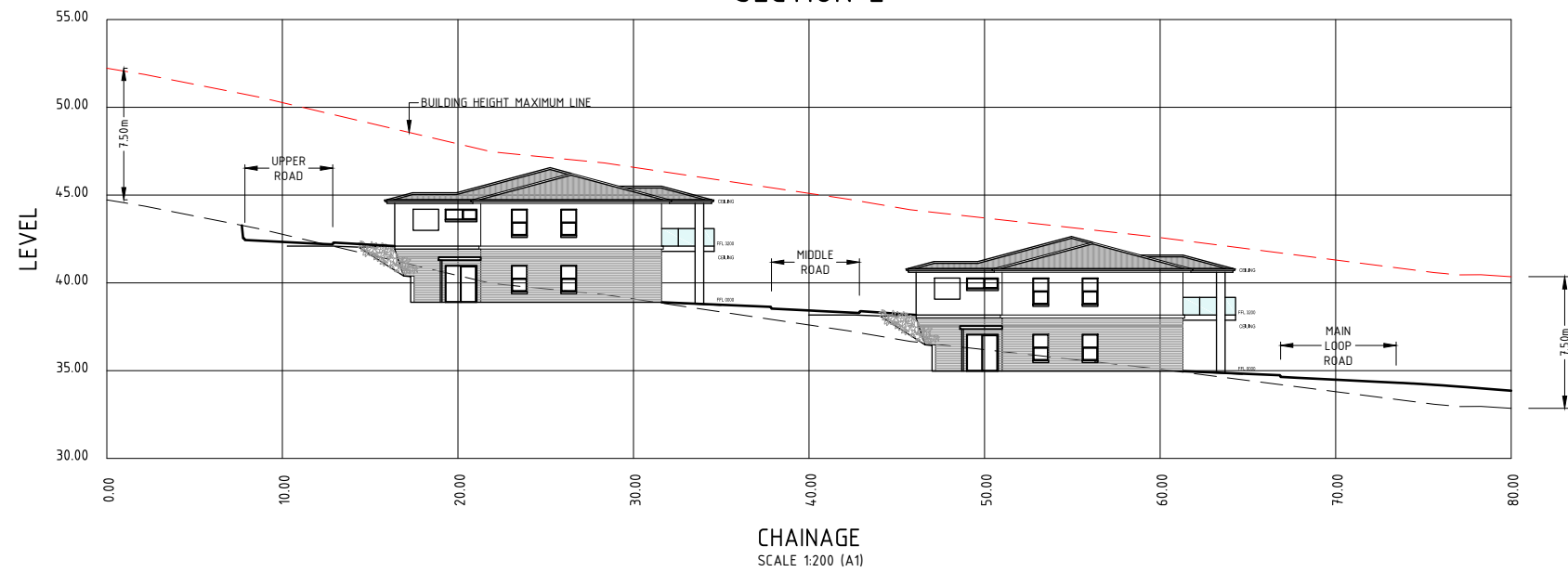


UPPER ROAD SITE PERSPECTIVE
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SECTION 1



SECTION 2



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| CLIENT: | SOUTHERN CROSS CARE (TAS) | SHEET: | SECTIONS AND PERSPECTIVES | DRAWN: | MM | APPROVED: | MM |
| ADDRESS: | GORDONS HILL ROAD LINDISFARNE, TAS 7054 | PROJECT: | SCC SPRINGHAVEN | SCALE: | 1:200 | SIZE: | A1 |
| ISSUE: | PLANNING APPROVAL | PROJECT No: | 16E66-11 | DATE: | 16/03/2017 | SHEET No: | C03 |
| | | | | | | REV No: | A |

Attachment 3

